



*The County of San Diego*

# Zoning Administrator Hearing Report

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Date:	January 23, 2025	Case/File No.:	Pala Mesa Commercial PDS2020-STP-20-021; PDS2020-ER-20-02-003
Place:	County Conference Center 5520 Overland Avenue San Diego, CA 92123	Project:	Commercial Center Expansion
Time:	8:30 a.m.	Location:	3233 Old Highway 395
Agenda Item:	#1	General Plan:	General Commercial (C-1)
Appeal Status:	Site Plan Decision Appealable to the Planning Commission	Zoning:	General Commercial (C36)
Applicant/Owner:	TGRH 3, LLC	Community:	Fallbrook Community Planning Area
Environmental:	CEQA §15183	APNs:	125-050-54-00

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## A. OVERVIEW

The purpose of this staff report is to provide the Zoning Administrator with the information necessary to make a finding that the mitigation measures identified in the General Plan Update Environmental Impact Report (GPU EIR) will be undertaken for a proposed Site Plan (STP) pursuant to California Environmental Quality Act (CEQA) Guidelines §15183(e)(2).

CEQA Guidelines §15183 allows a streamlined environmental review process for projects that are consistent with the uses established by existing zoning, community plan, or general plan policies for which an EIR was certified. CEQA Guidelines §15183 specifies that examination of environmental effects shall be limited to those effects that:

- 1) Are peculiar to the project or the parcel on which the project would be located, and were not analyzed as significant effects in a prior EIR on the zoning action, general plan, or community plan, with which the project is consistent;
- 2) Are potentially significant off-site impacts and cumulative impacts which were not discussed in the prior EIR prepared for the general plan, community plan or zoning action; or
- 3) Are previously identified significant effects which, as a result of substantial new information which was not known at the time the EIR was certified, are determined to have a more severe adverse impact than discussed in the prior EIR.

CEQA Guidelines §15183(c) further specifies that if an impact is not peculiar to the parcel or to the proposed project, has been addressed as a significant effect in the prior EIR, or can be substantially mitigated by the imposition of uniformly applied development policies or standards, then an additional EIR need not be prepared for that project solely on the basis of that impact.

CEQA Guidelines §15183(e)(2) further requires the lead agency to make a finding at a public hearing when significant impacts are identified that could be mitigated by undertaking mitigation measures previously identified in the EIR on the planning and zoning action.

In accordance with CEQA Guidelines §15183, the project was evaluated to examine whether additional environmental review might be necessary for the reasons stated in §15183. As discussed in the attached Statement of Reasons for Exemption from Additional Environmental Review and 15183 Checklist (15183 Findings) dated October 17, 2024, the project qualifies for an exemption from further environmental review.

The approval or denial of the proposed STP would be a subsequent and separate decision made by the Director of Planning & Development Services (PDS).

**B. DEVELOPMENT PROPOSAL**

**1. Project Description**

The Pala Mesa Commercial Site Plan (Project) is proposed on an approximately 4.36-acre site. The Project consists of the expansion and remodeling of an existing commercial plaza. The Project site contains an existing market with attached businesses consisting of restaurants and personal services such as financial and insurance companies. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. The proposed project is located at 3233 Old Highway 395 in the Fallbrook Community Planning Area in the unincorporated County of San Diego. (Figures 1 and 2). The project site is subject to the Semi-Rural Regional Category, Land Use Designation General Commercial (C-1). The Zoning Use Regulation for the site is General Commercial (C36). The site is also subject to a "B" Special Area Designator for community design review which requires the processing of a Site Plan permit. The existing parking lot will be expanded and re-designed to include a total of 134 parking spaces. The Project also includes proposed landscaping as well as signage for the commercial tenants of the site. Access to the site will be provided by two commercial driveways connecting to Old Highway 395. Water and sewer service will be provided by the Rainbow Municipal Water District. The existing road on the northern portion of the Project site known as Via Belmonte will be widened with a half width improvement of 14 feet and a graded half width of twenty feet along the south side of Via Belmonte. The existing Project frontage along Old Highway 395 will be improved and include restriping of Old Highway 395 in order to accommodate a left-turn lane into the Project while retaining the two existing commercial driveway access points. The Project also includes a vacation and/or removal and re-dedication of road, drainage, and slope easements related to maintenance along Old Highway 395. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export.

The proposed use is consistent with the Zoning and General Plan Land Use Designation of the property established by the General Plan Update for which an Environmental Impact Report (EIR) was certified by the Board of Supervisors on August 3, 2011 (GPU EIR).

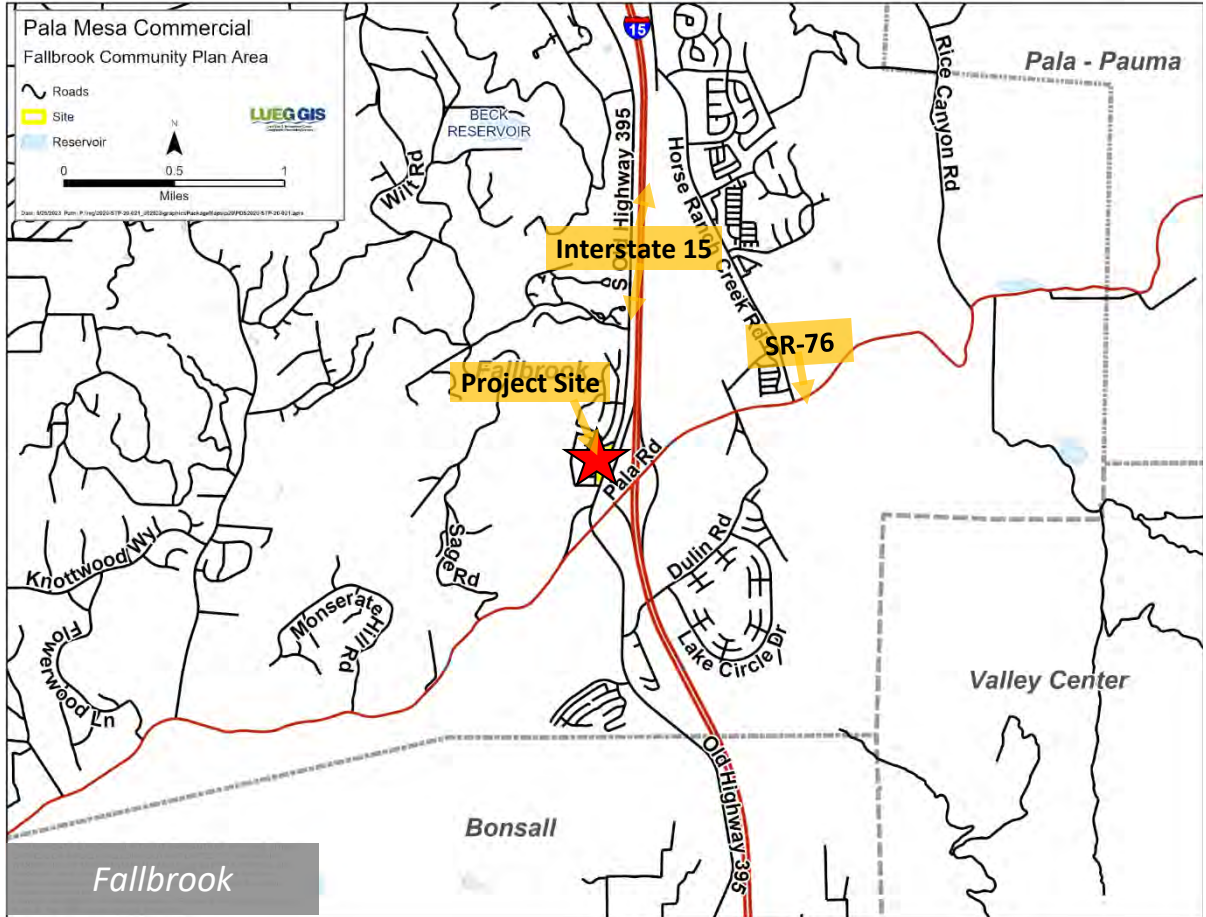


Figure 1: Vicinity Map



Figure 2: Aerial Map (Project Site, Existing Conditions)

## C. ANALYSIS AND DISCUSSION

### 1. Key Requirements for Requested Action

The Zoning Administrator should consider the requested actions and determine if the following findings can be made:

- a) The Project is consistent with the existing zoning, community plan, or general plan policies for which the GPU EIR was certified.
- b) There are no project specific effects which are peculiar to the project or its site.
- c) There are no project specific impacts which the GPU EIR failed to analyze as significant effects.
- d) There are no potentially significant off-site and/or cumulative impacts which the GPU EIR failed to evaluate.
- e) There is no substantial new information which results in more severe impacts than anticipated by the GPU EIR.

## 2. Project Analysis

### a. Traffic

The County of San Diego Transportation Study Guidelines were adopted by the County Board of Supervisors in September of 2022 to address Senate Bill 743 (SB 743). The new established criteria for determining the significance of transportation impacts is Vehicle Miles Traveled (VMT) and is further addressed below. Although no longer utilized as the standard for evaluating transportation impacts under CEQA, the County's General Plan identified level of service (LOS) as being a required analysis per Policy M-2.1 and is therefore also addressed.

Section 15064.3 of the CEQA Guidelines details new regulations, effective statewide July 1, 2020, based on SB 743 that sets forth specific considerations for evaluating a project's transportation impacts. As previously discussed, the new established criteria for determining the significance of transportation impacts is VMT which refers to the amount and distance of automobile travel attributable to a project. The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses.

In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. Based on the Local Mobility Analysis, all existing nearby road segments and intersections within the Project vicinity operate at Levels of Service of C or D or better (acceptable Levels of Service). The Local Mobility Analysis modeled the Levels of Service of segments and intersections in the area with the addition of the Project and concluded that all segments and intersections will continue to operate at a Level of Service of C or D or better (acceptable Levels of Service). The Local Mobility Analysis recommends the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The existing decomposed granite pathway along the frontage of the Project site in accordance with the Community Trails Master Plan will be retained along the frontage of Old Highway 395 with the minor widening. The Project also includes half width improvements to Via Belmonte including a half width improvement of 14 feet and a graded half width of twenty feet. The Project is conditioned to maintain adequate sight distance along the driveway entrances of the Project throughout the life of the Project. The Project is conditioned to verify adequate sight distance at various points of construction in order to ensure that items such as

landscaping or fencing are not installed in sight lines that would prevent sight distance. Lastly, the Project also includes a vacation and/or removal and re-dedication of road, drainage, and slope easements related to maintenance along Old Highway 395.

b. Noise

The Project will not expose people to potentially significant noise levels that exceed the allowable limits of the General Plan, Noise Ordinance, or other applicable standards. Based on a review of the County's noise contour maps, the Project is located within noise contours identified in the noise element of the General Plan ranging 60 CNEL or more due to the proximity of the Project site to Interstate 15 and Old Highway 395. The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the western property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles.

As previously discussed, the GPU EIR determined impacts from excessive noise levels to be less than significant with mitigation. The Project would have a less-than-significant impact with the incorporation of design features and conditions such as compliance with the Noise Ordinance

and to limit idling of trucks. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

c. Greenhouse Gas Emissions

The Project's Greenhouse Gas (GHG) impacts were assessed using a project-specific, locally appropriate threshold, as guided by CEQA Guidelines Section 15064.4. Due to the absence of an adopted CAP when the project was submitted with a complete application in 2020, compliance with the CAP was not utilized as the threshold in determining potential greenhouse gas (GHG) emission impacts. Based on the design elements and scope of the Project, current guidance provided by the Bay Area Air Quality Management District (BAAQMD) was used to evaluate greenhouse gas emissions. A justification report for the BAAQMD guidance has been prepared which outlines design elements focusing on building and transportation. The justification report and guidance supports how a project would contribute its "fair share" of the statewide long-term GHG reduction goals and the guidance applied in the San Diego region. Due to the absence of an adopted CAP when the project was submitted with a complete application in 2020, compliance with the CAP was not utilized as the threshold in determining potential greenhouse gas (GHG) emission impacts.

The Project is consistent with both the building and transportation design elements as outlined in the BAAQMD justification report. The Project is conditioned to not use natural gas for the operations of the Project. Operation of the project would require use of water for landscape maintenance which is required to be in conformance with the Landscape Ordinance and requirements to demonstrate water use reduction. The Project will obtain its water supply from Rainbow Municipal Water District and the Project is conditioned to process a final Landscape Plan which implements additional measures in verifying that the Project is consistent with the water use reduction requirements of the Landscape Ordinance. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. The Project is also conditioned to be consistent with the CALGreen Tier 2 standards for compliance with off-street electric vehicle requirements which will result in increased availability of electrical vehicle charging stations within the Fallbrook Community and motorists traveling along Old Highway 395 and Interstate 15. The Project is estimated to include 57 electrical vehicle capable spaces of which 19 spaces would include charging stations. Lastly, the emissions associated with construction and operation of the Project were quantified in the Greenhouse Gas Analysis dated July 2023 by Ldn Consulting, Inc. Construction emissions associated with the development of the Project are temporary and expected to be approximately 249 metric tons of carbon dioxide equivalent (MTCO<sub>2e</sub>). Operational emissions of the Project were estimated to be 825 MTCO<sub>2e</sub>. Therefore, the project would not generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment, and impacts would be less than significant.

**D. PUBLIC INPUT**

During the 32-day public disclosure period, from October 17, 2024 to November 18, 2024 staff received twelve comments, one late letter, and general questions. Several comments were related to general support for the Project or mixed feedback supporting the Project with the exception of the use of the gas station. Multiple comment letters stated that they supported remodeling the property. Additionally, several comments raised concerns with the use of the gas station, traffic, traffic noise, and vandalism.

Use of Gas Station:

The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with the Zoning Ordinance, the use of a gas station is a permitted use within the zone. Although the use is permitted within the zone, the Project includes a Site Plan permit in accordance with the “B” Special Area Designator of the property which requires community design review. The Project has been designed in conformance with the I-15 Design Review Corridor Guidelines and includes redevelopment of an existing commercial site that has been in the community since the 1970s.

Traffic and Traffic Noise:

As previously discussed, the traffic analysis prepared for the Project demonstrated that the Project will not have a significant impact associated with Traffic and Transportation and the Project is in conformance with County Standards for analyzing traffic impacts. The Project includes design features such as installing a left-turn lane in order to assist with access into the Project site. The Project is also consistent with the County of San Diego Transportation Study Guide for evaluating impacts related to traffic and VMT. As previously discussed, noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project.

Vandalism:

Several community concerns have been raised related to potential vandalism on the Project site and individuals gathering in vacant areas of the property. The Project includes remodeling of an existing commercial plaza on a Project site with vacant areas on the northern and southern portions of the property. Construction of the Project will result in removal of the vacant areas and additional lighting within a parking lot which will assist with security and addressing vandalism concerns.

Please see Attachment E for the comment letters and responses.

**E. COMMUNITY PLANNING GROUP & DESIGN REVIEW BOARD RECOMMENDATION**

On September 19, 2022, the Fallbrook Community Planning Group (CPG) voted to recommend approval of the Project by a vote of 13-0-2 (13-Yes, 0-No, 2 Vacant/Absent). No additional comments or concerns were raised in the CPG meeting.

On September 17, 2020, the Interstate 15 (I-15) Design Review Board (DRB) voted to recommend approval of the project by a vote of 4-0-1 (4-Yes, 0-No, 1 Vacant/Absent). The DRB stated that the architecture and scale were appropriate for the area.

Meeting minutes for the Fallbrook CPG and I-15 DRB can be found in Attachment E.



F. STAFF RECOMMENDATIONS

Staff recommends that the Zoning Administrator adopt the Environmental Findings included in Attachment B, which includes a finding that the project is exempt from further environmental review pursuant to §15183 of CEQA.

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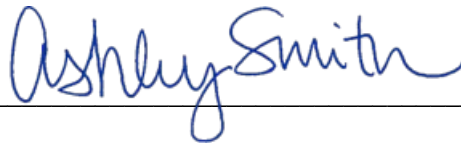
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AUTHORIZED REPRESENTATIVE: \_\_\_\_\_



ASHLEY SMITH, CHIEF

**ATTACHMENTS:**

- Attachment A – Planning Documentation
- Attachment B – Environmental Documentation
- Attachment C – Environmental Findings
- Attachment D – Site Plan
- Attachment E – Public Documentation

## Attachment A – Planning Documentation

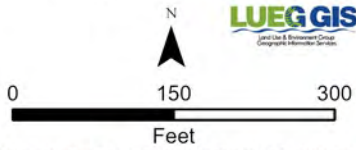


Pala Mesa Commercial  
PDS2020-STP-20-021

Fallbrook Community Plan Area

Roads

Site



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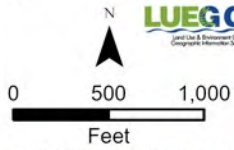
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Fallbrook Community Plan Area

Roads

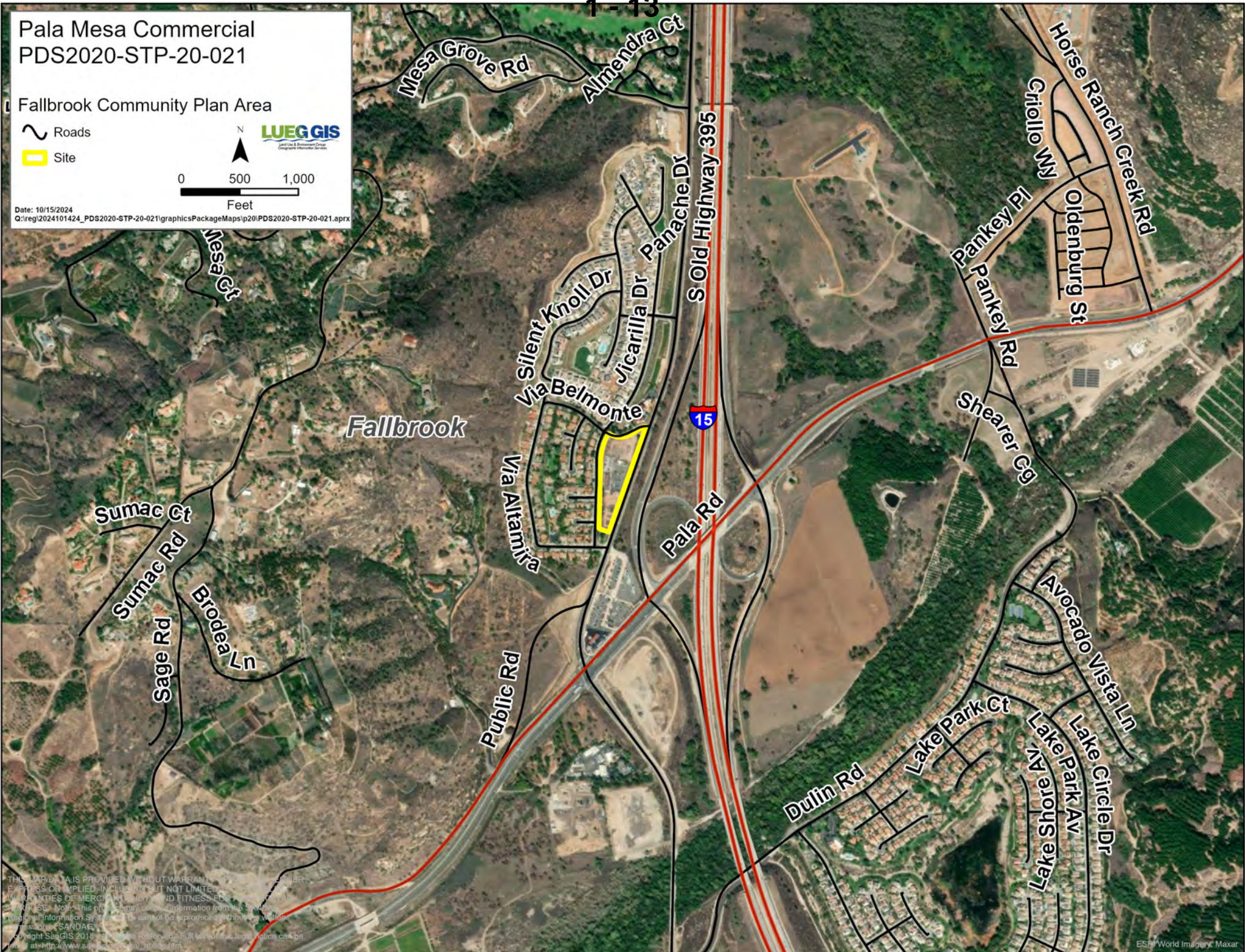
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Land Use & Environment Group

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Fallbrook

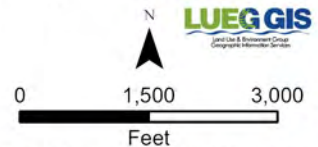
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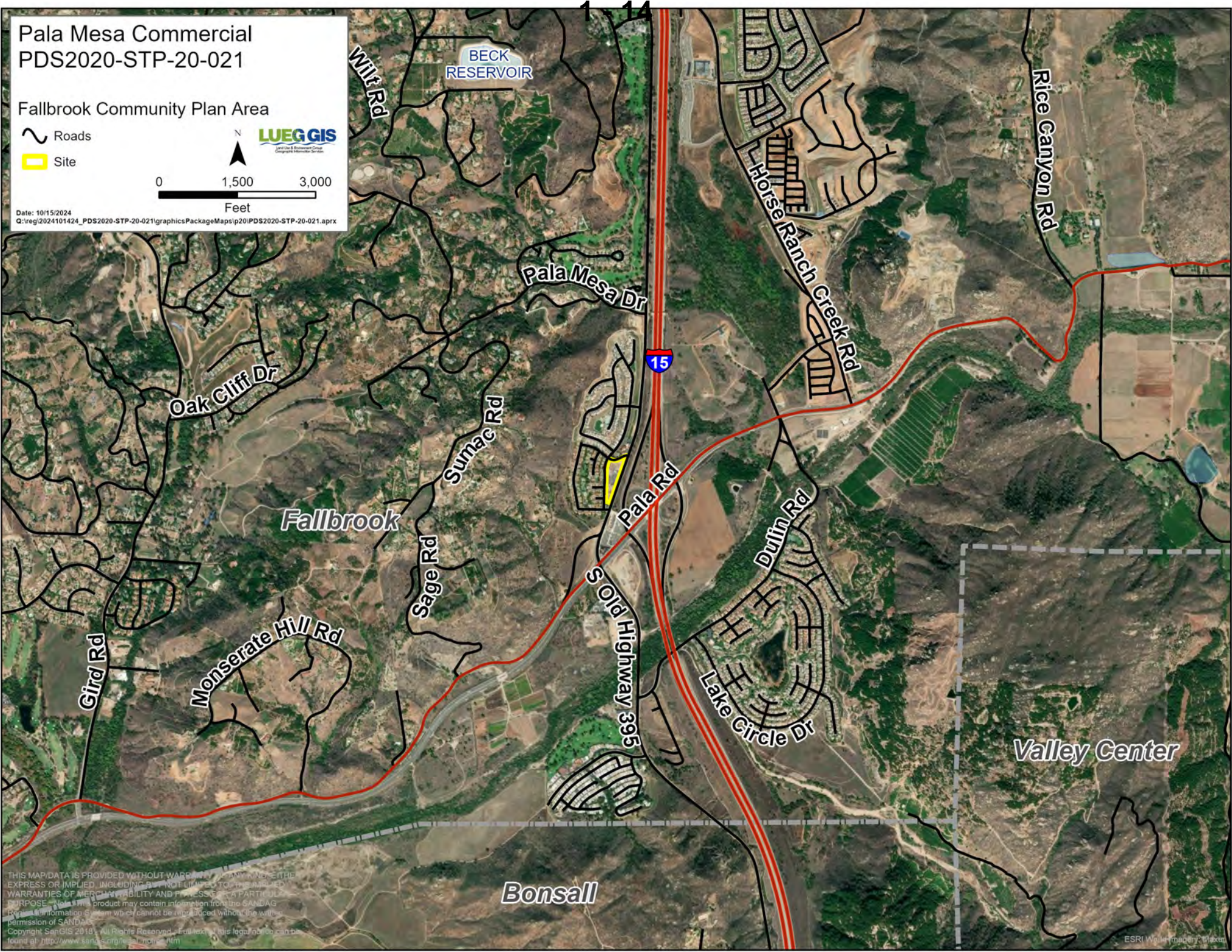
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Fallbrook Community Plan Area

- Roads
- Site



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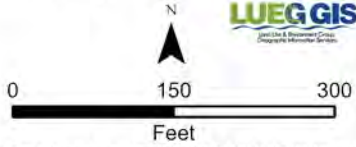
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Pala Mesa Commercial  
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Fallbrook Community Plan Area

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Silent Knoll Dr

Via Belmonte

Via Almonte

Via De Todos Santos

Fallbrook

S Old Highway 395

Via Serra

Via Tala

Via Altamira



Pala Rd

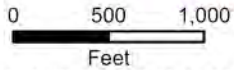
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Fallbrook Community Plan Area

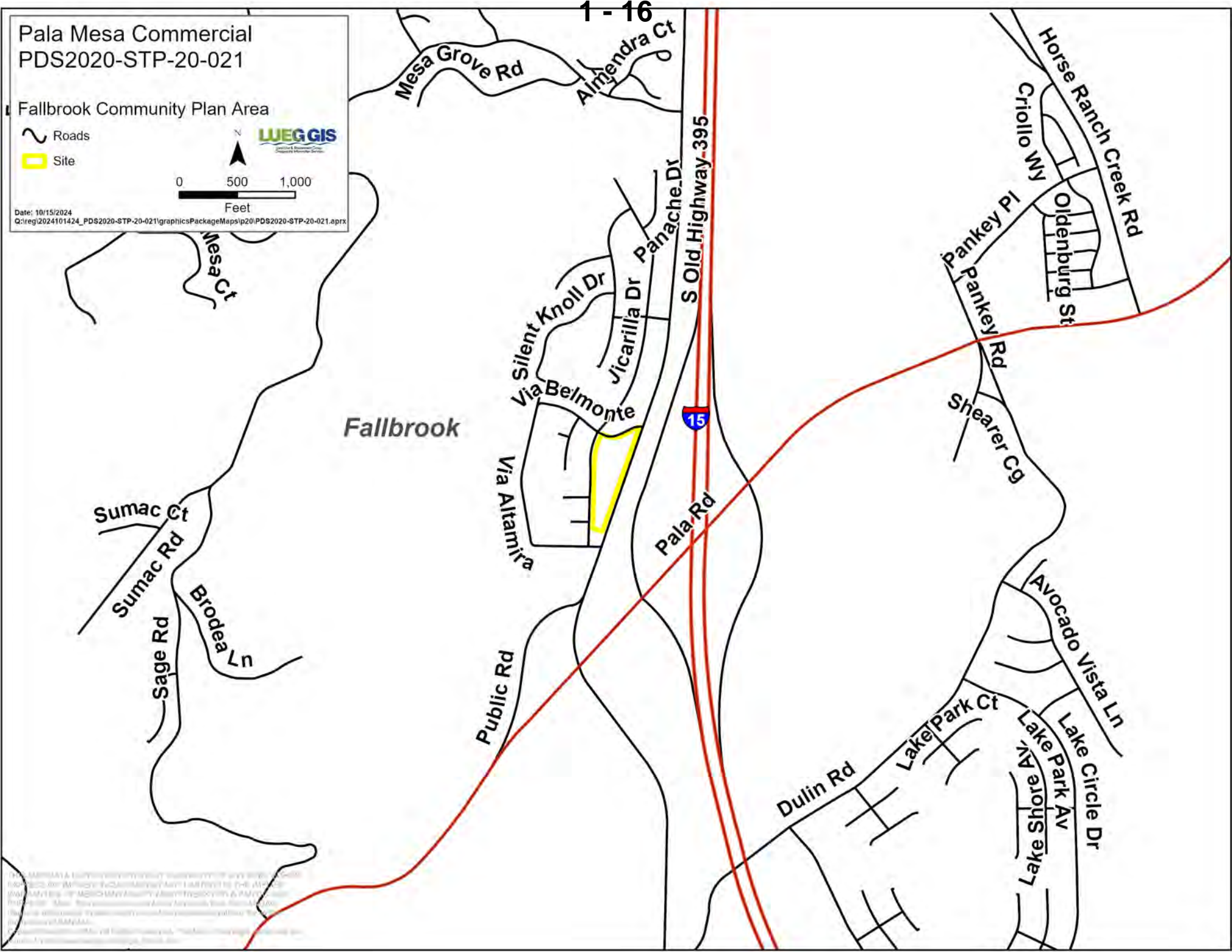
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# Pala Mesa Commercial PDS2020-STP-20-021

Fallbrook Community Plan Area

~ Roads  
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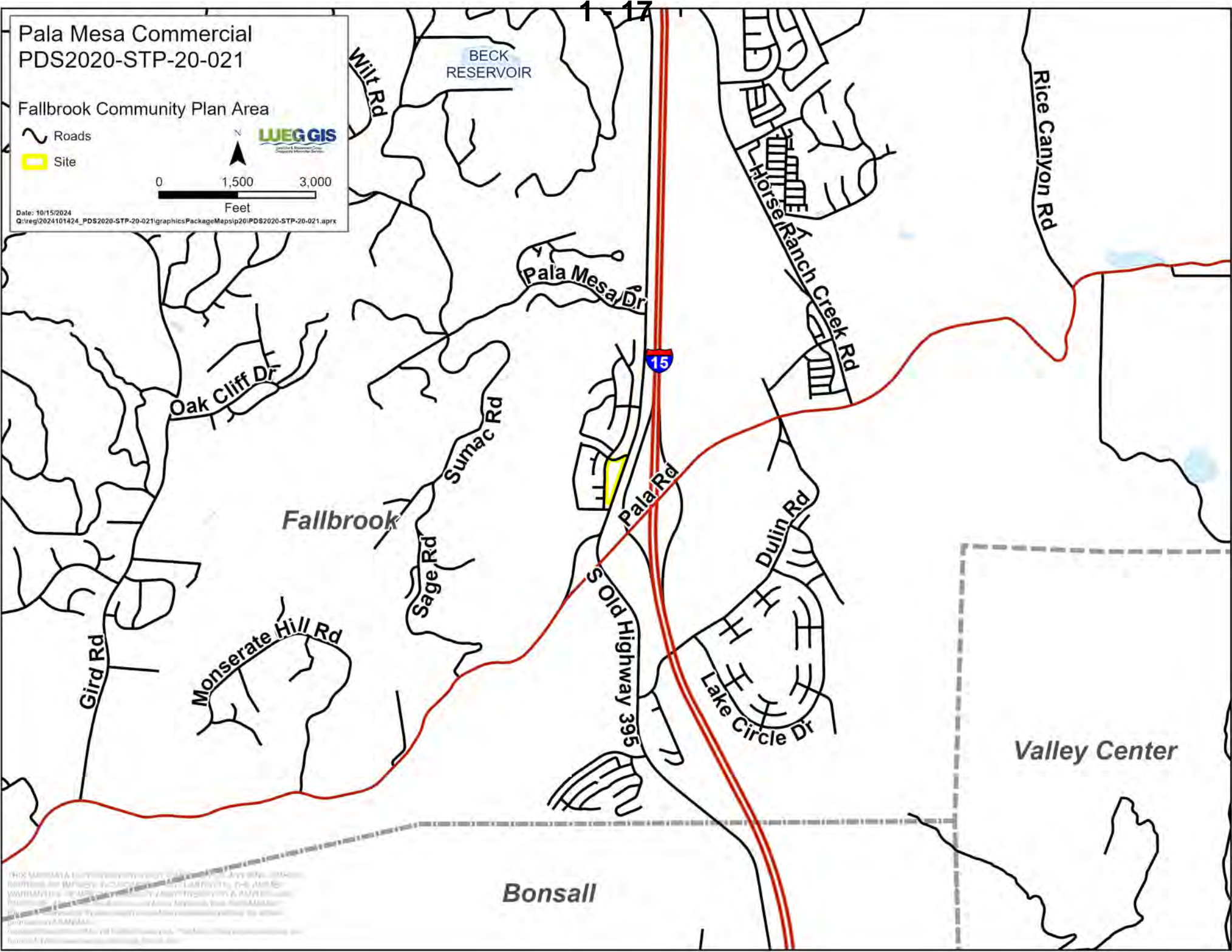


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Landscape & Watershed Engineering & GIS

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Feet

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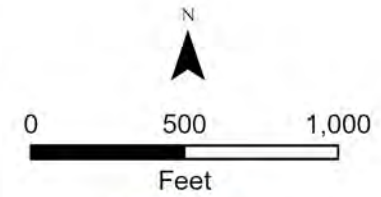
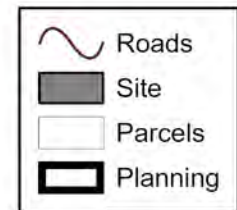
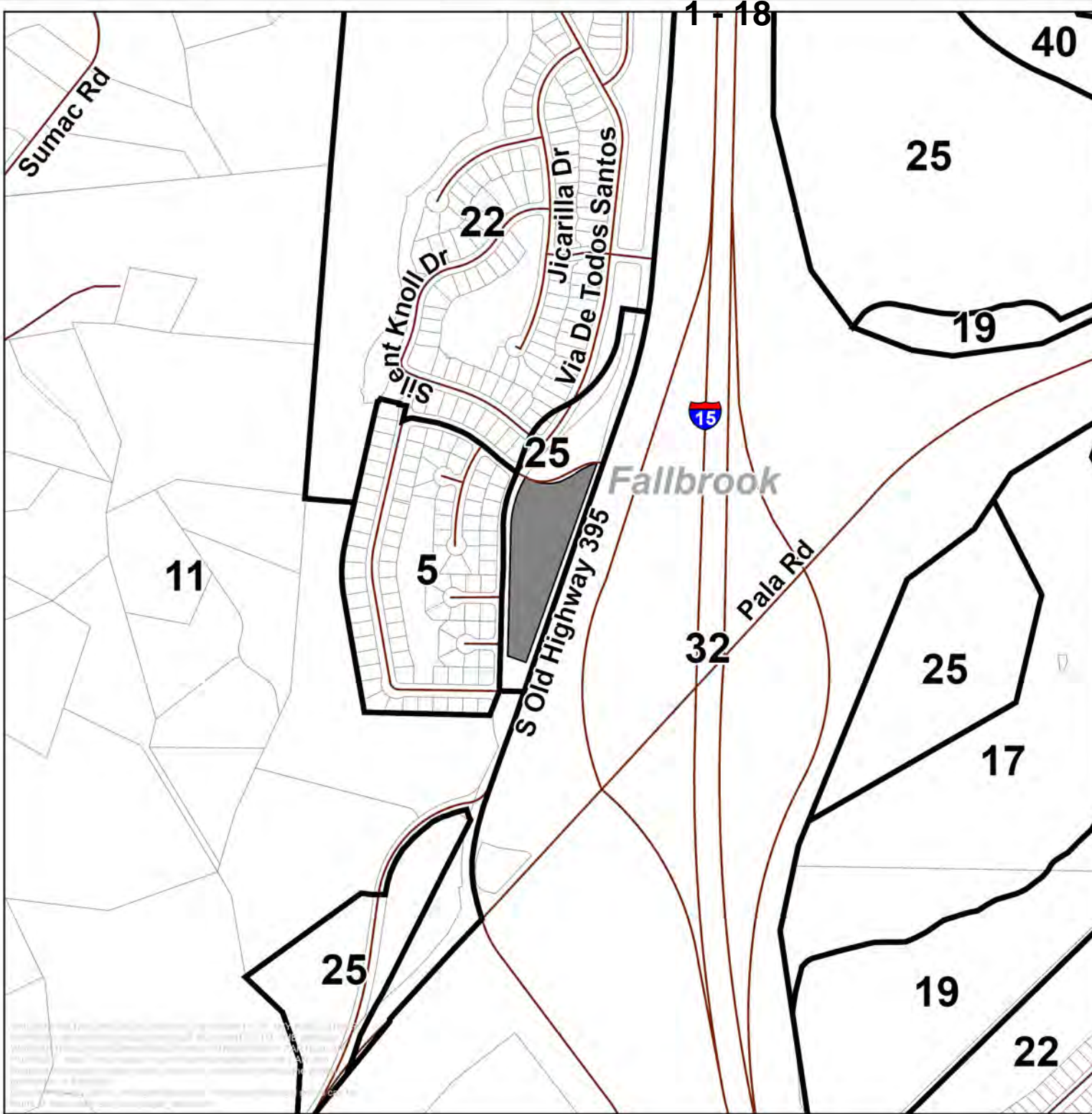


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Pala Mesa Commercial  
PDS2020-STP-20-021

General Plan

- Fallbrook*  
*Community Plan Area*
- (5) Village Residential (VR-7.3)
  - (11) Semi-Rural Residential (SR-2)
  - (17) Semi-Rural Residential (SR-10)
  - (19) Rural Lands (RL-40)
  - (22) Specific Plan Area
  - (25) General Commercial
  - (32) Public/Semi-Public Facilities
  - (40) Village Residential (VR-20)



# Pala Mesa Commercial PDS2020-STP-20-021

## Zoning

*Fallbrook*

*Community Plan Area*

A70 - Limited Agricultural

A72 - General Agricultural

C36 - General Commercial

C44 - Freeway Commercial

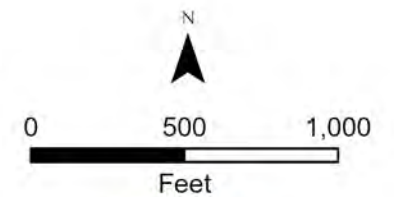
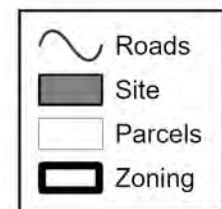
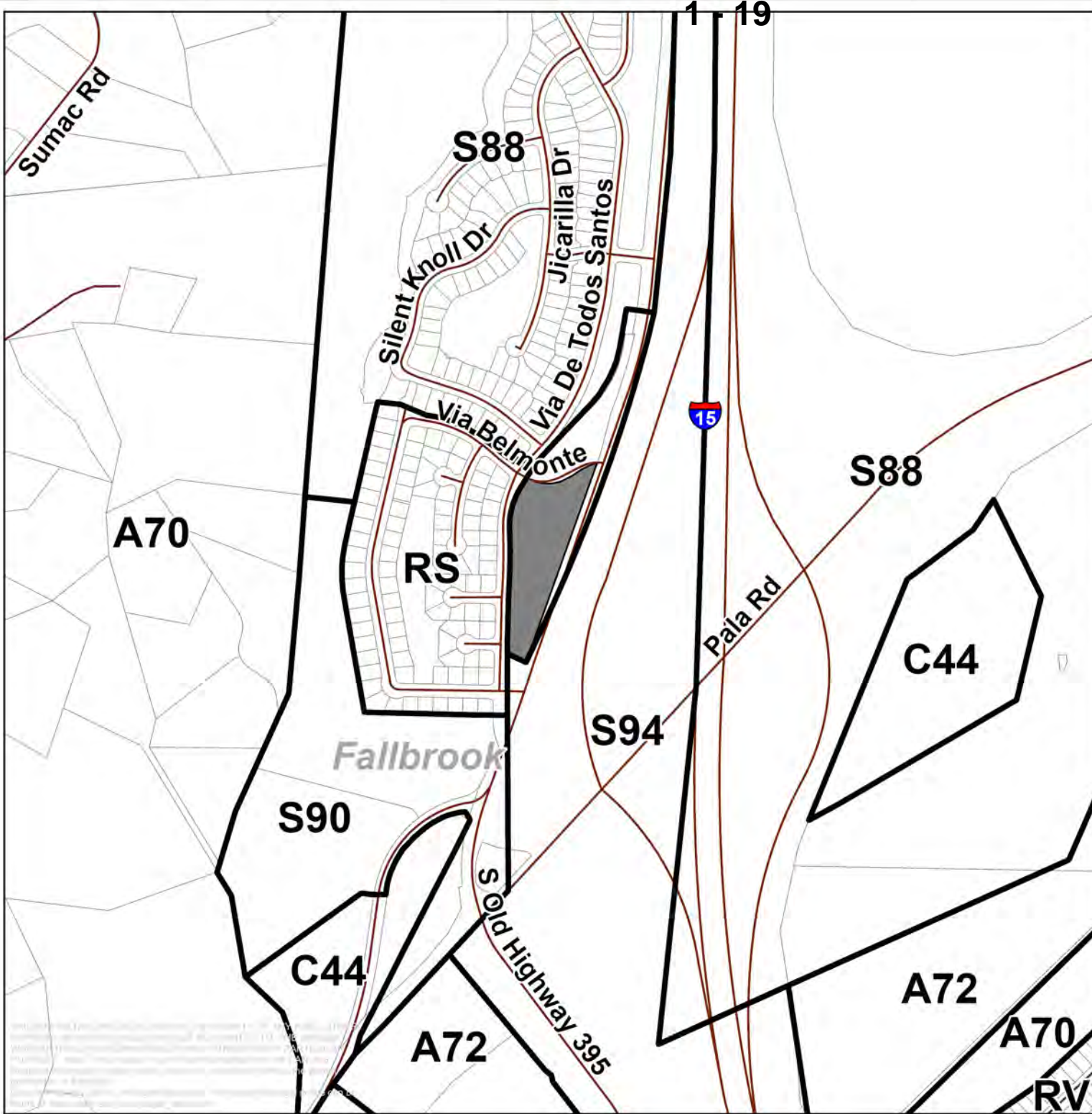
RS - Single Family Residential

RV - Variable Family Residential

S88 - Specific Planning Area

S90 - Holding Area

S94 - Transportation/Utility Corridor



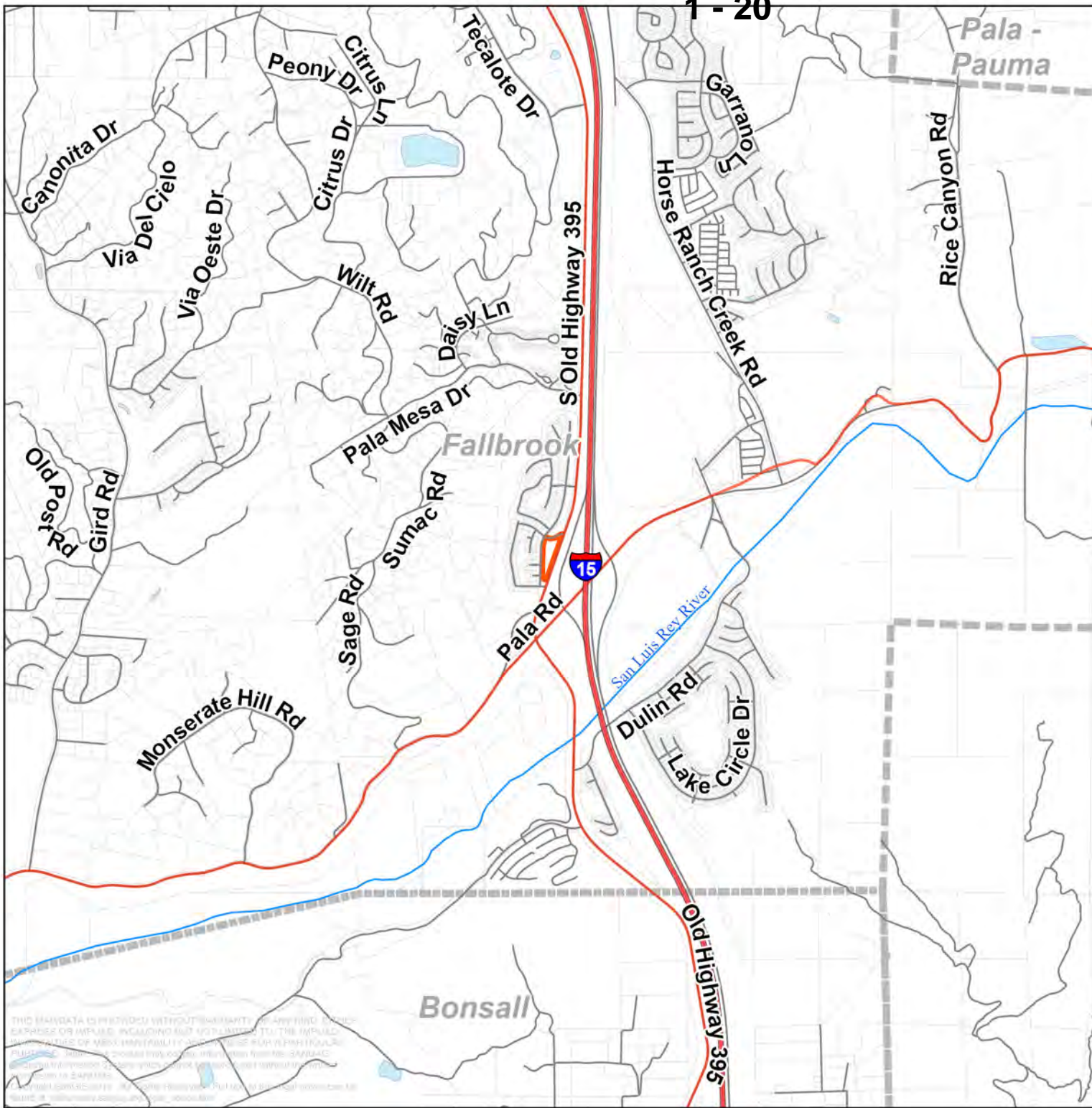
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Geographic Information Services

Vicinity Map

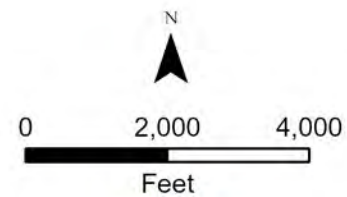
Fallbrook  
Community Plan Area



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	Roads
	Site
	Parcels



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## Attachment B – Environmental Documentation



VINCE NICOLETTI  
INTERIM DIRECTOR

**PLANNING & DEVELOPMENT SERVICES**  
5510 OVERLAND DRIVE, SUITE 210, SAN DIEGO, CALIFORNIA 92123  
(858) 505-6445 General • (858) 694-2705 Codes Compliance  
(858) 565-5920 Building Services

October 17, 2024

**Statement of Reasons for Exemption from  
Additional Environmental Review and 15183 Checklist  
Pursuant to CEQA Guidelines §15183**

**Project Name:** Pala Mesa Commercial  
**Project Record ID:** PDS2020-STP-20-021  
**Environmental Log No.** LOG NO. PDS2020-ER-20-02-003

**Lead Agency Name and Address:**  
County of San Diego  
Planning and Development Services  
5510 Overland Avenue, Suite 310  
San Diego, CA 92123

**County Staff Contact:**  
Sean Oberbauer, Project Manager  
Phone: (619) 323-5287  
Email: [sean.oberbauer@sdcounty.ca.gov](mailto:sean.oberbauer@sdcounty.ca.gov)

**Project Location:**  
3233 Old Highway 395, Fallbrook, CA 92028  
Fallbrook Community Planning Area  
Unincorporated County of San Diego  
Thomas Guide Coordinates: Page 1048, Grid H2  
APN: 125-050-54-00

**Project Applicant:**  
Rafid Hamika, 370 Bridgeton Ct. Las Vegas, NV 89148  
Phone: (702) 340-5116

**General Plan**  
Community Plan: Fallbrook Community Plan and I-15 Design Review Corridor  
Regional Category: Semi-Rural  
Land Use Designation: General Commercial (C-1)  
Density: General Commercial Density set by Zoning Density Designator  
Floor Area Ratio (FAR): 0.45/0.70

## Zoning

Use Regulation:	General Commercial (C36)
Animal Regulations:	Q – Various Allowances in accordance with the Animal Schedule
Density:	40 – 40 dwelling units per acre
Minimum Lot Size:	– – N/A
Building Type:	T – Triplex/Multi-dwelling or Mixed Residential/Nonresidential and Nonresidential Buildings
Setback:	O – 50-foot Front Yard, 0 foot Interior Side, 35 foot Exterior Side Yard, 25 foot Rear Yard (Not including footnotes as defined by the Zoning Ordinance)
Height:	G – 35-feet maximum, 2 stories
Open Space:	A – N/A
Special Area Designator:	B – Community Design Review

## Project Description

### Location:

The proposed project is located at 3233 Old Highway 395 in the Fallbrook Community Planning Area in the unincorporated County of San Diego. The project site is an approximately 4.36-acre parcel, APN: (125-050-54-00).

### Site Description:

The Site Plan is proposed on an approximately 4.36-acre property in order to authorize the construction and operation of a commercial plaza. The project site is subject to the Semi-Rural General Plan Regional Category, Land Use Designation General Commercial (C-1). The Zoning Use Regulation for the site is General Commercial (C36). The project site is located at 3233 Old Highway 395 in the Fallbrook Community Planning Area and in the I-15 Design Review Corridor in the unincorporated County of San Diego. The site contains an existing commercial plaza with vacant portions near the northern and southern ends.

### Discretionary Actions:

The project consists of the following action: Site Plan (STP). The STP would allow for the remodel and expansion of an existing commercial plaza on an approximately 4.36-acre property.

### Project Description:

The Project consists of the expansion and remodeling of an existing commercial plaza. The Project site contains an existing market with attached businesses consisting of restaurants and personal services such as financial and insurance companies. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. The existing parking lot will be expanded and re-designed to include a total of 134 parking spaces. The Project also includes proposed landscaping as well as signage for the commercial tenants of the site. Access to the site would be provided by two commercial driveways connecting to Old Highway 395. Water and Sewer service would be provided by the Rainbow Municipal Water District. The existing road on the northern portion of the Project site known as Via Belmonte will be widened with a half width improvement of 14 feet and a graded half width of twenty feet along the south side of Via Belmonte. The existing Project frontage along Old Highway 395 will be improved and include restriping of Old Highway 395 in order to accommodate a left-turn lane into the Project while retaining the two existing commercial driveway access points. The Project also includes a vacation and/or removal and re-dedication of road, drainage, and slope easements related to maintenance along Old



Highway 395. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export.

The project site is subject to the Semi-Rural Regional Category, Land Use Designation General Commercial (C-1). Zoning for the site is General Commercial (C36). The proposed uses are consistent with the Zoning and General Plan Land Use Designation of the property. The site is also subject a "B" Special Area Designator for community design review which requires the processing of a Site Plan permit.

## **Overview of 15183 Checklist**

California Public Resources Code section 21083.3 and California Environmental Quality Act (CEQA) Guidelines Section 15183 provides an environmental review process and exemption from additional environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an Environmental Impact Report (EIR) was certified, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that: (1) Are peculiar to the project or the parcel on which the project would be located, and were not analyzed as significant effects in a prior EIR on the zoning action, general plan, or community plan, with which the project is consistent, (2) Are potentially significant off-site impacts and cumulative impacts which were not discussed in the prior EIR prepared for the general plan, community plan or zoning action, or (3) Are previously identified significant effects which, as a result of substantial new information which was not known at the time the EIR was certified, are determined to have a more severe adverse impact than discussed in the prior EIR. Section 15183(c) further specifies that if an impact is not peculiar to the parcel or to the proposed project, has been addressed as a significant effect in the prior EIR, or can be substantially mitigated by the imposition of uniformly applied development policies or standards, then an additional EIR need not be prepared for that project solely on the basis of that impact.

## **General Plan Update Program EIR**

The County of San Diego General Plan Update (GPU) establishes a blueprint for future land development in the unincorporated County that meets community desires and balances the environmental protection goals with the need for housing, agriculture, infrastructure, and economic vitality. The GPU applies to all of the unincorporated portions of San Diego County and directs population growth and plans for infrastructure needs, development, and resource protection. The GPU included adoption of new General Plan elements, which set the goals and policies that guide future development. It also included a corresponding land use map, a County Road Network map, updates to Community and Subregional Plans, an Implementation Plan, and other implementing policies and ordinances. The GPU focuses population growth in the western areas of the County where infrastructure and services are available in order to reduce the potential for growth in the eastern areas. The objectives of this population distribution strategy are to: 1) facilitate efficient, orderly growth by containing development within areas potentially served by the San Diego County Water Authority (SDCWA) or other existing infrastructure; 2) protect natural resources through the reduction of population capacity in sensitive areas; and 3) retain or enhance the character of communities within the unincorporated County. The SDCWA service area covers approximately the western one third of the unincorporated County. The SDWCA boundary generally represents where water and wastewater infrastructure currently exist. This area is more developed than the eastern areas of the unincorporated County, and would accommodate more growth under the GPU.

The GPU EIR was certified in conjunction with adoption of the GPU on August 3, 2011. The GPU EIR comprehensively evaluated environmental impacts that would result from Plan implementation, including information related to existing site conditions, analyses of the types and magnitude of project-level and cumulative environmental impacts, and feasible mitigation measures that could reduce or avoid environmental impacts.

## Summary of Findings

The Project is consistent with the analysis performed for the GPU EIR. Further, the GPU EIR adequately anticipated and described the impacts of the proposed project, identified applicable mitigation measures necessary to reduce project specific impacts, and the project implements these mitigation measures (see [http://www.sdcounty.ca.gov/PDS/gpupdate/docs/BOS\\_Aug2011/EIR/FEIR\\_7.00 - Mitigation Measures 2011.pdf](http://www.sdcounty.ca.gov/PDS/gpupdate/docs/BOS_Aug2011/EIR/FEIR_7.00_-_Mitigation_Measures_2011.pdf) for complete list of GPU Mitigation Measures.

A comprehensive environmental evaluation has been completed for the project as documented in the attached §15183 Environmental Checklist. This evaluation concludes that the project qualifies for an conformance with CEQA in accordance with §15183 because it is consistent with the development density and use characteristics established by the County of San Diego General Plan, as analyzed by the San Diego County General Plan Update Final Program EIR (GPU EIR, ER #02-ZA-001, SCH #2002111067), and all required findings can be made.

The project qualifies for conformance with CEQA in accordance with CEQA Guidelines §15183 because the following findings can be made:

**1. The project is consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified.**

The proposed project consists of a commercial use and does not propose additional development density or residential uses that would be in conflict with the General Commercial (C-1) General Plan Land Use Designation or Semi-Rural Regional Category for which the GPU EIR was certified.

**2. There are no project specific effects which are peculiar to the project or its site, and which the GPU EIR Failed to analyze as significant effects.**

The subject property is no different than other properties in the surrounding area, and there are no project specific effects which are peculiar to the project or its site. The project site is located in an area adjacent to commercially zoned property along Old Highway 395. The property does not support any peculiar environmental features, and the project would not result in any peculiar effects.

In addition, as explained further in the 15183 Checklist below, all project impacts were adequately analyzed by the GPU EIR. The project could result in potentially significant impacts to Biological Resources, and Cultural Resources. However, applicable mitigation measures and project design features related to Cultural Resources as specified within the GPU EIR have been made conditions of approval for this project. Additionally, project design features consistent with recent State regulations as well as consistency with applicable ordinances, CEQA guidelines for determining significance, and Board Policies that were identified as mitigation measures within the GPU EIR associated with Transportation/Traffic, Air Quality, Greenhouse Gas Emissions, Noise, Wildfire, Hydrology/Water Quality, Public Services, and Land Use and Planning have been made conditions of approval for this project.

**3. There are no potentially significant off-site and/or cumulative impacts which the GPU EIR failed to evaluate.**

The proposed project is consistent with the use characteristics and limitations of the development considered by the GPU EIR through the application of a Site Plan and would represent a small part of the growth that was forecasted for build-out of the General Plan. The GPU EIR considered the incremental impacts of the proposed project, and as explained further in the 15183 Checklist below, no potentially significant off-site or cumulative impacts have been identified which were not previously evaluated.

**4. There is no substantial new information which results in more severe impacts than anticipated by the GPU EIR.**

As explained in the 15183 checklist below, no new information has been identified which would result in a determination of a more severe impact than what had been anticipated by the GPU EIR.

**5. The project will undertake feasible mitigation measures specified in the GPU EIR.**

As explained in the 15183 checklist below, the project will undertake feasible mitigation measures specified in the GPU EIR. These GPU EIR mitigation measures will be undertaken through project design, compliance with regulations and ordinances, or through the project's conditions of approval.

	<b>October 17, 2024</b>
Signature	Date
<b>Sean Oberbauer</b>	<b>Project Manager</b>
Printed Name	Title

## CEQA Guidelines §15183 Environmental Checklist

### Overview

This checklist provides an analysis of potential environmental impacts resulting from the proposed project. Following the format of CEQA Guidelines Appendix G, environmental effects are evaluated to determine if the project would result in a potentially significant impact triggering additional review under Guidelines section 15183.

- Items checked “Significant Project Impact” indicates that the project could result in a significant effect which either requires mitigation to be reduced to a less than significant level or which has a significant, unmitigated impact.
- Items checked “Impact not identified by GPU EIR” indicates the project would result in a project specific significant impact (peculiar off-site or cumulative that was not identified in the GPU EIR.
- Items checked “Substantial New Information” indicates that there is new information which leads to a determination that a project impact is more severe than what had been anticipated by the GPU EIR.

A project does not qualify for conformance with CEQA in accordance with §15183 if it is determined that it would result in: 1) a peculiar impact that was not identified as a significant impact under the GPU EIR; 2) a more severe impact due to new information; or 3) a potentially significant off-site impact or cumulative impact not discussed in the GPU EIR.

A summary of staff’s analysis of each potential environmental effect is provided below the checklist for each subject area. A list of references, significance guidelines, and technical studies used to support the analysis is attached in Appendix A. Appendix B contains a list of GPU EIR mitigation measures.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**1. Aesthetics** – Would the Project:

- |  |                          |                          |                          |
|--|--------------------------|--------------------------|--------------------------|
| a) Have a substantial adverse effect on a scenic vista?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Substantially degrade the existing visual character or quality of the site and its surroundings?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d) Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?                                   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

1(a) The GPU EIR concluded this impact to be less than significant with mitigation. A vista is a view from a particular location or composite views along a roadway or trail. Scenic vistas often refer to views of natural lands but may also be compositions of natural and developed areas, or even entirely of developed and unnatural areas, such as a scenic vista of a rural town and surrounding agricultural lands. What is scenic to one person may not be scenic to another, so the assessment of what constitutes a scenic vista must consider the perceptions of a variety of viewer groups.

The items that can be seen within a vista are visual resources. Adverse impacts to individual visual resources or the addition of structures or developed areas may or may not adversely affect the vista. Determining the level of impact to a scenic vista requires analyzing the changes to the vista as a whole and also to individual visual resources.

As described in the General Plan Update Environmental Impact Report (GPU EIR; County of San Diego 2011), the County contains visual resources affording opportunities for scenic vistas in every community. Resource Conservation Areas (RCAs) are identified within the GPU EIR and are the closest that the County comes to specifically designating scenic vistas. Many public roads in the County currently have views of RCAs or expanses of natural resources that would have the potential to be considered scenic vistas. Numerous public trails are also available throughout the County. New development can often have the potential to obstruct, interrupt, or detract from a scenic vista.

Approximately eleven Resource Conservation Areas (RCAs) have been identified by the County in the Fallbrook Community Plan. The eleven RCAs are primarily biological resources and are located more than a half of a mile away from the site. The Project site contains an existing market with attached businesses consisting of restaurants and personal services such as financial and insurance companies. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. The existing parking lot will be expanded and re-designed to include a total of 134 parking spaces. The Project also includes proposed landscaping

as well as signage for the commercial tenants of the site. The project will not impact or detract from views of RCAs identified in the Fallbrook Community Plan Area due to intervening topography and structures as well as the overall distance of the project site from the RCAs. Additionally, the Project site is previously developed and contains existing commercial structures. The additional commercial construction is an expected visual feature in the community.

Scenic vistas are also in the project vicinity as scenic highways as the project is located adjacent to Interstate 15, a Scenic Highway identified in the General Plan. The Project is also located within the boundaries of the I-15 Design Review Corridor and within the Fallbrook Community Plan on a site subject to a "B" Special Area Designator which requires the processing of a Site Plan permit in order to demonstrate conformance with the Fallbrook Community Plan and I-15 Design Guidelines. Plot plans including a site design and layout, architecture elevations, conceptual signage, and conceptual landscaping have been submitted as part of the Site Plan application process. Therefore, the Project will not have a substantial adverse effect on a scenic vista.

- 1(b) The GPU EIR concluded this impact to be less than significant with mitigation. State scenic highways refer to those highways that are officially designated by the California Department of Transportation (Caltrans) as scenic (Caltrans - California Scenic Highway Program). Generally, the area defined within a State Scenic Highway is the land adjacent to and visible from the vehicular right-of-way. The dimension of a scenic highway is usually identified using a motorist's line of vision, but a reasonable boundary is selected when the view extends to the distant horizon. The scenic highway corridor extends to the visual limits of the landscape abutting the scenic highway.

The Project site is adjacent to a portion of Interstate 15 that is listed as eligible but not officially designated by Caltrans as a State Scenic Highway. No officially designated State Scenic Highways designated by Caltrans are in proximity to the Project site. However, the Project site is within the boundaries of the Interstate 15 Design Review Corridor. Additionally, the County General Plan identifies roadways that are designated as scenic corridors within the Conservation and Open Space Element and have been included as part of the County Scenic Highway System. Interstate 15 is identified as a Scenic Highway in the County of San Diego General Plan. The proposed Project consists of remodeling and expanding an existing commercial plaza. The Project site contains an existing market structure with attached businesses as well as a burger restaurant. The market structure was constructed in the late 1970s and the Project site has contained commercial uses since the 1970s. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. The existing parking lot will be expanded and re-designed to include a total of 134 parking spaces. The Project also includes proposed landscaping as well as signage for the commercial tenants of the site.

The Project site is subject to the "G" Height Designator in the Zoning Ordinance which requires structures to be a maximum height of 35-feet. The remodel of the market will result in the structure to reach a maximum height of 26 feet. The gas station convenience store, retail store structures, and gas station canopy are proposed to be approximately 27 to 32 feet in height. All structures are compliant with the 35-foot "G" Height Designator of

the property. The existing visual character of views along Old Highway 395 and Interstate 15 consist of residences along hillsides, vacant land, and incidental non-residential uses such as commercial structures. The Project consists of redeveloping an existing commercial plaza which contains structures that have been on the subject property for over 40 years. The commercial structures are an expected visual feature within the community. The remodeled architectural design of the market is intended to be in conformance with the proposed new structures and uses of the property. The architectural design of the structures includes earth-tone colors as well as natural materials such as stone veneers. The structures also include features such as columns, landscape screens and shade covers in order to break up the verticality of the structures. Grading associated with the Project primarily consists of construction and installation of retaining walls in fill slopes in the rear of the property. The location of the retaining walls in the rear of the property as well as changes in elevations and existing vegetation along adjacent roadways will assist in screening the retaining walls from nearby views. Additional analysis regarding impacts to scenic resources and visual character can be found in response 1(a) and 1(c). As the proposed Project would have a less than significant impact for the reasons detailed above and response 1(a), the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 1(c) The GPU EIR concluded this impact to be significant and unavoidable. Visual character is the objective composition of the visible landscape within a viewshed. Visual character is based on the organization of the pattern elements line, form, color, and texture. Visual character is commonly discussed in terms of dominance, scale, diversity and continuity. Visual quality is the viewer's perception of the visual environment and varies based on exposure, sensitivity and expectation of the viewers.

The project would be consistent with existing visual character of the project site and views within the community. The proposed project consists of the redevelopment of a commercial plaza and will not substantially alter landform steep slopes. Refer to response 1(a) and 1(b) for additional discussions regarding impacts to the existing visual character of the project site and vicinity. The project as designed will not substantially degrade the existing visual character or quality of the site and its surroundings.

As previously discussed, the GPU EIR determined impacts on visual character or quality to be significant and unavoidable. However, the Project would have a less than significant impact with no required mitigation for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 1(d) The GPU EIR concluded this impact to be significant and unavoidable. Commercial lighting would be required to conform with the County's Light Pollution Code to prevent spillover onto adjacent properties and minimize impacts to dark skies. The project has been conditioned to ensure conformance with the County's Lighting Code during any processing of a building permit for the project. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties. Lighting for the signage and architectural features of the project have been designed to be fully shielded and externally illuminated such as the monument sign for the Project. Therefore, the project will not create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area

As previously discussed, the GPU EIR determined impacts from light or glare to be significant and unavoidable. However, the project would have a less than significant impact with no required mitigation for the reasons detailed above. Therefore, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Aesthetics, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
---	---	--

**2. Agriculture/Forestry Resources** – Would the Project:

- |  |                          |                          |                          |
|--|--------------------------|--------------------------|--------------------------|
| a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, or other agricultural resources, to a non-agricultural use? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Conflict with existing zoning for, or cause rezoning of, forest land, timberland, or timberland zoned Timberland Production?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d) Result in the loss of forest land, conversion of forest land to non-forest use, or involve other changes in the existing environment, which, due to their location or nature, could result in conversion of forest land to non-forest use?                                  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| e) Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Important Farmland or other agricultural resources, to non-agricultural use?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

2(a) The GPU EIR concluded this impact to be significant and unavoidable. The Project site contains lands designated as prime soils but not as Farmland of Local Importance



according to the Farmland Mapping and Monitoring Program (FMMP). However, the majority of the project site has been graded and disturbed since the mid-1970s. The existing market on the Project site was constructed in in the late 1970s. The soils have been historically disturbed and compacted as well as modified over the last 40 or more years. Additionally, the Project site does not contain 10 acres or contiguous Prime Farmland or Statewide Importance Soils as defined by the FMMP. Therefore, the site would not be considered to be a viable agricultural resource and no potentially significant project or cumulative level conversion of Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance to a non-agricultural use would occur as a result of this Project. Therefore, no potentially significant impact or conversion of Prime Farmland, Unique Farmland, or Farmland of Statewide or Local Importance to a non-agricultural use would occur as a result of this project. As previously discussed, the GPU EIR determined impacts from direct and indirect conversion of agricultural resources to be significant and unavoidable. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 2(b) The GPU EIR concluded this impact to be less than significant with mitigation. The Project site is subject to the C36 General Commercial Zoning Use Regulation, a commercial zone. The nearest land under Williamson Act Contract is located over two miles east of the Project site and the nearest land designated in an agricultural preserve is located over a mile east of the Project site. The Project site and nearest lands subject to a Williamson Act Contract or Agricultural Preserve are separate by major roadways and topography such as Interstate 15. Due to distance, no land-use interface conflicts would occur. Additionally, the Project is for the development of a commercial plaza with all uses being permitted within the C36 zone and is adjacent to properties subject to commercial, residential, and transit corridor zones. Therefore, the Project would not conflict with existing zoning for agricultural use or a Williamson Act Contract.

As previously discussed, the GPU EIR determined impacts from land use conflicts to be less than significant with mitigation. As the proposed Project would have a less than significant impact for the reasons detailed above, the Project would be consistent with the analysis provided in the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 2(c) The GPU EIR concluded this impact to be significant and unavoidable. The project site including any offsite improvements do not contain any forest lands as defined in Public Resources Code section 12220(g), therefore project implementation would not result in the loss or conversion of forest land to a non-forest use. The outer edge of the Cleveland National Forest is located over 40 miles southeast of the project site. Thus, due to distance, the Project would have no impact on the Forest. In addition, the County of San Diego does not have any existing Timberland Production Zones.

As previously discussed, the GPU EIR determined impacts from direct and indirect conversion of agricultural resources (including forest resources), to be significant and unavoidable. However, the project would have a less than significant impact to forest resources. Therefore, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 2(d) The GPU EIR concluded this impact to be significant and unavoidable. As indicated in response 2(c), the Project site, or any off-site improvements, are not located near any

forest lands. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 2(e) The GPU EIR concluded this impact to be significant and unavoidable. No agricultural operations are currently taking place on the Project site. In addition, no impacts would occur in association with interface conflicts. Please refer to response 2(a) and 2(b) for a discussion on off-site agricultural resources and interface conflicts.

As previously discussed, the GPU EIR determined impacts from direct and indirect conversion of agricultural resources (including forest resources) to be significant and unavoidable. However, the Project would have a less than significant impact to agricultural resources. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Agriculture/Forestry Resources, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

	Significant Project Impact	Impact not identified by GPU EIR	Substantial New Information
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**3. Air Quality** – Would the Project:

a) Conflict with or obstruct implementation of the San Diego Regional Air Quality Strategy (RAQS) or applicable portions of the State Implementation Plan (SIP)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

- e) Create objectionable odors affecting a substantial number of people?

**Discussion**

3(a) The GPU EIR concluded this impact to be less than significant. The Regional Air Quality Standard (RAQS) and State Implementation Plan (SIP) are based on General Plans within the region and the development assumptions contained within them. The project is for the remodel and expansion of a commercial plaza on an approximately 4.36-acre property and is consistent with the land use designation and use regulations allowed on the project site in accordance with the County General Plan and Zoning Ordinance. The proposed project is subject to the Semi-Rural Regional Category and General Commercial (C-1) Land Use Designation. The property is subject to the General Commercial Land Use Designation (C36) which permits the proposed uses of the site in accordance with Sections 2360 through 2363 of the Zoning Ordinance. The site is also subject a "B" Special Area Designator for community design review which requires the processing of a Site Plan permit. The proposed use is consistent with the General Plan Designation and the Zoning for the site and a General Plan Amendment or Zoning Reclassification is not required for the project. Because the proposed Project is allowed under the General Plan land use designation, which is used in San Diego Association of Governments' (SANDAG's) growth projections, it is consistent with San Diego County Air Pollution Control District's (SDAPCD's) Regional Air Quality Strategy (RAQS) and portions of the State Implementation Plan (SIP). The project is conditioned for the installation of electrical vehicle parking spaces consistent with CALGreen Tier 2 Standards which is consistent with measure Air-2.1 in the General Plan EIR. In addition, the construction and operational emissions from the Project would be below established screening-level thresholds (SLTs), as addressed under 3(b) below, and would not violate any ambient air quality standards. As such, the project would not conflict with either the RAQS or the SIP. Therefore, the project was anticipated in RAQS and SIP and would not conflict or obstruct implementation of these plans.

As previously discussed, the GPU EIR determined impacts on air quality plans to be less than significant. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

3(b) The GPU EIR concluded impacts to be significant and unavoidable. In general, air quality impacts from land use projects are the result of emissions from area sources (landscaping and consumer products), energy (natural gas), transportation (on-road mobile sources), and short-term construction activities. The County of San Diego (County) has identified significance SLTs which incorporate SDAPCD's established air quality impact analysis trigger levels for all new source review (NSR) in SDAPCD Rule 20.2 and Rule 20.3. These SLTs identified in the County Guidelines can be used as numeric methods to demonstrate that a project's total emissions (e.g., stationary and fugitive emissions, as well as emissions from mobile sources) would not result in a significant impact to air quality. SLTs for volatile organic compounds (VOCs) are based on the threshold of significance for VOCs from the South Coast Air Quality Management District (SCAQMD) for the Coachella Valley (which is more appropriate for the San Diego Air Basin). The County's SLTs and SDAPCD's trigger levels were developed in support of State and federal ambient air quality standards that are protective of human health.

Construction:

The air quality emissions for construction of the Project are evaluated in an Air Quality Analysis by Ldn Consulting, Inc. dated July 2023. The Air Quality Analysis concluded that emissions generated during construction activities and the operation of the project would not exceed San Diego County screening level thresholds for VOCs, NO<sub>x</sub>, CO, SO<sub>x</sub>, PM<sub>10</sub>, or PM<sub>2.5</sub> upon implementation of measures. Emissions during construction activities of the project would be temporary and localized. The existing market and attached businesses were constructed in the late 1970s. Due to the age of the structures, the Project will be conditioned to conduct lead and asbestos surveys prior to remodeling and construction on the market and attached structures. In the event that lead and asbestos are found in the existing structures, construction associated with the building permit of the market renovation will require lead and asbestos treatment in conformance with the Air Pollution and Control District regulations. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export. Grading associated with the Project primarily consists of construction and installation of retaining walls in fill slopes in the rear of the property. Construction activities would be subject to SDAPCD Rule 55 to reduce fugitive dust and the project is conditioned to implement dust control measures to reduce fugitive dust. For example, the Project is required to implement measures such as the use of water trucks in order to reduce fugitive dust. Additionally, the Project will be conditioned to use Tier 4 construction equipment which requires the use of specific vehicles and machinery designed to meet standards set by the United States Environmental Protection Agency and California Air Resources Board (CARB) that require use of engines and equipment that meet criteria to limit emissions.

#### Operations:

The air quality emissions for operations of the Project are evaluated in an Air Quality Analysis by Ldn Consulting, Inc. dated July 2023. The Air Quality Analysis concluded that the operations of the project would not exceed any standards or conflict with any applicable plan. The Project site contains an existing market with attached businesses consisting of restaurants and personal services such as financial and insurance companies. The Project site is not located adjacent to a school but is adjacent to residential uses west of the Project site. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. Potential sources of criteria pollutants for screening level thresholds for VOCs, NO<sub>x</sub>, CO, SO<sub>x</sub>, PM<sub>10</sub>, or PM<sub>2.5</sub> associated with operations of the Project consist of accumulation of particulates due to traffic and vapors from fueling pump stations. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along I-15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near I-15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. The Project includes the installation of vapor recovery systems in accordance with the San Diego Air Pollution Control District requirements which are designed to minimize emissions from the operations of a gas station. Fuels associated with the operation of the gas station must be permitted through the Department of Environmental Health Hazardous Materials Division through a hazardous materials business plan and permits for underground storage tanks.

Underground storage tanks require maintenance and inspections in order to ensure that no leaks of fuel product will result in exposing any potential sensitive receptors to pollutants or leaking of product into the soil on the project site. As detailed in the Air Quality analysis, the emissions associated with operations of the Project would not exceed standards or screening level thresholds including the traffic generated by the Project and accumulation of vehicles on the Project site. Lastly, the Project includes a condition for consistency with CALGreen Tier 2 standards for off-street vehicle requirements.

The emissions generated during construction activities and the operation of the project would not exceed San Diego County screening level thresholds for VOCs, NO<sub>x</sub>, CO, SO<sub>x</sub>, PM<sub>10</sub>, or PM<sub>2.5</sub>. Lastly, the project is consistent with measures Air-2.5, 2.6, and 2.7 outlined in the General Plan EIR which require projects to be consistent with APCD requirements, implementation of dust control measures, and the use of the County Guidelines of Determining Significance for Air Quality. Therefore, the project's regional air quality impacts would be less than significant through the implementation of measures as detailed in the General Plan EIR.

- 3(c) The GPU EIR concluded this impact to be significant and unavoidable. The project would contribute PM<sub>10</sub>, NO<sub>x</sub>, and VOCs emissions from construction/grading activities; however, the incremental increase would not exceed established screening thresholds upon implementation of measures (see response 3(b)).

As previously discussed, the GPU EIR determined significant and unavoidable impacts to non-attainment criteria pollutants. However, the project would have a less than significant impact to non-attainment criteria pollutants for the reasons stated above. Therefore, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 3(d) The GPU EIR concluded this impact to be significant and unavoidable. The Project consists of the remodeling and expansion of an existing commercial plaza. An Air Quality Analysis prepared for the Project evaluated potential impacts associated with emissions of pollutants associated with the Project and demonstrates that the Project will not expose sensitive receptors to substantial criteria pollutants beyond standards. Further information can be found in response 3(b) regarding potential emissions of criteria pollutants.

As previously discussed, the GPU EIR determined significant and unavoidable impacts to sensitive receptors. However, the Project would have a less than significant impact to sensitive receptors. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 3(e) The GPU EIR determined less than significant impacts from objectionable odors. The project could produce objectionable odors during construction; however, these substances, if present at all, would only be in trace amounts and would not be distinguishable due to the location of the project adjacent to Old Highway 395 and Interstate 15. Land uses and industrial operations typically associated with odor complaints include agricultural uses, wastewater treatment plants, food processing plants, chemical plants, refineries, landfills, dairies, and fiberglass molding. Potential objectionable odors associated with operations of the Project could be a result of spilled gasoline from fueling stations. However, as detailed in Section 10 Hydrology and Water Quality, the Project is required to implement Best Management Practices (BMPs) which minimize and reduce the amount of accumulation of gasoline on surfaces and require the fueling station area to be routinely cleaned and maintained. Additionally, the fueling

stations are required to comply with the San Diego Air Pollution Control District requirements of installing vapor recovery systems which minimize objectionable odors. Lastly, the project site is adjacent to residential uses as well as Interstate 15 and Old Highway 395, and it is unlikely that the odors from the project would be distinguishable from existing sources given the vehicle emissions associated with adjacent roadways in the vicinity of the project site. The project is also required to comply with SDAPCD Rule 51, public nuisance, which would require the limiting of objectionable odors to be emitted from the site. Therefore, the project would not create objectionable odors affecting a substantial number of people and the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

As previously discussed, the GPU EIR determined less than significant impacts from objectionable odors. As the Project would have a less than significant impact from objectionable odors for the reasons stated above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

The project could result in potentially significant impacts to Air Quality; however, further environmental analysis is not required because:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. Feasible mitigation measures contained within the GPU EIR will be applied to the project. Air-2.1, Air-2.5, Air-2.6, Air-2.7

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**4. Biological Resources – Would the Project:**

- |  |                          |                          |                          |
|--|--------------------------|--------------------------|--------------------------|
| a) Have a substantial adverse effect, either directly or through habitat modifications, on any candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or US Fish and Wildlife Service?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

e) Conflict with the provisions of any adopted Habitat Conservation Plan, Natural Communities Conservation Plan, other approved local, regional or state habitat conservation plan or any other local policies or ordinances that protect biological resources?

**Discussion**

4(a) The GPU EIR concluded this impact to be significant and unavoidable. Biological resources on the project site were evaluated through a site visit by County staff as well as review of permit history and historic aerials. The Project site has been disturbed and graded since the mid-1970s and the existing market was constructed in the late 1970s. The Nussy Burger restaurant was remodeled in the mid-2010s. The Project site is also surrounded by development including Interstate 15 and Old Highway 395 as well as the Pala Mesa Highlands residential development. As a result of previous development and permits, the Project site has been found to be considered urban/developed and does not support habitat or sensitive species that would require mitigation as a result of impacts from the Project.

As previously discussed, the GPU EIR determined impacts to sensitive habitat and/or species to be significant and unavoidable. However, the Project impacts were determined to be less than significant for the reasons detailed above. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

4(b) The GPU EIR concluded this impact to be significant and unavoidable. As detailed in response a) the Project site has been previously disturbed and developed since the 1970s. As a result, the Project site does not support riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or US Fish and Wildlife Service.

As previously discussed, the GPU EIR determined impacts to riparian habitat or other sensitive natural community to be significant and unavoidable. However, the Project impacts were determined to be less than significant for the reasons detailed above. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

4(c) The GPU EIR concluded this impact to be less than significant with mitigation. The proposed project site does not contain any jurisdictional water features or wetlands as defined by Section 404 of the Clean Water Act; therefore, no impacts will occur. As previously discussed, the GPU EIR determined impacts to federally protected wetlands as significant with mitigation. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 4(d) The GPU EIR concluded this impact to be significant and unavoidable. As detailed in response (a), Biological Resources were evaluated through a site visit by County staff as well as review of previous permits and historical aeriels. it was determined that the site is not part of a regional linkage/corridor nor is it in an area considered regionally important for wildlife dispersal. The site would not assist in local wildlife movement as it lacks connecting vegetation and visual continuity with other potential habitat areas in the general project vicinity. The Project site is also surrounded by development including Interstate 15 and Old Highway 395 as well as the Pala Mesa Highlands residential development. The project site is also not located within a pre-approved mitigation area within an adopted MSCP Subarea.

As previously discussed, the GPU EIR determined impacts to wildlife movement corridors as significant and unavoidable. However, the Project impacts were determined to be less than significant for the reasons detailed above. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 4(e) The GPU EIR concluded this impact to be less than significant. The project is located within the draft North County Multiple Species Conservation Program (NCMSCP) and outside of the adopted South County MSCP. Therefore, it does not require conformance with the Biological Mitigation Ordinance (BMO). The project is consistent with the County's Guidelines for Determining Significance for Biological Resources, the Resource Protection Ordinance (RPO), and the Migratory Bird Treaty Act (MBTA) as the site has been previously developed and disturbed as described in response 4(a). The Project will include a standard notice regarding breeding season avoidance in the event that nesting birds are found on the property prior to construction in order to comply with the MBTA. The Project will not conflict with the provisions of any adopted Habitat Conservation Plan, Natural Communities Conservation Plan, other approved local, regional, or state habitat conservation plan or any other local policies or ordinances that protect biological resources. Therefore, the project will not conflict with the provisions of any adopted Habitat Conservation Plan, Natural Communities Conservation Plan, other approved local, regional or state habitat conservation plan or any other local policies or ordinances that protect biological resources.

As previously discussed, the GPU EIR determined impacts on local policies and ordinances as well as habitat conservation plans and natural community conservation plans as less than significant. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

## Conclusion

With regards to the issue area of Biological Resources, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.



- 4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

	Significant Project Impact	Impact not identified by GPU EIR	Substantial New Information
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**5. Cultural Resources** – Would the Project:

- |  |                          |                          |                          |
|--|--------------------------|--------------------------|--------------------------|
| a) Cause a substantial adverse change in the significance of a historical resource as defined in 15064.5?    | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to 15064.5? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Directly or indirectly destroy a unique geologic feature?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d) Directly or indirectly destroy a unique paleontological resource or site?                                 | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| e) Disturb any human remains, including those interred outside of formal cemeteries?                         | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

5(a) The GPU EIR concluded this impact to be less than significant with mitigation. Based on an analysis of records and a survey of the property by County approved archaeologist, Andrew Pigniolo, it has been determined that there are no impacts to historical resources because they do not occur within the project site. The results of the survey are provided in an historical resources report titled, Cultural Resource Survey of the Pala Mesa Plaza Project, 3233 Old Highway 395, Fallbrook, San Diego County, California (PDS2020-STP-20-021) (September 2021), prepared by Andrew Pigniolo. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

5(b) The GPU EIR concluded this impact to be less than significant with mitigation. Based on an analysis of records and a survey of the property by County approved archaeologist, Andrew Pigniolo, it has been determined that there are no impacts to archaeological resources because they do not occur within the project site. The results of the survey are provided in an historical resources report titled, Cultural Resource Survey of the Pala Mesa Plaza Project, 3233 Old Highway 395, Fallbrook, San Diego County, California (PDS2020-STP-20-021) (September 2021), prepared by Andrew Pigniolo.

In addition, the Native American Heritage Commission (NAHC) was contacted for a listing of Native American Tribes whose ancestral lands may be impacted by the project. The NAHC response was received on October 27, 2021, indicating sacred sites, on record with the commission, were present on the project property. The NAHC recommended contacting the Kwaaymii Laguna Band of Mission Indians, the Rincon Band of Luiseño Indians, and the San Luis Rey Band of Mission Indians. All of the Native American groups and the Native American list provided by the NAHC were contacted. Only one tribe (Agua Caliente Band of Cahuilla Indians) responded identifying that the project is outside of their traditional use area, and they deferred to other tribes in the area.

Aleshanee Ventura of Saving Sacred Sites served as the Native American monitor during the cultural survey. No information has been obtained through Native American outreach or communication with the Native American monitor during fieldwork that any culturally or spiritually significant resources were present. No Traditional Cultural Properties that currently serve religious or other community practices are known to exist within the project area. During the current archaeological evaluation, no artifacts or remains were identified or recovered that could be reasonably associated with such practices.

Although no resources were identified during site surveys, responses from Native American tribes during outreach efforts by Laguna Mountain Environmental during the preparation of the cultural study, requested monitoring during earth-disturbing activities. In addition, the potential exists for subsurface deposits because several recorded archaeological sites are located within the vicinity of the project.

As considered by the GPU EIR, potential impacts to cultural resources will be mitigated through ordinance compliance and through implementation of the following mitigation measures: grading monitoring under the supervision of a County-approved archaeologist and a Luiseño Native American monitor and conformance with the County's Cultural Resource Guidelines if resources are encountered. The GPU EIR identified these mitigation measures as Cul 2.5. The project will be conditioned with archaeological monitoring (Cul-2.5) that includes the following requirements:

- Pre-Construction
  - Contract with a County approved archaeologist to perform archaeological monitoring and a potential data recovery program during all earth-disturbing activities. The Project Archaeologist shall perform the monitoring duties before, during and after construction.
  - Pre-construction meeting to be attended by the Project Archaeologist and Luiseño Native American monitor to explain the monitoring requirements.
- Construction
  - Monitoring. Both the Project Archaeologist and Luiseño Native American monitor are to be onsite during earth disturbing activities. The frequency and location of monitoring of native soils will be determined by the Project Archaeologist in consultation with the Luiseño Native American monitor. Both the Project Archaeologist and Luiseño Native American monitor will evaluate fill soils to ensure that they are negative for cultural resources
  - If cultural resources are identified:
    - Both the Project Archaeologist and Luiseño Native American monitor have the authority to divert or temporarily halt ground disturbance operations in the area of the discovery.
    - The Project Archaeologist shall contact the County Archaeologist, and culturally-affiliated tribes as identified in the Treatment Agreement and Preservation Plan at the time of discovery.
    - All ground disturbance activities within 100 feet of the discovered cultural resources shall be halted until a meeting is convened between the developer, the project archaeologist, tribal monitor(s), and the tribal representative(s) to discuss the significance of the find. Optionally, the County Archaeologist may attend the meeting to discuss the significance of the find.

- Construction activities shall not resume in the area of discovery until an agreement has been reached by all parties as to appropriate mitigation. Work shall be allowed to continue outside of the buffer area and shall be monitored.
- Isolates and non-significant deposits shall be minimally documented in the field. The isolates and/or non-significant deposits shall be reburied onsite as identified in the Treatment Agreement and Preservation Plan.
- Treatment and avoidance of the newly discovered resources shall be consistent with the Treatment Agreement and Preservation Plan entered into with the appropriate tribes. This may include avoidance of the cultural resources through project design, in-place preservation of cultural resources located in native soils and/or re-burial on the Project property so they are not subject to further disturbance in perpetuity.
- If cultural resources are identified, one or more of the following treatments, in order of preference, shall be employed:
  - Preservation in place of the Cultural Resources, if feasible. Preservation in place means avoiding the resources, leaving them in place where they were found with no development affecting the integrity of the resources.
  - Reburial of the resources on the project property. The measures for reburial shall include, at least, the following:
    - Measures and provisions to protect the future reburial area from any impacts in perpetuity.
    - Reburial shall not occur until all legally required cataloging and basic recordation have been completed, with the exception that sacred items, burial goods, and Native American human remains are excluded.
    - Any reburial process shall be culturally appropriate.
    - Listing of contents and location of the reburial shall be included in the confidential appendix of the Monitoring Report.
    - The Monitoring Report shall be filed with the County under a confidential cover and is not subject to Public Records requests.
- If preservation in place or reburial is not feasible, a Research Design and Data Recovery Program (Program) shall be prepared by the Project Archaeologist in consultation with the Tribe, and the Luiseño Native American Monitor and approved by the County Archaeologist prior to implementation. There shall be no destructive or invasive testing on sacred items, burial goods, and Native American human remains. Results concerning finds of any inadvertent discoveries shall be included in the Monitoring Report.

Pursuant to Calif. Pub. Res. Code § 21083.2(b) avoidance is the preferred method of preservation for archaeological resources and cultural resources. If the landowner and the Tribe(s) cannot agree on the significance or the mitigation for the archaeological or cultural resources, these issues will be presented to the Planning & Development Services Director for decision. The Planning & Development Services Director shall make the determination based on the provisions of the California Environmental Quality Act with respect to archaeological resources, recommendations of the project archeologist and shall take into account the cultural and religious principles and practices of the Tribe.

- Human Remains.
  - The Property Owner or their representative shall contact the County Coroner and the PDS Staff Archaeologist.

- Upon identification of human remains, no further disturbance shall occur in the area of the find until the County Coroner has made the necessary findings as to origin. If the human remains are to be taken offsite for evaluation, they shall be accompanied by the Luiseño Native American monitor.
  - If the remains are determined to be of Native American origin, the NAHC shall immediately contact the Most Likely Descendant (MLD).
  - The immediate vicinity where the Native American human remains are located is not to be damaged or disturbed by further development activity until consultation with the MLD regarding their recommendations as required by Public Resources Code Section 5097.98 has been conducted.
  - The MLD may with the permission of the landowner, or their authorized representative, inspect the site of the discovery of the Native American human remains and may recommend to the owner or the person responsible for the excavation work means for treatment or disposition, with appropriate dignity, of the human remains and any associated grave goods. The descendants shall complete their inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site.
  - Public Resources Code §5097.98, CEQA §15064.5 and Health & Safety Code §7050.5 shall be followed in the event that human remains are discovered.
- Tribal Cultural Resources
    - If tribal cultural resources are discovered, the Project Archaeologist shall conduct consultation with culturally-affiliated tribes to determine the most appropriate mitigation. Should the two parties not be able to reach consensus, then the County Archaeologist shall consider the concerns of the culturally-affiliated tribe and the Project Archaeologist, and the Director of Planning & Development Services shall make a final decision regarding appropriate mitigation.
  - Fill Soils
    - The Project Archaeologist and Luiseño Native American monitor shall evaluate fill soils to determine that they are clean of cultural resources.
- Rough Grading
    - Monitoring Report. Upon completion of Rough Grading, a monitoring report shall be prepared identifying whether resources were encountered. A copy of the monitoring report shall be provided to the South Coastal Information Center and any culturally-affiliated tribe who requests a copy.
  - Final Grading
    - Final Report. A final report shall be prepared substantiating that earth-disturbing activities are completed and whether cultural resources were encountered. A copy of the final report shall be submitted to the South Coastal Information Center, and any culturally-affiliated tribe who requests a copy.
    - Cultural Material Conveyance
      - The final report shall include evidence that all Native American cultural materials in order of preference have been conveyed as follows:
        - Evidence that all prehistoric materials collected during the archaeological monitoring program have been reburied.

or

- Evidence that all prehistoric materials collected during the grading monitoring program have been repatriated to a Native American group of appropriate tribal affinity. Evidence shall be in the form of a letter from the Native American tribe to whom the cultural resources have been repatriated identifying that the archaeological materials have been received.
- The final report shall include evidence that all historic materials have been curated at a San Diego curation facility and shall not be curated at a Tribal curation facility or repatriated. The collections and associated records, including title, shall be transferred to the San Diego curation facility and shall be accompanied by payment of the fees necessary for permanent curation. Evidence shall be in the form of a letter from the curation facility stating that the historic materials have been received and that all fees have been paid.

The GPU EIR identified these mitigation measures as Cul-2.5. The environmental documentation associated with the project does not consist of a Mitigated Negative Declaration, Negative Declaration, or Environmental Impact Report which requires AB-52 consultation. The project is required to conform with Grading Ordinance Sections 87.429 and 87.430 which requires grading operations to be suspended in the event that resources are encountered and a County Official shall be informed to evaluate potentially significant resources.

- 5(c) The GPU EIR concluded this impact to be less than significant. The site does not contain any unique geologic features that have been listed in the County's Guidelines for Determining Significance for Unique Geology Resources nor does the site support any known geologic characteristics that have the potential to support unique geologic features.

As previously discussed, the GPU EIR determined impacts on unique geologic features as less than significant. As the Project would have a less-than-significant impacts for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 5(d) The GPU EIR concluded this impact to be less than significant with mitigation. A review of the County's Paleontological Resources Maps and data on San Diego County's geologic formations indicates that the project is located on geological formations that do not contain unique paleontological resources. The project is required to conform with Grading Ordinance Sections 87.429 and 87.430 which requires grading operations to be suspended in the event that resources are encountered and a County Official shall be informed to evaluate potentially significant resources.

As considered by the GPU EIR, potential impacts to paleontological resources will be mitigated through ordinance compliance and through implementation of the following mitigation measures: grading monitoring by the project contractor and conformance with the County's Paleontological Resource Guidelines if resources are encountered. The GPU EIR identified these mitigation measures as Cul-3.1.

- 5(e) The GPU EIR concluded this impact to be less than significant with mitigation. Based on an analysis of records and archaeological surveys of the property, it has been determined that the project site does not include a formal cemetery or any archaeological resources

that might contain interred human remains. The project is required to conform with Grading Ordinance Sections 87.429 and 87.430 which requires grading operations to be suspended in the event that resources are encountered and a County Official shall be informed to evaluate potentially significant resources. As previously discussed, the GPU EIR determined impacts to human remains as less than significant with mitigation. The proposed Project determined impacts to human remains as potentially significant.

**Conclusion**

With regards to the issue area of cultural/paleontological resources, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. Feasible mitigation measures contained within the GPU EIR will be applied to the project.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**6. Energy Use – Would the Project:**

- |   |                          |                          |                          |
|---|--------------------------|--------------------------|--------------------------|
| a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

Energy use was not specifically analyzed within the GPU EIR as a separate issue area under CEQA. At the time, Energy Use was contained within Appendix F of the CEQA Guidelines and since then has been moved to the issue areas within Appendix G of the CEQA Guidelines. However, the issue of energy use in general was discussed within the GPU and the GPU EIR. For example, within the Conservation and Open Space Element of the GPU, Goal COS-15 promotes sustainable architecture and building techniques that reduce emissions of criteria pollutants and Greenhouse Gas (GHG), while protecting public health and contributing to a more sustainable environment. Policies, COS-15.1, COS-15.2, and COS-15.3 would support this goal by encouraging design and construction of new buildings and upgrades of existing buildings to maximize energy efficiency and reduce GHG. Goal COS-17 promotes sustainable solid waste management. Policies COS-17.1 and COS-17.5 would support this goal by reducing GHG emissions through waste reduction techniques and methane recapture. The analysis below specifically analyzes the energy use of the project.

- 6(a) The project would increase the demand and consumption of electricity at the project site during construction and operation, relative to existing conditions. CEQA requires

mitigation measures to reduce “wasteful, inefficient and unnecessary” energy usages (Public Resources Code Section 21100, subdivision [b][3]). Neither the law nor the State CEQA Guidelines establish criteria that define wasteful, inefficient, or unnecessary use. The project consists of expansion and redevelopment of an existing commercial center near the Interstate 15 and State Route 76 Interchange. The Project is conditioned to not expand the use of natural gas for operations of the project throughout the duration of the permit. Compliance with building code standards would result in highly energy-efficient buildings. However, compliance with building codes does not adequately address all potential energy impacts during construction and operation. It can be expected that energy consumption, outside of the building code regulations, would occur through the transport of construction materials to and from the site during the construction phase, and trips to and from the site during the operational phase.

During the grading and construction phases of the Project, the primary energy source utilized would be petroleum from construction equipment and vehicle trips. To a lesser extent, electricity would also be consumed for the temporary electric power for as-necessary lighting and electronic equipment. Activities including electricity would be temporary and negligible; therefore, electricity use during grading and construction would not result in wasteful, inefficient, or unnecessary consumption of energy. Vehicle trips associated with the transportation of construction materials and construction workers commutes would also result in petroleum consumption, but to a lesser extent. Petroleum consumptions would be necessary for operation and maintenance of construction equipment and would not be beyond what is necessary for the Project. Additionally, the Project will be conditioned to use Tier 4 construction equipment which requires the use of specific vehicles and machinery designed to meet standards set by the United States Environmental Protection Agency and California Air Resources Board (CARB) that require use of energy efficient engines and equipment that meet criteria to limit emissions. Due to the aforementioned factors, the Project’s energy consumption during the grading and construction phase would not be considered wasteful, inefficient, or unnecessary.

Operation of the Project would require the use of water for landscape maintenance as well as petroleum for maintenance and employee related activities of the Project. Operation of the Project would be typical of commercial land uses within the surrounding project vicinity as the site is zoned as General Commercial (C36) and the use is permitted within the zoning use regulation. Over the lifetime of the proposed Project, fuel efficiency of vehicles is expected to increase as older vehicles are replaced with newer, more efficient models. As such, the amount of petroleum consumed as a result of vehicle trips to and from the Project site during operation would decrease over time. State and Federal regulations regarding standards for vehicles (e.g., Advanced Clean Cars Program, CAFÉ Standards) are designed to reduce wasteful, unnecessary, and inefficient use of fuel. The coupling of various State policies and regulations such as the Zero-Emission Vehicles Mandate and Senate Bill (SB) 350 would result in the deployment of electric vehicles (EVs) which would be powered by an increasingly renewable electrical grid. It should be noted that the Project consists of redevelopment of an existing commercial center which will result in the reuse of existing facilities and structures and limit wasteful use of energy related resources. Additionally, the Project would provide multiple sustainability features that would reduce transportation and building energy consumption and increase the efficient use of water through consistency with the County’s Landscaping Ordinance, the Project not requiring the use of natural gas for new structures on the site, and consistency with CALGreen Tier 2 standards for off-street vehicle requirements.

As previously discussed, the GPU EIR did not analyze Energy as a separate issue area under CEQA. Energy was analyzed under the GPU and GPU EIR and has been incorporated within General Plan Elements. The Project would not conflict with policies within the GPU related to energy use, nor would it result in the wasteful, inefficient, or unnecessary consumption of energy resources, as specified within Appendix G of the CEQA Guidelines.

- 6(b) Many of the regulations regarding energy efficiency are focused on increasing the energy efficiency of buildings and renewable energy generation, as well as reducing water consumption and reliance on fossil fuels. The project includes sustainability measures such as water reduction measures as required by the Landscaping Ordinance and complying with CALGreen Tier 2 standards for off-street vehicle requirements. By complying the CALGreen Tier 2 standards, the Project will increase the availability of electrical vehicle charging spaces within the Fallbrook community and for motorists traveling along Old Highway 395 and Interstate 15. Additionally, the project would be consistent with sustainable development and energy reduction policies such as policies COS-14.3 and COS-15.4 of the General Plan, through compliance with the most recent building code and Energy Efficiency Standards at the time of project construction. Further information can be found within response 6(a) as well as Section 8: Greenhouse Gas Emissions of the 15183 Environmental Checklist. Therefore, the project would not conflict with or obstruct a state or local plan for renewable energy or energy efficiency.

As previously discussed, the GPU EIR did not analyze Energy as a separate issue area under CEQA. Energy was analyzed under the GPU and GPU EIR and has been incorporated within General Plan Elements. The Project would not conflict with policies within the GPU related to energy use or conflict with or obstruct a state or local plan for renewable energy or energy efficiency as specified within Appendix G of the CEQA Guidelines.

**Conclusion**

With regards to the issue area of Energy, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
5. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
6. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**7. Geology and Soils – Would the Project:**

- |  |                          |                          |                          |
|--|--------------------------|--------------------------|--------------------------|
| a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: rupture of a known earthquake fault, strong | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|



seismic ground shaking, seismic-related ground failure, liquefaction, and/or landslides?

- b) Result in substantial soil erosion or the loss of topsoil?
  
- c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in an on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?
  
- d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?
  
- e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

**Discussion**

- 7(a)(i) The GPU EIR concluded this impact to be less than significant. The Project is not located in a fault rupture hazard zone identified by the Alquist-Priolo Earthquake Fault Zoning Act, Special Publication 42, Revised 1997, Fault-Rupture Hazards Zones in California, nor is it located within a known Active Fault Near-Source Zone. The County Guidelines for Determining Significance for Geologic Hazards consider a project to have a potentially significant impact if the project proposes any building or structure to be used for human occupancy over or within 50 feet of the trace of an Alquist-Priolo fault or County Special Study Zone Fault. The Project site is located approximately 8 and a half miles west of the nearest Alquist-Priolo Fault Zone. Additionally, construction in accordance with the California Building Code Seismic Requirements would be required prior to the issue of a building permit. Therefore, a less than significant impact from the exposure of people or structures to adverse effects from a known fault-rupture hazard zone would occur as a result of the proposed Project.
  
- 7(a)(ii) The GPU EIR concluded this impact to be less than significant. To ensure the structural integrity of all buildings and structures, the project must conform to the Seismic Requirements as outlined within the California Building Code. Therefore, compliance with the California Building Code and the County Building Code will ensure that the project will not result in a significant impact.
  
- 7(a)(iii) The GPU EIR concluded this impact to be less than significant. To ensure the structural integrity of all buildings and structures, the project must conform to the Seismic Requirements as outlined within the California Building Code. Therefore, compliance with the California Building Code and the County Building Code would ensure that the project would not result in a significant impact.
  
- 7(a)(iv) The GPU EIR concluded this impact to be less than significant. The site is not located within a "Landslide Susceptibility Area" as identified in the County Guidelines for Determining Significance for Geologic Hazards. Landslide Susceptibility Areas were developed based on landslide risk profiles included in the Multi-Jurisdictional Hazard Mitigation Plan, San Diego, CA (URS, 2004). Landslide risk areas from this plan were based on data including steep slopes (greater than 25%); soil series data (SANDAG based

on USGS 1970s series); soil-slip susceptibility from USGS; and Landslide Hazard Zone Maps (limited to western portion of the County) developed by the California Department of Conservation Division of Mines and Geology (DMG). Based on the flat topography of the site, potential hazards associated with landslides are less than significant. As previously discussed, the GPU EIR determined less than significant impacts from exposure to seismic-related hazards and soil stability. The project site has been previously graded and contains an existing commercial plaza. Additionally, earthwork associated with the project consists of approximately 7,725 cubic yards of cut and 5,575 cubic yards of fill and proposes retaining walls along the western boundary of the project site which will assist in stabilize existing slopes. As the proposed Project would have a less-than-significant, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 7(b) The GPU EIR concluded this impact to be less than significant. According to the Soil Survey of San Diego County, the soils on-site are identified as Greenfield sandy loam, 5 to 9 percent slopes, and Ramona sandy loam, 2 to 5 percent slopes, that have a soil erodibility rating of severe. The project will not result in substantial soil erosion or the loss of topsoil because the project will be required to comply with the Watershed Protection Ordinance (WPO) and Grading Ordinance which will ensure that the project would not result in any unprotected erodible soils, will not alter existing drainage patterns, and will not develop steep slopes. Additionally, the project will be required to implement Best Management Practices (BMPs) to prevent fugitive sediment. The majority of grading associated with the Project is tied to cut for slopes in the rear of the property that will be supported by retaining walls ranging in size from approximately two to 12 feet in height. Further analysis related to erosion or loss of topsoil can be found in responses to Section 10 – Hydrology and Water Quality. As previously discussed, the GPU EIR determined impacts from soil erosion and topsoil loss to be less than significant. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.
- 7(c) The GPU EIR concluded this impact to be less than significant. Landslide Susceptibility Areas was discussed in response (a)(iv). As indicated in response (a)(iv), the project site is not located within a “Landslide Susceptibility Area” as identified in the County Guidelines for Determining Significance for Geologic Hazards, and the potential for landslides to impact the proposed development is considered to be low.

Lateral spreading is a principal effect from liquefaction which was discussed in response 7(a)(iii). As discussed in response 7(a)(iii), the project site is not located within a “Potential Liquefaction Area” as identified in the County Guidelines for Determining Significance for Geologic Hazards. Subsidence and collapse may be caused by unstable geological structures or conditions. As stated in response 7(a), impacts to the project site from rupture of a known earthquake fault and strong seismic ground shaking or seismic-related ground failure would be unlikely to occur due to compliance with building code standards. Structures associated with the Project consist of a remodeled approximately 9,075 square foot market structure, an existing hamburger restaurant, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store, and two new 6,000 square foot retail buildings. All structures associated with the Project are commercial structures and do not have any units or residential occupancy. As previously discussed, the GPU EIR determined impacts from soil stability to be less than significant. As the project would have a less than significant impact with the consistency with building code

standards, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 7(d) The GPU EIR determined impacts from expansive soils to be less than significant. The Project is not underlain by expansive soils. In addition, the Project would not result in a significant impact because compliance with the Building Code, preparation of a Soils Engineering Report, and implementation of standard engineering techniques would ensure structural safety. According to the Soil Survey of San Diego County the Project is underlain by Greenfield sandy loam, 5 to 9 percent slopes, and Ramona sandy loam, 2 to 5 percent slopes, and is not in an area anticipated to have expansive soils. The project will not result in a significant impact because compliance with the Building Code and implementation of standard engineering techniques will ensure structural safety.

As previously discussed, the GPU EIR determined impacts from expansive soils to be less than significant. As the Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis within the GPU EIR because it would not create new impacts, increase impacts, and there is no new information of substantial importance than identified within the GPU EIR.

- 7(e) The GPU EIR concluded this impact to be less than significant. The Project site would rely on public water and sewer for the disposal of wastewater and does not include construction of septic tanks or alternative wastewater disposal systems. As such, the Project would not place septic tanks or alternative wastewater disposal systems on soils incapable of adequately supporting the tanks or system.

As previously discussed, the GPU EIR determined impacts to wastewater disposal systems to be less than significant. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Geology/Soils, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**8. Greenhouse Gas Emissions** – Would the Project:

a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

**Discussion**

8(a) The General Plan and GPU EIR contain policies and mitigation measures associated with reducing greenhouse gas emissions including but not limited to compliance with the County Groundwater Ordinance, Landscape Ordinance, as well as implementation of solid waste reduction measures, reduction of Vehicle Miles Traveled (VMT). Additionally, the County of San Diego (County) General Plan incorporates smart growth and land planning principles intended to reduce vehicle miles traveled, and thereby reduce GHG emissions. Specifically, the General Plan directed preparation of a County Climate Action Plan (CAP) with reduction targets; development of regulations to encourage energy efficient building design and construction; and development of regulations that encourage energy recovery and renewable energy facilities, among other actions. As such, on February 14, 2018, the County Board of Supervisors (Board) adopted a CAP, which identifies specific strategies and measures to reduce GHG emissions in the largely rural, unincorporated areas of San Diego County as well as County government operations. The County’s 2018 Climate Action Plan (CAP) was set aside by the Fourth District Court of Appeal and rescinded by the Board. On September 30, 2020, the Board voted to set aside its approval of the County’s 2018 CAP and related actions because the Final Supplemental Environmental Impact Report (2018 CAP SEIR) was found to be out of compliance with CEQA. In response to this Board action, the County is preparing a CAP Update to revise the 2018 CAP and correct the items identified by the 4th District Court of Appeal in San Diego within the Final 2018 CAP SEIR that were not compliant. Therefore, compliance with the 2018 CAP was not utilized to determine potential greenhouse gas (GHG) emission impacts.

The County of San Diego Board of Supervisors recently adopted a new Climate Action Plan (CAP) on September 11, 2024. The Draft Supplemental Environmental Impact Report and Draft Climate Action Plan were circulated for public review at the end of 2023 and the beginning of 2024. Due to the absence of an adopted CAP when the project was submitted with a complete application in 2020, compliance with the CAP was not utilized as the threshold in determining potential greenhouse gas (GHG) emission impacts. It should be notes that the thresholds summarized below are reflexive of several of the CAP’s overarching goals including building decarbonization and the electrification of the on-road vehicle fleet.

Executive Order (EO) S-3-05 and EO B-30-15 established GHG emission reduction targets for the state, and AB 32 launched the CARB Climate Change Scoping Plan that outlined the reduction measures needed to reach the 2020 target, which the state has achieved. As required by SB 32, the California Air Resource Board’s (CARB) 2017 Climate Change Scoping Plan outlines reduction measures needed to achieve the 2030 target. AB 1279, the California Climate Crisis Act, codified the carbon neutrality target as 85 percent below 1990 levels by 2045. CARB’s 2022 Scoping Plan was adopted by the CARB Board December of 2022. Project impacts were assessed using a project-specific, locally appropriate threshold, as guided by CEQA Guidelines Section 15064.4. Based on the specific characteristics of this project including the Project’s less than significant impact associated with Vehicle Miles Traveled (VMT), current guidance provided by the Bay Area

Air Quality Management District (BAAQMD) was used to evaluate GHG emissions. For land use development projects, the BAAQMD recommends using the approach endorsed by the California Supreme Court in *Center for Biological Diversity v. Department of Fish & Wildlife* (2015) (62 Cal.4th 204), which evaluates a project based on its effect on California's efforts to meet the state's long-term climate goals. As the Supreme Court held in that case, a project that would be consistent with meeting those goals can be found to have a less than significant impact on climate change under CEQA. If a project would contribute its "fair share" of what would be required to achieve those long-term climate goals, then a reviewing agency can find that the impact would not be significant because the project would help to solve the problem of global climate change (62 Cal.4th 220–223). If a land use project incorporates all of the design elements necessary for it to be carbon neutral by 2045, then it would contribute its portion of what is needed to achieve the state's climate goals and would help to solve the cumulative problem. It can therefore be found to make a less than cumulatively-considerable climate impact. Because this guidance supports how a project would contribute its "fair share" of the statewide long-term GHG reduction goals, it is not specific to the BAAQMD region and can also be applied in the San Diego region. The information provided in the BAAQMD Justification Report is intended to provide the substantial evidence that lead agencies need to support their determinations about significance using these thresholds. The BAAQMD Justification Report analyzes what would be required of new land use development projects to achieve California's long-term climate goal of carbon neutrality by 2045. A new land use development project being built today needs to incorporate the following design elements to do its "fair share" of implementing the goal of carbon neutrality by 2045:

A) Projects must include, at a minimum, the following project design elements:

1) Buildings

- a) The project will not include natural gas appliances or natural gas plumbing (in both residential and nonresidential development).
- b) The project will not result in any wasteful, inefficient, or unnecessary energy usage as determined by the analysis required under CEQA Section 21100(b)(3) and Section 15126.2(b) of the State CEQA Guidelines.

2) Transportation

- a) Achieve a reduction in project-generated VMT below the regional average consistent with the current version of the California Climate Change Scoping Plan (currently 15 percent) or meet a locally adopted Senate Bill 743 VMT target, reflecting the recommendations provided in the Governor's Office of Planning and Research's Technical Advisory on Evaluating Transportation Impacts in CEQA:
  - (i) Residential projects: 15 percent below the existing VMT per capita
  - (ii) Office projects: 15 percent below the existing VMT per employee
  - (iii) Retail projects: no net increase in existing VMT
- b) Achieve compliance with off-street electric vehicle requirements in the most recently adopted version of CALGreen Tier 2.

The Project is consistent with both the Building and Transportation design elements as outlined in the BAAQMD Justification Report. The Project is conditioned to not use natural gas for the operations of the Project. Primary utilities and energy use associated with the operation of the commercial center are anticipated to be associated with the use of electricity to power lighting and general utilities associated with the commercial structures of the Project. The Project does not propose features or uses that would require additional resources or energy such as decorative water features. Operation of the project would

require use of water for landscape maintenance which is required to be in conformance with the Landscape Ordinance and requirements to demonstrating water use reduction. The Project will obtain its water supply from Rainbow Municipal Water District and the Project is conditioned to process a final Landscape Plan which implements additional measures in verifying that the Project is consistent with the water use reduction requirements of the Landscape Ordinance. As detailed in Section 6: Energy Use, the Project is not expected to result in the wasteful use of energy.

The Project site contains an existing market with attached businesses consisting of restaurants and personal services such as financial and insurance companies. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along I-15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near I-15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technically Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally-serving commercial uses. The Project is also conditioned to be consistent with the CALGreen Tier 2 standards for compliance with off-street electric vehicle requirements which will result in increased availability of electrical vehicle charging stations within the Fallbrook Community and motorists traveling along Old Highway 395 and Interstate 15.

Lastly, the emissions associated with construction and operation of the Project were quantified in the Greenhouse Gas Analysis dated July 2023 by Ldn Consulting, Inc. Construction emissions associated with the development of the Project are temporary and expected to be approximately 249 metric tons of carbon dioxide equivalent (MTCO<sub>2e</sub>). Operational emissions of the Project were estimated to be 825 MTCO<sub>2e</sub>.

The project's implementation of design features for a "fair share" contribution towards the statewide goal of carbon neutrality by 2045 and the project's less than significant impact related to Vehicle Miles Traveled demonstrates that the project would not make a cumulatively considerable contribution to GHG emissions. Therefore, the project would not generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment, and impacts would be less than significant.

- 8(b) The proposed Project is subject to the General Plan Semi-Rural Regional Category and General Commercial (C-1) Land Use Designation. The Project is also subject to the Fallbrook Community Plan Policies. The property is subject to the General Commercial Land Use Designation (C36) which permits the proposed uses of the site in accordance with Sections 2360 through 2363 of the Zoning Ordinance. The site is also subject a "B" Special Area Designator for community design review which requires the processing of a

Site Plan permit. All proposed uses are consistent with the General Plan Designation and the Zoning for the site and a General Plan Amendment or Zoning Reclassification is not required for the project. Through its goals, policies, and land use designations, the County’s General Plan aims to reduce County-wide GHG emissions. Furthermore, the County’s General Plan growth projections were used to inform the development of the SANDAG Regional Transportation Plan and Sustainable Communities Strategy (SANDAG RTP/SCS). SANDAG’s RTP/SCS is the region’s applicable plan for reducing GHG emissions and is consistent with State GHG emissions reductions goals set by the California Air Resources Board (CARB).

In December of 2022, the CARB adopted a new Scoping Plan which outlined policies and strategies focused on three priority areas: 1. Transportation Electrification, 2. VMT Reduction, and 3. Building Decarbonization. As detailed in response 8(a), the Project will comply the three priority areas of the CARB scoping plan by increasing the availability of electrical vehicle charging stations, having a less than significant impact associated with VMT, and not including the use of natural gas for the operation of the Project. Because the proposed project is consistent with the General Plan land use and zoning, it is also consistent with State GHG emission reduction targets as identified in the SANDAG RTP/SCS. Therefore, the project would be consistent with any applicable plan, policy, or regulation adopted for the purpose of reducing greenhouse gas emissions.

**Conclusion**

With regards to the issue area of Greenhouse Gas Emissions, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. The project will apply measures outlined within GPU EIR including but not limited to compliance with the County Groundwater Ordinance, Landscape Ordinance, as well as reduction of Vehicle Miles Traveled (VMT). Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR. The project will also apply project design features such as no use of natural gas as well as consistency with CALGreen Tier 2 standards for off-street parking.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**9. Hazards and Hazardous Materials – Would the Project:**

a) Create a significant hazard to the public or the environment through the routine transport, storage, use, or disposal of hazardous materials or wastes or through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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b) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

c) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5, or is otherwise known to have been subject to a release of hazardous substances and, as a result, would it create a significant hazard to the public or the environment?

d) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?

e) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?

f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

g) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

h) Propose a use, or place residents adjacent to an existing or reasonably foreseeable use that would substantially increase current or future resident's exposure to vectors, including mosquitoes, rats or flies, which are capable of transmitting significant public health diseases or nuisances?

**Discussion**

9(a) The GPU EIR concluded this impact to be less than significant. The Project consists of the expansion and remodeling of an existing commercial plaza. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. The existing parking lot will be expanded and re-designed to include a total of 134 parking spaces. The Project also includes proposed landscaping as well as signage for the commercial tenants of the site. Access to the site would be provided by two commercial driveways connecting to Old Highway 395. Water and Sewer service would be provided by the Rainbow Municipal Water District. The existing road on the northern portion of the Project site known as Via Belmonte will be widened with a half width improvement of 14 feet and a graded half width of twenty feet along the south side of Via Belmonte. The Project also includes a vacation and/or removal and re-dedication of road, drainage, and slope easements related to maintenance along Old Highway 395.



Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export.

Operations:

The Project includes on-site sale of gasoline and would result in handling, storage, and disposal of hazardous substances. The project proposes storage of potentially hazardous materials consisting of fuels that will be sold in product dispensers and stored in underground storage tanks on the project site. Fuels associated with the operation of the gas station must be permitted through the Department of Environmental Health Hazardous Materials Division through a hazardous materials business plan (HMBP) and permits for underground storage tanks. Underground storage tanks require maintenance and inspections in order to ensure that no leaks of fuel product will result in exposing any potential sensitive receptors to pollutants or leaking of product into the soil on the project site. The purpose of the HMBP is to prevent or minimize damage to public health, safety, and the environment from a release of a hazardous material. All other uses associated with the Project are not anticipated to require handling, storage, or disposal of hazardous materials. Any waste associated with the Project and commercial structures will be stored within a trash enclosure that is required to be routinely maintained. Additionally, a Phase 1 Environmental Site Assessment dated May 2020 prepared by GeoTek, Inc. was prepared for the Project and concluded that there is no evidence of a recognized environmental condition of concern on the Project site. Therefore, the project will not create a significant hazard to the public or the environment through the routine transport, storage, use, or disposal of hazardous materials or wastes or through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment as the project requires additional permits for construction and operation of the site.

Construction:

The existing market and attached businesses were constructed in the late 1970s. Due to the age of the structures, the Project will be conditioned to conduct lead and asbestos surveys prior to remodeling and construction on the market and attached structures. In the event that lead and asbestos are found in the existing structures, construction associated with the building permit of the market renovation will require lead and asbestos treatment in conformance with the Air Pollution and Control District regulations. Construction associated with the Project will also require standard dust control measures such as the use of water trucks consistent with the requirements of the Air Pollution Control District. Potential emissions of hazardous materials during construction will be temporary and required to conform with standard requirements of the Air Pollution Control District.

As previously discussed, the GPU EIR determined impacts from transport, use and disposal of hazardous materials and accidental release of hazardous materials to be less than significant. Additional analysis related to emissions of hazards and hazardous materials can be found in Section 3 – Air Quality. The project conditions are consistent with General Plan Policy S-11.4 as analyzed in the GPU EIR. Thus, for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 9(b) The GPU EIR concluded this impact to be less than significant. The Project is not located within a quarter of a mile of a nearby school. The nearest schools or property designated for a school are northeast of the Project site and across Interstate 15. The schools consist of Palomar College as well as a school site designated within the Meadowood/Citro development. Further information can be found in response 9(a) regarding required

maintenance and handling of potentially hazardous materials associated with the project. Therefore, the project will not have any effect on an existing or proposed school.

- 9(c) The GPU EIR concluded this impact to be less than significant. Based on historic imagery, review of previous permits, and review of applicable databases, the Project is not proposed on a site that has been known to release hazardous materials. The nearest property that has the potential to release potentially hazardous materials is located approximately 800 feet of the Project site on a property containing an existing Mobil gas station property had a case listed GeoTracker which has since been closed in 2016. The Project does not propose structures for human occupancy or significant linear excavation within 1,000 feet of an open, abandoned, or closed landfill, is not located on or within 250 feet of the boundary of a parcel identified as containing burn ash (from the historic burning of trash), and is not on or within 1,000 feet of a Formerly Used Defense Site. Further information regarding ongoing operations of the site and potential release of hazardous substances can be found in response 9(a). Therefore, the project will not emit or release hazardous materials due to the historic uses of the site.
- 9(d) The GPU EIR concluded this impact to be less than significant with mitigation. The proposed Project is not located within an Airport Influence Area or an Airport Land Use Compatibility Plan Area. Additionally, the Project is not located within an Airport Safety Zone, within an Avigation Easement, an Overflight area or within a Federal Aviation Administration Height Notification Surface area. In addition, the Project does not propose construction of any structure equal to or greater than 150 feet in height, constituting a safety hazard to aircraft and/or operations from an airport or heliport.

As previously discussed, the GPU EIR determined impacts on public airports to be less than significant. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 9(e) The GPU EIR concluded this impact to be a significant and unavoidable impact. The proposed Project site is not within one mile of a private airstrip. The Project site is located approximately a half of a mile away from a runway that is used for model, remote-controlled airplanes. However, the existing model airplane runway will be removed upon the development of the approved Campus Park West project located on the same property as the model airplane runway. Additionally, the Project consists of expanding and remodeling an existing commercial center and does not consist of residential uses for occupancy. Therefore, the proposed project would not result in a safety hazard for people residing or working in the project area.
- 9(f)(i) **OPERATIONAL AREA EMERGENCY PLAN AND MULTI-JURISDICTIONAL HAZARD MITIGATION PLAN:** The GPU EIR concluded this impact related to section 9(f) to be significant and unavoidable. The Operational Area Emergency Plan is a comprehensive emergency plan that defines responsibilities, establishes an emergency organization, defines lines of communications, and is designed to be part of the statewide Standardized Emergency Management System. The Operational Area Emergency Plan provides guidance for emergency planning and requires subsequent plans to be established by each jurisdiction that has responsibilities in a disaster situation. The Multi-Jurisdictional Hazard Mitigation Plan includes an overview of the risk assessment process, identifies hazards present in the jurisdiction, hazard profiles, and vulnerability assessments. The plan also identifies goals, objectives and actions for each jurisdiction in the County of San Diego, including all cities and the County unincorporated areas. The project will not

interfere with this plan because it will not prohibit subsequent plans from being established or prevent the goals and objectives of existing plans from being carried out.

9(f)(ii) SAN DIEGO COUNTY NUCLEAR POWER STATION EMERGENCY RESPONSE PLAN: The property is not within the San Onofre emergency planning zone.

9(f)(iii) OIL SPILL CONTINGENCY ELEMENT: The project is not located along the coastal zone.

9(f)(iv) EMERGENCY WATER CONTINGENCIES ANNEX AND ENERGY SHORTAGE RESPONSE PLAN: The Emergency Water Contingencies Annex and Energy Shortage Response Plan will not be interfered with because the project does not propose altering major water or energy supply infrastructure.

9(f)(v) DAM EVACUATION PLAN: The Project site is not located within an identified dam inundation zone. Additionally, the development would not constitute a "Unique Institution" such as a hospital, school, or retirement home pursuant to the Office of Emergency Services included within the County Guidelines for Determining Significance, Emergency Response Plans.

As previously discussed, the GPU EIR determined impacts from emergency response and evacuation plans to be less than significant with mitigation. As the Project would have a less than significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

9(g) The GPU EIR concluded this impact to be significant and unavoidable. The proposed project is adjacent to wildlands that have the potential to support wildland fires. However, the project will not expose people or structures to a significant risk of loss, injury or death involving wildland fires because the project will comply with the regulations relating to emergency access, water supply, and defensible space specified in the Consolidated Fire Code. The project design has been reviewed and approved by the San Diego County Fire District. The project does not propose a residential use for occupancy. Therefore, the project will not expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands

9(h) The GPU EIR concluded this impact as less than significant. The project does not involve or support uses that would allow water to stand for a period of 72 hours or more (e.g. artificial lakes, agricultural ponds). The stormwater quality basins associated with the Project are tied to a Priority Development Project Stormwater Quality Management Plan which requires maintenance and installation of engineered soil in order to prevent standing water within the basins. Also, the project does not involve or support uses that will produce or collect animal waste, such as equestrian facilities, agricultural operations (chicken coops, dairies etc.), solid waste facility or other similar uses. Therefore, the project will not substantially increase current or future resident's exposure to vectors, including mosquitoes, rats, or flies.

As previously discussed, the GPU EIR determined impacts from vectors to be less than significant with mitigation. As the proposed project would have a less than significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Hazards, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant by adhering to the Project conditions of approval, which are consistent with the GPU EIR.

	<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
<b>10. Hydrology and Water Quality</b> – Would the Project:			
a) Violate any waste discharge requirements?			
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Is the project tributary to an already impaired water body, as listed on the Clean Water Act Section 303(d) list? If so, could the project result in an increase in any pollutant for which the water body is already impaired?			
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Could the proposed project cause or contribute to an exceedance of applicable surface or groundwater receiving water quality objectives or degradation of beneficial uses?			
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?			
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?			
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
f) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

- g) Create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems?
- h) Provide substantial additional sources of polluted runoff?
- i) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map, including County Floodplain Maps?
- j) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?
- k) Expose people or structures to a significant risk of loss, injury or death involving flooding?
- l) Expose people or structures to a significant risk of loss, injury or death involving flooding as a result of the failure of a levee or dam?
- m) Inundation by seiche, tsunami, or mudflow?

**Discussion**

10(a) The GPU EIR concluded this impact to be significant and unavoidable. The Project will require a National Pollutant Discharge Elimination System (NPDES) General Permit for Discharges of Storm Water Associated with Construction Activities. A Priority Development Project Stormwater Quality Management Plan (SWQMP) was prepared for the project by Excel Engineering dated June 2022. The SWQMP demonstrates that the project would comply with all requirements of the Watershed Protection Ordinance (WPO). The project will be required to implement site design measures, source control BMPs, and/or structural BMPs to reduce potential pollutants and address hydromodification impacts to the maximum extent practicable. For example, the Project includes a biofiltration basin that in order to treat runoff of pollutants related to operations of the Project that are collected on impervious surfaces. The biofiltration basin is required to be maintained and contains engineered soil that is design to treat runoff of pollutants. All proposed BMPs are outlined in the Priority Development Project Stormwater Quality Management Plan will be reviewed for conformance with any future construction permit associated with the project such as a Major Grading Permit reviewed during final engineering. These measures will enable the project to meet waste discharge requirements as required by the San Diego Municipal Permit, as implemented by the BMP Design Manual.

In addition to WPO compliance this facility is subject to compliance with the Industrial Storm Water Permit with the CA State Water Resources Control Board (SWRCB) and is required to file a Notice of Intent (NOI) and develop and implement a Storm Water Pollution Prevention Plan (SWPPP) as the project consists of construction on over 1 acre. These measures will enable the project to meet waste discharge requirements as required by the San Diego Municipal Permit, as implemented by the BMP Design Manual.

10(b) The GPU EIR concluded this impact to be significant and unavoidable. The Project lies in the Bonsall sub-basin (903.12) of the San Luis Rey hydrologic unit. According to the Clean Water Act Section 303(d) list, a portion of this watershed is impaired. Pollutants of concern in the San Luis Rey River watershed include nutrients/agricultural runoff, salinity, and phosphorous. The project would comply with the Watershed Protection Ordinance (WPO) and would implement BMPs in order to prevent additional runoff and increase of pollutants into the water body. As previously discussed, the GPU EIR determined significant and unavoidable impacts to water quality standards and requirements. However, the proposed Project would have a less than significant impact to water quality standards with the implementation of a Project condition and compliance with local and state requirements. These requirements were identified by the GPU EIR as mitigation measures Hyd-1.2 through Hyd-1.5 for implementation of Low Impact Development Standards (LID), compliance with the Watershed Protection Ordinance (WPO), the Best Management Practices Design Manual, Groundwater Ordinance, and the County Guidelines for Determining Significance for Surface Water Quality, Hydrology and Groundwater Resources. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

10(c) The GPU EIR concluded this impact to be significant and unavoidable. Potential pollutant sources associated with the Project are trash enclosures, fuel dispensing areas, and runoff of pollutants caused by vehicles in a parking lot. The fuel dispensing areas are covered by a canopy and will not have permeable floors. Additionally, the underground storage tanks associated with the fuel dispensers must be permitted through the Department of Environmental Health and Quality (DEHQ) which includes permitting in accordance with the underground storage tank program. As stated in responses 10(a) and 10(b) above, implementation of BMPs and compliance with required ordinances will ensure that project impacts are less than significant. Further information related to groundwater usage can be found in response 10(d).

As previously discussed, the GPU EIR determine significant and unavoidable impacts to water quality standards and requirements and groundwater supplies and recharge. However, the proposed Project would have a less than significant impact with mitigation to water quality standards and requirements, and groundwater supplies and recharge (Hyd-1.2 through Hyd-1.5). Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

10(d) The GPU EIR concluded this impact to be significant and unavoidable. The Project does not propose the use of groundwater and would obtain water service from Rainbow Municipal Water District as detailed in Service Availability Forms dated December 2020. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

10(e) The GPU EIR concluded this impact to be less than significant with mitigation. As outlined in the project's SWQMP and in responses 10(a) and 10(b), the Project will implement temporary/construction BMP's to reduce potential pollutants, including sediment from erosion or siltation, to the maximum extent practicable from entering storm water runoff and will ensure that project impacts are less than significant. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export. The Project site contains existing commercial uses on a site that was previously grading in the 1970s. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining

walls in order to support the slopes in the rear of the property. Additionally, a Hydrology/Hydraulics study dated April 26, 2024 was prepared by Excel Engineering that demonstrates that the Project does not increase off-site drainage or flow of water. Therefore, the project will not substantially alter the existing drainage pattern of the area and would be consistent with the analysis provided within the GPU EIR.

As previously discussed, the GPU EIR determined significant and unavoidable impacts to erosion or siltation and less than significant impacts. However, the proposed Project would have a less-than-significant impact to erosion or siltation (Hyd-1.2 through Hyd-1.5). Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 10(f) The GPU EIR concluded this impact to be less than significant with mitigation. As outlined in the project's SWQMP and in responses 10(a) and 10(b), the project will implement temporary/construction BMP's to reduce potential pollutants, including sediment from erosion or siltation, to the maximum extent practicable from entering storm water runoff and will ensure that project impacts are less than significant. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export. The Project site contains existing commercial uses on a site that was previously graded in the 1970s. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property. Additionally, a Hydrology/Hydraulics study dated April 26, 2024 was prepared by Excel Engineering that demonstrates that the Project does not increase off-site drainage or flow of water. The project does not propose construction within a 100-year floodplain as the Project does not contain a 100-year floodplain. Therefore, the project will not substantially alter the existing drainage pattern of the area and would be consistent with the analysis provided within the GPU EIR.

As previously discussed, the GPU EIR determined to be less than significant with mitigation. However, the proposed Project would have a less-than-significant impact to erosion or siltation (Hyd-1.2 through Hyd-1.5). Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 10(g) The GPU EIR concluded this impact to be less than significant with mitigation. As outlined in the project's SWQMP and in responses 10(a) and 10(b), the project will implement temporary/construction BMP's to reduce potential pollutants, including sediment from erosion or siltation, to the maximum extent practicable from entering storm water runoff and will ensure that project impacts are less than significant. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export. The Project site contains existing commercial uses on a site that was previously grading in the 1970s. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property. Additionally, a Hydrology/Hydraulics study dated April 26, 2024 was prepared by Excel Engineering that demonstrates that the Project does not increase off-site drainage or flow of water. With mitigation, the proposed Project would have a less-than-significant impact with regards to exceeding the capacity of stormwater systems with consistency with measures (Hyd-1.2 through Hyd-1.5). Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 10(h) The GPU EIR concluded this impact to be significant and unavoidable. Potential pollutant sources associated with the Project are trash enclosures, fuel dispensing areas, and runoff of pollutants caused by vehicles in a parking lot. The fuel dispensing areas are covered by a canopy and will not have permeable floors. Additionally, the underground storage tanks associated with the fuel dispensers must be permitted through the Department of Environmental Health and Quality (DEHQ) which includes permitting in accordance with the underground storage tank program. The trash enclosure is required to be routinely maintained and include a cover in order to limit pollutants. As stated in responses 10(a) and 10(b) above, implementation of BMPs and compliance with required ordinances will ensure that project impacts are less than significant.

As previously discussed, the GPU EIR determined impacts to water quality standards and requirements as significant and unavoidable. However, the proposed Project would have a less-than-significant impact to water quality standards with the implementation of project conditions listed in 10(a). The conditions are consistent with the GPU EIR mitigation measures Hyd-1.2 through Hyd-1.5. Therefore, the Project would not be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 10(i) The GPU EIR concluded this impact to be less than significant with mitigation. The Project does not propose any housing as it consists of remodeling and expansion of an existing commercial plaza. The Project site does not contain a 100-year floodplain or a floodway. As previously discussed, the GPU EIR determined impacts from housing within a 100-year flood hazard area as less than significant with mitigation. The proposed Project would have a less-than-significant impact for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.
- 10(j) The GPU EIR concluded this impact to be less than significant with mitigation. The Project site does not contain a 100-year floodplain. Additionally, the Project does not involve structures designed for residential occupancy. Because the project does not propose any construction within the limits of the 100-year floodplain of the property and will not substantially alter drainage patterns as outlined in the Hydrology/Hydraulics Study dated April 26, 2024, the project will not impede or redirect flood flows. Therefore, no structures would be placed within a 100-year flood hazard area which would impede or redirect flood flows.
- 10(k) The GPU EIR concluded this impact to be less than significant with mitigation. Proposed construction associated with the project lies outside any identified special flood hazard area. The project consists of commercial development and does not propose residential uses. Additional information related to the project's analysis associated with flood hazards can be found in response 10(j). The proposed Project would have a less than significant impact for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.
- 10(l) The GPU EIR concluded this impact to be less than significant with mitigation. The County Office of Emergency Services maintains Dam Evacuation Plans for each dam operational area. These plans contain information concerning the physical situation, affected jurisdictions, evacuation routes, unique institutions, and event responses. If a "unique institution" is proposed, such as a hospital, school, or retirement home, within dam



inundation area, an amendment to the Dam Evacuation Plan would be required. As previously discussed in response 10(j), the project site lies outside a mapped dam inundation area for a major dam/reservoir within San Diego County.

10(m) The GPU EIR concluded this impact to be less than significant with mitigation.

10(m)(i) SEICHE: The project site is not located along the shoreline of a lake or reservoir.

10(m)(ii) TSUNAMI: The project site is not located in a tsunami hazard zone.

10(m)(iii) MUDFLOW: Mudflow is type of landslide. See response to question 6(a)(iv).

**Conclusion**

With regards to the issue area of Hydrology and Water Quality, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. Feasible mitigation measures contained within the GPU EIR (Hyd-1.2 through Hyd-1.5) would be applied to the Project. The mitigation measures, as detailed above, requires the Project applicant to comply with Watershed Protection Ordinance, Stormwater Standards Manual/BMP Design Manual, Groundwater Ordinance, and Guidelines for Determining Significance for Hydrology and Water Quality.

Significant Project Impact	Impact not identified by GPU EIR	Substantial New Information
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**11. Land Use and Planning – Would the Project:**

- |   |                          |                          |                          |
|---|--------------------------|--------------------------|--------------------------|
| a) Physically divide an established community?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

11(a) The GPU EIR concluded this impact to be less than significant with mitigation. The Project consists of expanding and remodeling an existing commercial plaza. The Project does not propose the introduction of new infrastructure such as major roadways, water supply systems, or utilities to the area. Improvements associated with infrastructure of the site include on-site utility lines and fire hydrants, entry improvements to two existing commercial driveways, and frontage improvements to Via Belmonte. Additionally, build-out of this site was anticipated in the GPU EIR and GPU EIR mitigation measures Lan-1.1 through Lan-1.3 requiring coordination efforts for roadway widening and improvements to ensure that development of the site would not divide an established community. As

previously discussed, the GPU EIR determined impacts from physically dividing an established community as less than significant with mitigation. However, the proposed Project would have a less-than-significant impact for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 11(b) The GPU EIR concluded this impact to be less than significant. The proposed Project is subject to the General Plan Semi-Rural Regional Category and General Commercial (C-1) Land Use Designation. The project is also subject to the Fallbrook Community Plan Policies and I-15 Design Review Corridor Guidelines. The property is subject to the General Commercial Land Use Designation (C36) which permits the proposed uses of the site in accordance with Sections 2360 through 2363 of the Zoning Ordinance. The site is also subject a "B" Special Area Designator for community design review which requires the processing of a Site Plan permit. The Project site is subject to the "G" Height Designator in the Zoning Ordinance which requires structures to be a maximum height of 35-feet. The remodel of the market will result in the structure to reach a maximum height of 26 feet. The gas station convenience store, retail store structures, and gas station canopy are proposed to be approximately 27 to 32 feet in height. All structures are compliant with the 35-foot "G" Height Designator of the property. The existing visual character of views along Old Highway 395 and Interstate 15 consist of residences along hillsides, vacant land, and incidental non-residential uses such as commercial structures. The Project consists of redeveloping an existing commercial plaza which contains structures that have been on the subject property for over 40 years. The commercial structures are an expected visual feature within the community. The remodeled architectural design of the market is intended to be in conformance with the proposed new structures and uses of the property. The architectural design of the structures includes earth-tone colors as well as natural materials such as stone veneers. The Project is conditioned to require verification of adequate parking and signage in conformance with the Zoning Ordinance standards in order to ensure that the site has adequate parking and signage consistent with the Zoning Ordinance in the event that commercial tenants change. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties as well as prevent the production of noise in excess of levels beyond adopted ordinances. Therefore, the project would not conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect, including policies of the General Plan and Community Plan. Therefore, the project would not conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project.

As previously discussed, the GPU EIR determined impacts to conflicts with land use plans, policies, and regulations as less than significant. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

## **Conclusion**

With regards to the issue area of Land Use and Planning, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.

- 3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
- 4. Feasible mitigation measures contained within the GPU EIR will be applied to the project.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**12. Mineral Resources** – Would the Project:

- |   |                          |                          |                          |
|---|--------------------------|--------------------------|--------------------------|
| a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?                                | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

12(a) The GPU EIR determined that impacts to mineral resources would be significant and unavoidable. The California Surface Mining and Reclamation Act (SMARA) required classification of land into Mineral Resource Zones (MRZs). The Project site has not been classified by the California Department of Conservation – Division of Mines and Geology (Update of Mineral Land Classification: Aggregate Materials in the Western San Diego Production-Consumption Region, 1997) as an area for mineral resources. The Project site is primarily classified as a developed and commercially zoned land adjacent to commercial and residential uses. The Project site is also directly adjacent to Interstate 15 and near the State Route 76 and Interstate 15 Interchange. A future mining operation at the Project site would likely create a significant impact to neighboring properties for issues such as noise, air quality, traffic, and possibly other impacts. Additionally, the Project site is less than 5 acres and in between a highway and residential uses and it would be unlikely to support a successful mining operation. Therefore, implementation of the Project will not result in the loss of availability of a known mineral resource that would be of value since the mineral resource has already been lost due to incompatible land uses.

As previously discussed, the GPU EIR determined impacts to mineral resources to be significant and unavoidable. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

12(b) The GPU EIR concluded this impact to be significant and unavoidable. The project site is not located in an Extractive Use Zone (S-82), nor does it have an Impact Sensitive Land Use Designation (24) with an Extractive Land Use Overlay (25). The Project site is not located in an area that has MRZ-2 designated lands, but the Project site is within 1,300 feet of lands designated as MRZ-2. As detailed in response (a), the Project site is primarily classified as a developed and commercially zoned land adjacent to commercial and residential uses. The Project site is also directly adjacent to Interstate 15 and near the State Route 76 and Interstate 15 Interchange. The majority of MRZ-2 designated lands near the Project site are developed with existing uses such as Interstate 15 or within floodway/floodplains near the San Luis Rey River. The Project site is previously developed

and contains existing commercial uses and will not be introducing new uses to an area that does not previously contain uses that are not compatible with a potential mine or extractive use. Therefore, no potentially significant loss of availability of a known mineral resource would occur as a result of the project.

As previously discussed, the GPU EIR determined impacts to mineral resources to be significant and unavoidable. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Mineral Resources, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. The GPU EIR concluded significant and unavoidable impacts to mineral resources, however, the Project would have less than significant impacts for the reasons detailed above. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

	<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
<b>13. Noise</b> – Would the Project:			
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

**Discussion**

13(a) The GPU EIR concluded this impact to be less than significant with mitigation.

The area surrounding the project site consists of commercial and residential uses. The project will not expose people to potentially significant noise levels that exceed the allowable limits of the General Plan, Noise Ordinance, or other applicable standards for the following reasons:

General Plan – Noise Element: Policy 4b addresses noise sensitive areas and requires projects to comply with a Community Noise Equivalent Level (CNEL) of 60 decibels (dBA). Projects which could produce noise in excess of 60 dB(A) are required to incorporate design measures or mitigation as necessary to comply with the Noise Element. Based on a review of the County’s noise contour maps, the Project is located within noise contours identified in the noise element of the General Plan ranging 60 CNEL or more due to the proximity of the Project site to Interstate 15 and Old Highway 395. The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along I-15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near I-15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. The additional trips added by the Project are anticipated to be a minimal increase compared to the existing traffic on Old Highway 395 and Interstate 15. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the western property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County’s noise standards and standards identified in the Noise Element of the General Plan.

Noise Ordinance – Section 36-404: Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project’s property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject

to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. The Project consists of redevelopment of an existing commercial center. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles as previously discussed in the General Plan – Noise Element analysis. It should be noted that the residential zones along the western property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Therefore, the project complies with Section 36.404 of the Noise Ordinance.

Noise Ordinance – Section 36-410: The project will not generate construction noise in excess of Noise Ordinance standards. Construction operations will occur only during permitted hours of operation. The project does not propose the use of blasting during construction activities. Also, it is not anticipated that the Project will require the operation of construction equipment in excess of an average sound level of 75 dB between the hours of 7 AM and 10 PM. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property. The Project will also implement standard good practice recommendations that are conditions of the Project such as:

- a. Turn off equipment when not in use.
- b. Equipment used in construction should be maintained in proper operating condition,  
and all loads should be properly secured, to prevent rattling and banging.
- c. Use equipment with effective mufflers.
- d. Minimize the use of backup alarms.

As previously discussed, the GPU EIR determined impacts from excessive noise levels to be less than significant with mitigation. The Project would have a less-than-significant impact with the incorporation of design features and conditions. Therefore, the Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 13(b) The GPU EIR concluded this impact to be less than significant with mitigation. The proposed project does not propose residential occupancy or introduction of sensitive receptors to groundborne noise or vibration, nor does the project propose any major, new, or expanded infrastructure such as highways, or intensive extractive industry that could generate excessive groundborne vibration or groundborne noise levels. Therefore, the project will not expose persons to or generate excessive groundborne vibration or groundborne noise levels on a project or cumulative level. As previously discussed, the GPU EIR determined impacts to excessive groundborne vibration as less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 13(c) The GPU EIR concluded this impact to be less than significant with mitigation. As indicated in the response listed under Section 13(a), the project would not expose existing or planned noise sensitive areas in the vicinity to a substantial permanent increase in noise levels that exceed the allowable limits of any applicable noise standards. Also, the project is not expected to expose existing or planned noise sensitive areas to noise 10 dB CNEL over existing ambient noise levels.

As previously discussed, the GPU EIR determined impacts to permanent increase in ambient noise levels as less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 13(d) The GPU EIR concluded this impact to be less than significant with mitigation. As indicated in the response listed under Section 13(a), the project does not involve any operational uses that may create substantial temporary or periodic increases in ambient noise levels in the project vicinity. In addition, general construction noise is not expected to exceed the construction noise limits of the County of San Diego Noise Ordinance (Section 36.409), which are derived from State regulations to address human health and quality of life concerns. Construction operations will occur only during permitted hours of operation. Also, the project will not operate construction equipment in excess of 75 dB for more than 8 hours during a 24-hour period. Therefore, the project would not result in a substantial temporary or periodic increase in existing ambient noise levels in the project vicinity.

As previously discussed, the GPU EIR determined impacts to temporary or permanent increase in ambient noise levels as less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 13(e) The GPU EIR concluded this impact to be less than significant with mitigation. The project is not located within an Airport Land Use Compatibility Plan (ALUCP) or within 2 miles of a public airport or public use airport. The Project site is located approximately a half of a mile away from a runway that is used for model, remote-controlled airplanes. However, the existing model airplane runway will be removed upon the development of the approved Campus Park West project located on the same property as the model airplane runway. Additionally, the Project does not propose uses for residential occupancy that would be subject to noise standards when located near a public airport.

As previously discussed, the GPU EIR determined impacts from excessive noise exposure from a public or private airport as less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 13(f) The GPU EIR concluded this impact to be less than significant with mitigation. The project is not located within a one-mile vicinity of a private airstrip. The Project site is located approximately a half of a mile away from a runway that is used for model, remote-controlled airplanes. However, the existing model airplane runway will be removed upon the development of the approved Campus Park West project located on the same property as the model airplane runway. Additionally, the Project consists of expanding and

remodeling an existing commercial center and does not consist of residential uses for occupancy. As previously discussed, the GPU EIR determined impacts from excessive noise exposure from a public or private airport as less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Noise, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant. Therefore, the Project would not result in an impact which was not adequately evaluated by the GPU EIR.

Significant Project Impact	Impact not identified by GPU EIR	Substantial New Information
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**14. Population and Housing – Would the Project:**

- |   |                          |                          |                          |
|---|--------------------------|--------------------------|--------------------------|
| a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

14(a) The GPU EIR concluded this impact to be less than significant. The project site is subject to the General Commercial (C-1) Land Use Designation and the Zoning Use Regulation is General Commercial (C36) which are intended for commercial uses. The project will not induce substantial population growth in an area because the project does not propose any physical or regulatory change that would remove a restriction to or encourage population growth in an area. The Project does not include an increase in population as it consists of a commercial use and includes remodeling and expansion of an existing commercial plaza. All roadway improvements associated with the Project consists of minor widening to an existing road located on the northern portion of the Project site and access improvements to existing driveways.

As previously discussed, the GPU EIR determined impacts from population growth to be less than significant. As the project would have a less than significant impact for the



reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 14(b) The GPU EIR concluded this impact to be less than significant. The Project will not displace existing housing as the Project involves remodeling and expansion of an existing commercial plaza on a property containing existing commercial uses that does not have any existing residential uses. No occupied residential structures or housing are proposed to be removed as part of the project.

As previously discussed, the GPU EIR determined impacts from displacement of housing to be less than significant. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 14(c) The GPU EIR concluded this impact to be less than significant. As indicated in response 14(b), the project will not displace existing housing as the project involves remodeling and expansion of an existing commercial plaza on a property containing existing commercial uses that does not have any existing residential uses. No occupied residential structures or housing are proposed to be removed as part of the project.

As previously discussed, the GPU EIR determined impacts from displacement of people to be less than significant. As the project would have a less than significant impact for the reasons detailed above, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Population and Housing, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because project specific impacts would be less than significant.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**15. Public Services – Would the Project:**

a) Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance service ratios for fire protection, police protection, schools, parks, or other public facilities?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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**Discussion**

15(a) The GPU EIR concluded this impact to be less than significant with mitigation for the exception of school services, which remained significant and unavoidable. The Project consists of the expansion and remodeling of an existing commercial plaza. Based on service availability forms from Rainbow Municipal Water District and North County Fire Protection District dated December 2020, the Project has adequate and available service for water, fire, and sewer services. The Project site is within the North County Fire Protection District and is located approximately half of a mile south from the nearest fire station. Based on a review by County Staff of GIS Aerial Imagery and the Fire Service Availability Form dated July 2020, the site would have an Emergency Response Travel Time of 0 to 5 minutes, which meets the General Plan Safety Element standard for lands designated as Commercial within the Semi-Rural Regional Category of 10 minutes. The Project does not include construction of new or altered public service facilities including but not limited to fire protection facilities, sheriff facilities, schools, or parks in order to maintain acceptable service ratios, response times, or other performance objects for public services. The proposed development is consistent with the General Plan projections and Land Use regulations, therefore, service ratios for public services associated with the project were analyzed within the GPU EIR and the Project is not anticipated to require additional services. Based on the project’s service availability forms, the project would not result in the need for significantly altered services or facilities.

**Conclusion**

With regards to the issue area of Public Services, the following findings can be made:

1. No peculiar impacts to the project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. Feasible mitigation measures from the GPU EIR (Pub-3.2) would be applied to the project. This mitigation measure, as detailed above, requires the project to conform to Board Policy I-84, which requires project Facility Availability and Commitment for Public Sewer, Water, School and Fire Services.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**16. Recreation – Would the Project:**

a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
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b) Does the project include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------	--------------------------

**Discussion**

16(a) The GPU EIR concluded this impact to be less than significant with mitigation. The Project does not propose any residential uses, including but not limited to a residential subdivision, mobile home park, or construction for a single-family residence that may increase the use of existing neighborhood and regional parks or other recreational facilities in the vicinity. The Project includes the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The existing decomposed granite pathway along the frontage of the Project site in accordance with the Community Trails Master Plan will be retained and along the frontage of Old Highway 395 with the minor widening. No impact to parks or recreation facilities would occur as a result of the Project as it consists of commercial uses.

As previously discussed, the GPU EIR determined impacts related to deterioration of parks and recreational facilities to be less than significant with mitigation. As the proposed Project would have a less than significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

16(b) The GPU EIR concluded this impact to be less than significant with mitigation. The Project does not include recreational facilities or require the construction or expansion of recreational facilities such as parks. As previously discussed, the GPU EIR determined impacts related to construction of new recreational facilities to be less than significant. As the proposed Project would have a less than significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Recreation, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**17. Transportation and Traffic** – Would the Project:

a) Conflict with an applicable plan, ordinance or policy establishing measures of the effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets,

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------	--------------------------

highways and freeways, pedestrian and bicycle paths and mass transit?

b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?

c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?

d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

e) Result in inadequate emergency access?

f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?

**Discussion**

17(a) The GPU EIR concluded this impact to be significant and unavoidable. The County of San Diego Transportation Study Guidelines have been adopted by the County Board of Supervisors in September of 2022 to address Senate Bill 743 (SB 743). SB 743 changed the way that public agencies evaluate transportation impacts under CEQA. A key element of this law is the elimination of using auto delay, Level of Service (LOS), and other similar measures of vehicular capacity or traffic congestion as a basis for determining significant transportation impacts under CEQA. The new established criteria for determining the significance of transportation impacts is Vehicle Miles Traveled (VMT) and is further addressed below. Although no longer utilized as the standard for evaluating transportation impacts under CEQA, the County’s General Plan identified LOS as being a required analysis per Policy M-2.1 and is therefore also addressed.

Section 15064.3 of the CEQA Guidelines details new regulations, effective statewide July 1, 2020, based on SB 743 that sets forth specific considerations for evaluating a project’s transportation impacts. As previously discussed, the new established criteria for determining the significance of transportation impacts is Vehicle Miles Traveled (VMT). VMT refers to the amount and distance of automobile travel attributable to a project. The Project consists of remodeling and expanding an existing commercial plaza. The Project site contains an existing market with attached businesses consisting of restaurants and personal services such as financial and insurance companies. The Project site also contains an existing hamburger restaurant as well as a parking lot. The existing market building with attached businesses will be remodeled with architectural changes resulting in an approximately 9,075 square foot structure. The existing hamburger restaurant will be retained. Additionally, a new gas station with 12 fueling stations and an approximately 4,980 square foot convenience store as well as two new approximately 6,000 square foot retail buildings are proposed. In accordance with a Local Mobility Analysis prepared for

the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along I-15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near I-15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technically Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally-serving commercial uses.

The project is demonstrating conformance with CEQA in accordance with CEQA Section 15183. The General Plan EIR contains implementation of the Transportation Impact Fee (TIF) as an overall programmatic solution that addresses existing and projected future road deficiencies in the unincorporated portion of San Diego County. The TIF program creates a mechanism to proportionally fund improvements to roadways necessary to mitigate potential cumulative impacts caused by traffic from future development. The potential growth represented by this project was included in the growth projections upon which the TIF program is based. The TIF measure was identified by the GPU EIR as Tra-1.7.

A Local Mobility Analysis is the tool utilized by the Transportation Study Guidelines to assess projects impacts to LOS. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project because it generates more than 250 average daily trips. The Local Mobility Analysis evaluated operations of nearby road segments and intersections within the Project vicinity. Based on the Local Mobility Analysis, all existing nearby road segments and intersections within the Project vicinity operate at Levels of Service of C or D or better. The Local Mobility Analysis modeled the Levels of Service of segments and intersections in the area with the addition of the Project and concluded that all segments and intersections will continue to operate at a Level of Service of C or D or better. The Local Mobility Analysis recommends the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The existing decomposed granite pathway along the frontage of the Project site in accordance with the Community Trails Master Plan will be retained along the frontage of Old Highway 395 with the minor widening. The Project also includes half width improvements to Via Belmonte including a half width improvement of 14 feet and a graded half width of twenty feet. The Project is conditioned to maintain adequate sight distance along the driveway entrances of the Project throughout the life of the Project. The Project is conditioned to verify adequate sight distance at various points of construction in order to ensure that items such as landscaping or fencing are not installed in sight lines that would prevent sight distance. Lastly, the Project also includes a vacation and/or removal and re-dedication of road, drainage, and slope easements related to maintenance along Old Highway 395. Therefore, the project would not conflict with an applicable plan, ordinance or policy related to local mobility.

As previously discussed, the GPU EIR determined significant and unavoidable impacts to unincorporated County traffic and LOS standards. However, the project would have a less than significant impact to County traffic and LOS standards as well as VMT with the incorporation of mitigation as detailed above. The measures were identified in the GPU

EIR as Tra-1.7 and Tra-6.9 which require payment into the County TIF program as well as implementation of the County Subdivision Ordinance and the Community Trails Master Plan. Therefore, the project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 17(b) The GPU EIR concluded this impact to be significant and unavoidable. The designated congestion management agency for the County is the San Diego Association of governments (SANDAG). In October 2009, the San Diego region elected to be exempt from the State CMP and, since this decision, SANDAG has been abiding by 23 CFR 450.320 to ensure the region's continued compliance with the federal congestion management process.

As previously stated, Section 15064.3 of the CEQA Guidelines details new regulations, effective statewide July 1, 2020 that sets forth specific considerations for evaluating a project's transportation impacts. As discussed in 17(a), the project would result in a less than significant impact associated with VMT. As discussed in 17(a), the Project is conditioned for widening and restriping of Old Highway 395 to accommodate a center turn lane, frontage improvements along Via Belmonte, and maintaining adequate sight distance at the Project site driveways that have been reviewed by the County of San Diego Department of Public Works. Therefore, the project would not conflict with an applicable congestion management program.

As previously discussed, the GPU EIR determined impacts on significant and unavoidable impacts to unincorporated County traffic and LOS standards. However, the project would have a less than significant impact for the reasons detailed above. Therefore, the project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 17(c) The GPU EIR concluded this impact to be less than significant with mitigation. The Project site is not located within an Airport Influence Area, Airport Safety Zone, Airport Land Use Compatibility Plan Area, Avigation Easement, or Overflight Area. The Project site is located approximately a half of a mile away from a runway that is used for model, remote-controlled airplanes. However, the existing model airplane runway will be removed upon the development of the approved Campus Park West project located on the same property as the model airplane runway. Therefore, the Project would have a less than significant impact to air traffic patterns. The Project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 17(d) The GPU EIR concluded this impact to be significant and unavoidable. The proposed Project would not substantially alter traffic patterns, roadway design, place incompatible uses (e.g., farm equipment) on existing roadways, or create curves, slopes or walls which would impede adequate sight distance on a road. As discussed in response 17(a), the Project includes minor frontage improvements along Via Belmonte, the addition of a center turn lane and widening of Old Highway 395, and maintenance of adequate sight distance along Old Highway 395. Therefore, the proposed project will not alter traffic patterns, roadway design, place incompatible uses (e.g., farm equipment) on existing roadways, or create curves, slopes or walls which would impede adequate sight distance on a road.

As previously discussed, the GPU EIR determined impacts on rural road safety to be significant and unavoidable. However, the Project would have a less-than-significant impact with no mitigation required for the reasons detailed above. Therefore, the project

would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 17(e) The GPU EIR concluded this impact to be less than significant with mitigation. The proposed Project will not result in inadequate emergency access. The project is not served by a dead-end road that exceeds the maximum cumulative length permitted by the San Diego County Consolidated Fire Code. The Project is required to maintain adequate width of drive aisles between parking spaces within the on-site parking lot in accordance with the San Diego County Consolidated Fire Code and the requirements of North County Fire Protection District. In addition, consistent with GPU EIR mitigation measure Tra-4.2, the Project would implement the Building and Fire codes to ensure emergency vehicle accessibility.

As previously discussed, the GPU EIR determined impacts on emergency access as less than significant with mitigation. As the Project would have a less-than-significant impact for the reasons detailed above and is consistent with GPU EIR Mitigation Measure Tra-4.2, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 17(f) The GPU EIR concluded this impact to be less than significant with mitigation. The Project would not result in the construction of any road improvements or new road design features that would interfere with the provision of public transit, bicycle or pedestrian facilities. The Project includes widening of Old Highway 395 in order to accommodate a center turn lane and the geometrics and cross-sections detailed in the Local Mobility Analysis demonstrate that the Project will not prevent bike lanes from being constructed or the removal of existing decomposed granite pathways. All improvements and widening associated with the Project are detailed in responses 17(a) and 17(b).

As previously discussed, the GPU EIR determined impacts on alternative transportation and rural safety as less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

## Conclusion

With regards to the issue area of Transportation and Traffic, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
5. Feasible measures contained within the GPU EIR (Tra-1.4, Tra-1.7, and Tra-4.2, and) would be applied to the project. The mitigation measures, as detailed above, would require payment into the County TIF Program as well as consistency with the Building Code, Fire Code, and County Public Road Standards.

<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
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**18. Utilities and Service Systems** – Would the Project:

- |  |                          |                          |                          |
|--|--------------------------|--------------------------|--------------------------|
| a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?                             | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?                                      | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| e) Result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| g) Comply with federal, state, and local statutes and regulations related to solid waste?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

**Discussion**

18(a) The GPU EIR concluded this impact to be less than significant with mitigation. The Project would discharge domestic waste to a community sewer system that is permitted to operate by the Regional Water Quality Control Board (RWQCB). A service availability letter dated December 2020 from Rainbow Municipal Water District identifies that there is adequate service for operations of the Project for disposal of wastewater associated with the Project.

As previously discussed, the GPU EIR determined impacts on wastewater treatment requirements to be less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

18(b) The GPU EIR concluded this impact to be less than significant with mitigation. The Project involves new water and wastewater pipeline extensions. However, these extensions would be on-site, and would not result in additional adverse physical effects beyond those already identified in other sections of this environmental analysis. Water and sewer service



availability forms have been provided by Rainbow Municipal Water District dated December 2020.

As previously discussed, the GPU EIR determined impacts on wastewater treatment requirements to be less than significant with mitigation. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 18(c) The GPU EIR concluded this impact to be less than significant with mitigation. As outlined in the project's SWQMP and in responses in Section 10 – Hydrology and Water Quality, the Project will implement on-site temporary/construction BMP's to reduce potential pollutants, including sediment from erosion or siltation, to the maximum extent practicable from entering storm water runoff and will ensure that project impacts are less than significant. The proposed on-site biofiltration basin and other BMP's are required to be maintained by the property owner. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export. The Project site contains existing commercial uses on a site that was previously graded in the 1970s. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property. Additionally, a Hydrology/Hydraulics study dated April 26, 2024 was prepared by Excel Engineering that demonstrates that the Project does not increase off-site drainage or flow of water.

As previously discussed, the GPU EIR determined impacts on sufficient stormwater drainage facilities to be less than significant. As the proposed Project would have a less-than-significant impact for the reasons detailed above, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 18(d) The GPU EIR concluded this impact to be significant and unavoidable. A water service availability letter dated December 2020 has been provided by the Rainbow Municipal Water District for operations of the project and show that there is adequate service for the Project. The Project does not include the use of groundwater.

As previously discussed, the GPU EIR determined impacts to adequate water supplies be significant and unavoidable. However, the proposed Project would have a less-than-significant impact with no required mitigation for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 18(f) The GPU EIR concluded this impact to be less than significant. All solid waste facilities, including landfills require solid waste facility permits to operate. There are five, permitted active landfills in San Diego County with remaining capacity to adequately serve the project. Therefore, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 18(g) The GPU EIR concluded this impact to be less than significant. The project would deposit all solid waste at a permitted solid waste facility. Therefore, the project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

**Conclusion**

With regards to the issue area of Utilities and Service Systems, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. No mitigation measures contained within the GPU EIR would be required because Project specific impacts would be less than significant.

	<b>Significant Project Impact</b>	<b>Impact not identified by GPU EIR</b>	<b>Substantial New Information</b>
<b>19. Wildfire</b> – If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the Project:			
a) Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts in the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Expose people or structures to significant risk, including downslopes or downstream flooding or landslides, as a result of runoff, post-fire instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Discussion**

Wildfire was analyzed within the GPU EIR within Section 2.7, Hazards and Hazardous Materials. The guidelines for determining significance stated: the proposed General Plan Update would have a significant impact if it would expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands. In 2019, the issue of Wildfire was separated into its own section within Appendix G of the CEQA Guidelines to incorporate the four issue questions above. The GPU EIR did address these issues within the analysis; however they were not called out as separate issue areas. Within the GPU EIR, the issue of Wildland Fires was determined to be significant and unavoidable.

- 19(a) The GPU EIR concluded this impact to be significant and unavoidable. The site is located within a very high fire hazard severity zone (FHSZ). The Project site is within the North County Fire Protection District and is located approximately half of a mile south from the nearest fire station. Based on a review by County Staff of GIS Aerial Imagery and the Fire Service Availability Form dated July 2020, the site would have an Emergency Response Travel Time of 0 to 5 minutes, which meets the General Plan Safety Element standard for lands designated as Commercial within the Semi-Rural Regional Category of 10 minutes.

As previously stated, Wildfire was analyzed within the GPU EIR within Section 2.7, Hazards and Hazardous Materials and was determined to be significant and unavoidable. However, the proposed Project would have a less-than-significant impact for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 19(b) The GPU EIR concluded this impact to be significant and unavoidable. The Project is within a very high fire hazard severity zone and within the Urban-Wildlife Interface Zone. The Project site is primarily developed and surrounded by development including residential structures as well as roads and highways such as Old Highway 395 and Interstate 15. The Project consists of remodeling and expanding an existing commercial plaza. The Project is conditioned to comply with fire code requirements such as maintaining adequate width for fire access within the on-site parking lot as well as install fire hydrants. The Project would comply with regulations relating to emergency access, water supply, and defensible space specified in the County Fire Code and Consolidated Fire Code. Implementation of these fire safety standards would occur during the building permit process and is consistent with GPU mitigation measures Haz-4.2 and Haz-4.3. In addition, the Project is consistent with the Zoning Ordinance and the County of San Diego General Plan. Therefore, for the reasons stated above, the Project would not be expected to experience exacerbated wildfire risks due to slope, prevailing, winds or other factors.

As previously stated, Wildfire was analyzed within the GPU EIR within Section 2.7, Hazards and Hazardous Materials and was determined to be significant and unavoidable. However, the proposed Project would have a less-than-significant impact for the reasons detailed above. Therefore, the Project would be consistent with the analysis provided within the GPU EIR because it would not increase impacts identified within the GPU EIR.

- 19(c) The GPU EIR concluded this impact to be significant and unavoidable. The GPU EIR concluded this impact to be significant and unavoidable. The Project consists of expanding and remodeling an existing commercial plaza. The Project will result in minor road widening to Via Belmonte on the northern portion of the Project site as well as installation of fire hydrants. The Project will also result in the construction of a gas station with a convenience store which includes flammable materials stored in underground storage tanks. Fuels associated with the operation of the gas station must be permitted through the Department of Environmental Health and Quality Hazardous Materials Division through a hazardous materials business plan and permits for underground storage tanks. Underground storage tanks require maintenance and inspections in order to minimize any leaks of fuel and prevent potential risk of fires. No major road improvements or extensions of roadways are required for the construction and operation of the use. See Section 9 – Hazards and Hazardous Materials for additional responses and analysis associated with installation of infrastructure or structures that may exacerbate fire risk. Therefore, no additional temporary or ongoing impacts to the environment related to associated infrastructure would occur that have not been analyzed in other sections of this environmental document.

19(d) The GPU EIR concluded this impact to be significant and unavoidable. As stated in response 10(f), a Hydrology/Hydraulics study dated April 26, 2024 was prepared by Excel Engineering that demonstrates that the Project does not increase off-site drainage or flow of water. Proposed earthwork quantities for the project consist of approximately 7,725 cubic yards of cut, 5,575 cubic yards of fill, and 2,150 cubic yards of export. The Project site contains existing commercial uses on a site that was previously graded in the 1970s. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property. In addition, as stated in responses 10(i) and 10(j), construction associated with the project will not be within a 100-year flood hazard area, floodway, or floodplain and would not be impacted from downstream flooding. Therefore, the Project would not expose people or structures to a significant risk, including downslopes or downstream flooding or landslides, as a result of runoff, post-fire instability, or drainage changes.

As previously discussed, the GPU EIR determined impacts from Wildfire to be significant and unavoidable. The GPU EIR measure Haz-4.3 requires compliance with the Building and Fire Code and the project has incorporated the GPU EIR Mitigation Measure Haz-4.2 for brush management as a standard project design feature. Therefore, the project would be consistent with the analysis within the GPU EIR because it would not increase impacts identified within the GPU EIR.

### **Conclusion**

The GPU EIR concluded significant and unavoidable impacts associated with wildfire under Section 2.7, Hazards and Hazardous Materials. Based on the incorporation of project design features measures, impacts associated with wildfire would be less than significant. Therefore, the project would not exacerbate wildfire risks and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire.

With regards to the issue area of Wildfire, the following findings can be made:

1. No peculiar impacts to the Project or its site have been identified.
2. There are no potentially significant off-site and/or cumulative impacts which were not discussed by the GPU EIR.
3. No substantial new information has been identified which results in an impact which is more severe than anticipated by the GPU EIR.
4. Feasible mitigation measures incorporated as standard design features associated with the project contained within the GPU EIR (Haz-4.2 and Haz-4.3) would be applied to the Project. These measures, as detailed above, requires the Project applicant to implement brush management and comply with the building and fire codes.

### **Attachments:**

Appendix A – References

Appendix B – Summary of Determinations and Mitigation within the Final Environmental Impact Report, County of San Diego General Plan Update, SCH # 2002111067

## Appendix A

The following is a list of project specific technical studies used to support the analysis of each potential environmental effect:

### **Air Quality and Greenhouse Gas**

Pala Mesa Plaza – PDS2020-STP-20-021 – Greenhouse Gas Screening Letter San Diego County, Ldn Consulting, Inc. July 20, 2023

Air Quality Assessment, Pala Mesa Plaza PDS2020-STP-20-021, Commercial Development, County of San Diego, Ldn Consulting, Inc. July 17, 2023

Bay Area Air Quality Management District, Justification Report: CEQA Thresholds for Evaluating the Significance of Climate Impacts From Land Use Projects and Plans, Bay Area Air Quality Management District and Ascent Environmental, April 2022

### **Cultural Resources Report:**

Cultural Resource Survey of the Pala Mesa Plaza Project, Andrew R. Pignuolo, RPA, Laguna Mountain Environmental, Inc., September 2021

### **Hazards and Geology/Soils:**

Phase 1 Environmental Site Assessment, Geotek, Inc., May 4, 2020

Geotechnical and Infiltration Evaluation, Geotek, Inc. June 2, 2020

### **Hydrology/Water Quality:**

Priority Development Project (PDP) Stormwater Quality Management Plan (SWQMP), Excel Engineering, June 3, 2022

Hydrology/Hydraulics Study, Excel Engineering, April 26, 2024

### **Transportation & Traffic:**

Revised Local Mobility Analysis (LMA) and Vehicle Miles Traveled (VMT) for Old Highway 395 Retail Center, Darnell & Associates, March 4, 2022

### **Service Availability Forms:**

Project Facility Availability – Fire, December 2020

Project Facility Availability – Sewer, December 2020

Project Facility Availability – Water, December 2020

For a complete list of technical studies, references, and significance guidelines used to support the analysis of the General Plan Update Final Certified Program EIR, dated August 3, 2011, please visit the County's websites at:

[http://www.sdcounty.ca.gov/PDS/gpupdate/docs/BOS\\_Aug2011/EIR/FEIR\\_5.00\\_-\\_References\\_2011.pdf](http://www.sdcounty.ca.gov/PDS/gpupdate/docs/BOS_Aug2011/EIR/FEIR_5.00_-_References_2011.pdf)

<https://www.sandiegocounty.gov/content/sdc/pds/generalplan/GP-EIR.html#EIR>

## Appendix B

A Summary of Determinations and Mitigation within the Final Environmental Impact Report, County of San Diego General Plan Update, SCH # 2002111067 is available on the Planning and Development Services website at:

[http://www.sdcounty.ca.gov/pds/gpupdate/GPU\\_FEIR\\_Summary\\_15183\\_Reference.pdf](http://www.sdcounty.ca.gov/pds/gpupdate/GPU_FEIR_Summary_15183_Reference.pdf)

**REVIEW FOR APPLICABILITY OF/COMPLIANCE WITH  
ORDINANCES/POLICIES**

**FOR PURPOSES OF CONSIDERATION OF  
Pala Mesa Commercial  
PDS2020-STP-20-021; PDS2020-ER-20-02-003**

**October 17, 2024**

**I. HABITAT LOSS PERMIT ORDINANCE** – Does the proposed project conform to the Habitat Loss Permit/Coastal Sage Scrub Ordinance findings?

YES                      NO                      NOT APPLICABLE/EXEMPT  
                                           

The proposed Project and any off-site improvements are not located within the boundaries of the Multiple Species Conservation Program. However, the Project does not propose impacts to Coastal Sage Scrub. Therefore, conformance to the Habitat Loss Permit/Coastal Sage Scrub Ordinance findings is not required.

**II. MSCP/BMO** – Does the proposed project conform to the Multiple Species Conservation Program and Biological Mitigation Ordinance?

YES                      NO                      NOT APPLICABLE/EXEMPT  
                                           

The proposed Project and any off-site improvements related to the proposed Project are not located within the boundaries of the Multiple Species Conservation Program.

**III. GROUNDWATER ORDINANCE** – Does the project comply with the requirements of the San Diego County Groundwater Ordinance?

YES                      NO                      NOT APPLICABLE/EXEMPT  
                                           

As detailed in a water availability form dated December 2020, the Project will obtain its water supply from the Rainbow Municipal Water District. The Project will not use groundwater for construction or operations of the Project.

**IV. RESOURCE PROTECTION ORDINANCE** – Does the project comply with:

The wetland and wetland buffer regulations (Sections 86.604(a) and (b)) of the Resource Protection Ordinance?                      YES                      NO                      NOT APPLICABLE/EXEMPT

The Floodways and Floodplain Fringe section (Sections 86.604(c) and (d)) of the Resource Protection Ordinance?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>
The Steep Slope section (Section 86.604(e))?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>
The Sensitive Habitat Lands section (Section 86.604(f)) of the Resource Protection Ordinance?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>
The Significant Prehistoric and Historic Sites section (Section 86.604(g)) of the Resource Protection Ordinance?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	NOT APPLICABLE/EXEMPT <input type="checkbox"/>

***Wetland and Wetland Buffers:***

The project site does not support wetlands or wetlands buffers. The project has been found to be consistent with Article IV of the Resource Protection Ordinance, due to the following reasons: a) the project will not place any non-permitted uses within wetlands; b) the project will not allow grading, filling, construction, or placement of structures within identified wetlands; and c) the project will not allow any non-permitted uses within wetland buffer areas. The project site has been previously graded since the 1970s. Therefore, it has been found that the proposed project complies with Sections 86.604(a) and (b) of the Resource Protection Ordinance.

***Floodways and Floodplain Fringe:***

The project does not contain a 100-year floodway or floodplain. Construction associated with the project will not be located within a Floodway or Floodplain fringe. Therefore, it has been found that the proposed project complies with Sections 86.604(c) and (d) of the Resource Protection Ordinance.

***Steep Slopes:***

Slopes with a gradient of 25 percent or greater and 50 feet or higher in vertical height are required to be placed in open space easements by the San Diego County Resource Protection Ordinance (RPO). The project site has been previously graded since the 1970s. The project site does not contain steep slopes as defined by the RPO. Therefore, it has been found that the proposed project complies with Sections 86.604(e) of the RPO.

***Sensitive Habitats:***

Sensitive habitat lands include unique vegetation communities and/or habitat that is either necessary to support a viable population of sensitive species, is critical to the proper functioning of a balanced natural ecosystem, or which serves as a functioning wildlife corridor. Based on review of historic imagery, previous permits, and a site visit, sensitive habitat lands were not identified on the Project site. Therefore, it has been found that the proposed project complies with Section 86.604(f) of the RPO.

***Significant Prehistoric and Historic Sites:***

The property has been surveyed by a County of San Diego approved archaeologist/historian (or County of San Diego staff archaeologist/ historian) and it has



been determined that the property does not contain any archaeological/ historical sites. As such, the project complies with the RPO. Therefore, the project complies with the RPO.

**V. STORMWATER ORDINANCE (WPO)** – Does the project comply with the County of San Diego Watershed Protection, Stormwater Management and Discharge Control Ordinance (WPO)?

YES

NO

NOT APPLICABLE

A Stormwater Quality Management Plan dated June 3, 2022 by Excel Engineering has been reviewed and are found to be complete in compliance with the WPO.

**VI. NOISE ORDINANCE** – Does the project comply with the County of San Diego Noise Element of the General Plan and the County of San Diego Noise Ordinance?

YES

NO

NOT APPLICABLE

General Plan – Noise Element: Policy 4b addresses noise sensitive areas and requires projects to comply with a Community Noise Equivalent Level (CNEL) of 60 decibels (dBA). Projects which could produce noise in excess of 60 dB(A) are required to incorporate design measures or mitigation as necessary to comply with the Noise Element. Based on a review of the County’s noise contour maps, the Project is located within noise contours identified in the noise element of the General Plan ranging 60 CNEL or more due to the proximity of the Project site to Interstate 15 and Old Highway 395. The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along I-15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near I-15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. The additional trips added by the Project are anticipated to be a minimal increase compared to the existing traffic on Old Highway 395 and Interstate 15. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the western property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill

slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

Noise Ordinance – Section 36-404: Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. The Project consists of redevelopment of an existing commercial center. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles as previously discussed in the General Plan – Noise Element analysis. It should be noted that the residential zones along the western property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Therefore, the project complies with Section 36.404 of the Noise Ordinance.

Noise Ordinance – Section 36-410: The project will not generate construction noise in excess of Noise Ordinance standards. Construction operations will occur only during permitted hours of operation. The project does not propose the use of blasting during construction activities. Also, it is not anticipated that the Project will require the operation of construction equipment in excess of an average sound level of 75 dB between the hours of 7 AM and 10 PM. The majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property. The Project will also implement standard good practice recommendations that are conditions of the Project such as:

- a. Turn off equipment when not in use.
- b. Equipment used in construction should be maintained in proper operating condition,  
and all loads should be properly secured, to prevent rattling and banging.
- c. Use equipment with effective mufflers.
- d. Minimize the use of backup alarms.

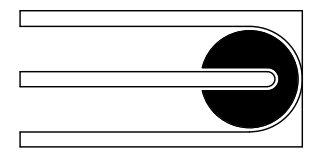
## Attachment C – Environmental Findings

**ENVIRONMENTAL FINDINGS**

PALA MESA COMMERCIAL  
PDS2020-STP-20-021, LOG NO. PDS2020-ER-20-02-003  
January 23, 2025

1. In accordance with State CEQA Guidelines section 15183, find the project is exempt from further environmental review for the reasons stated in the 15183 Statement of Reasons dated October 17, 2024, because the project is consistent with the General Plan for which an environmental impact report dated August 2011 on file with Planning & Development Services as Environmental Review Number 02-ZA-001 (GPU EIR) was certified, there are no project specific effects which are peculiar to the project or its site, there are no project impacts which the GPU EIR failed to analyze as significant effects, there are no potentially significant off-site and/or cumulative impacts which the GPU EIR failed to evaluate, there is no substantial new information which results in more severe impacts than anticipated by the GPU EIR, and that the application of uniformly applied development standards and policies, in addition to feasible mitigation measures included as project conditions would substantially mitigate the effects of the project.
2. In accordance with State CEQA Guidelines section 15183(e)2, the Zoning Administrator, at a duly noticed public hearing on January 23, 2025, found that feasible mitigation measures identified in the General Plan Update EIR will be undertaken.
3. Find that the proposed project is consistent with the Resource Protection Ordinance (County Code, section 86.601 et seq.).
4. Find that plans and documentation have been prepared for the proposed project that demonstrate that the project complies with the Watershed Protection, Stormwater Management, and Discharge Control Ordinance (County Code, section 67.801 et seq.).

## Attachment D – Site Plan



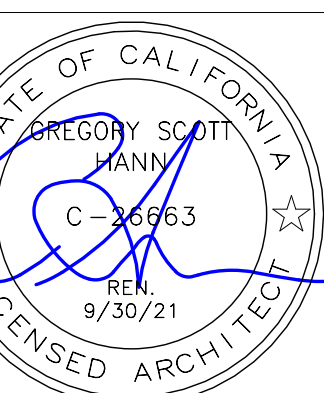
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CLIENT:

RAFID HAMIKA

RAFID HAMIKA  
PROPOSED NTI GASOLINE SERVICE STATION/SHOPPING CENTER  
3233 OLD HIGHWAY 395  
COUNTY OF SAN DIEGO - FALLBROOK, CA 92028

Architect of Record:  
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MURRIETA, CA 92562  
TEL: 951-696-1490  
CEL: 951-899-7601  
FAX: 951-696-1443  
E-MAIL: gshann@empregr.biz



Date: MARCH 17, 2020

Project Number: EDG#04672

NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM

CHECKED BY: GH

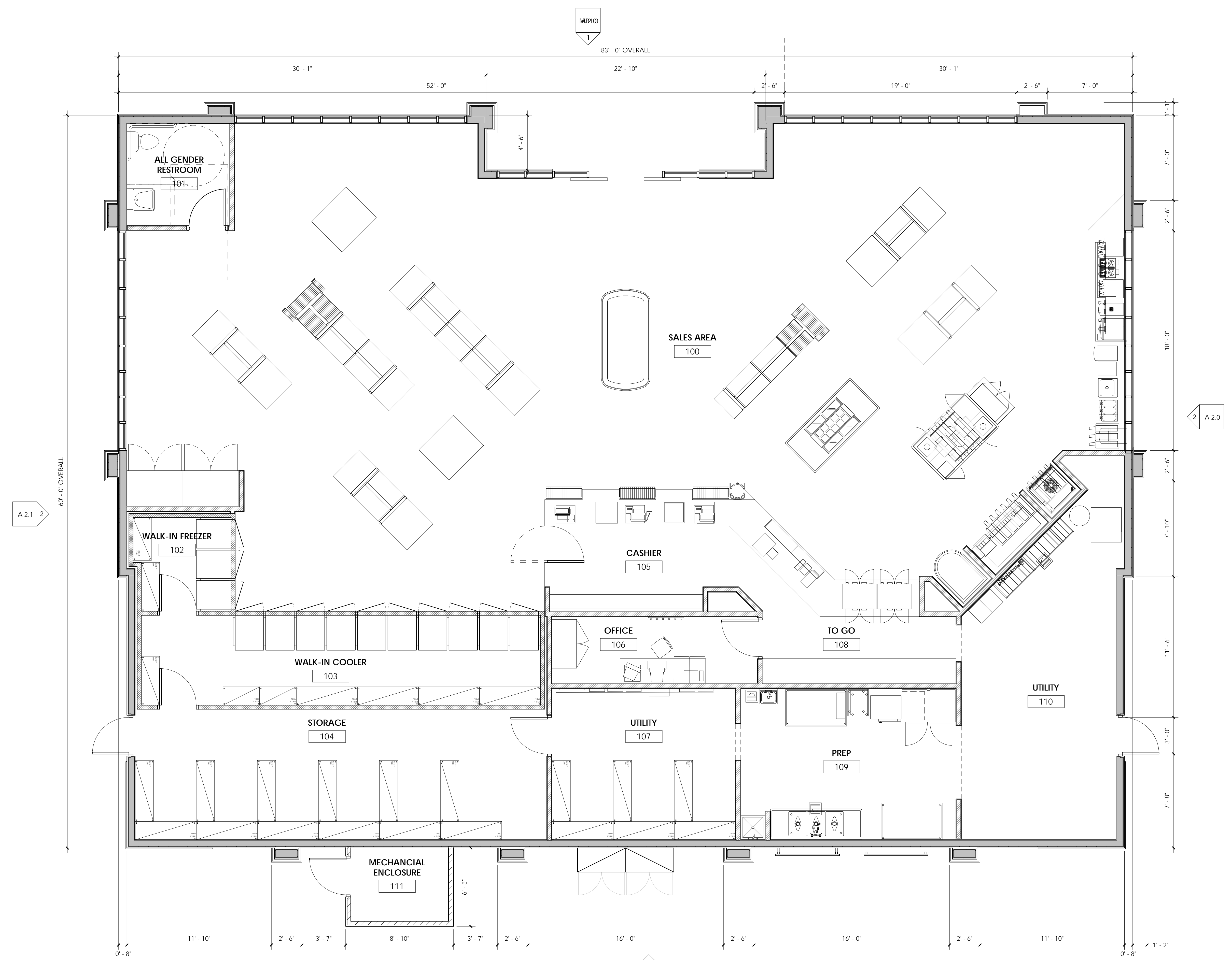
DRAWN BY: RM

DRAWING TITLE:

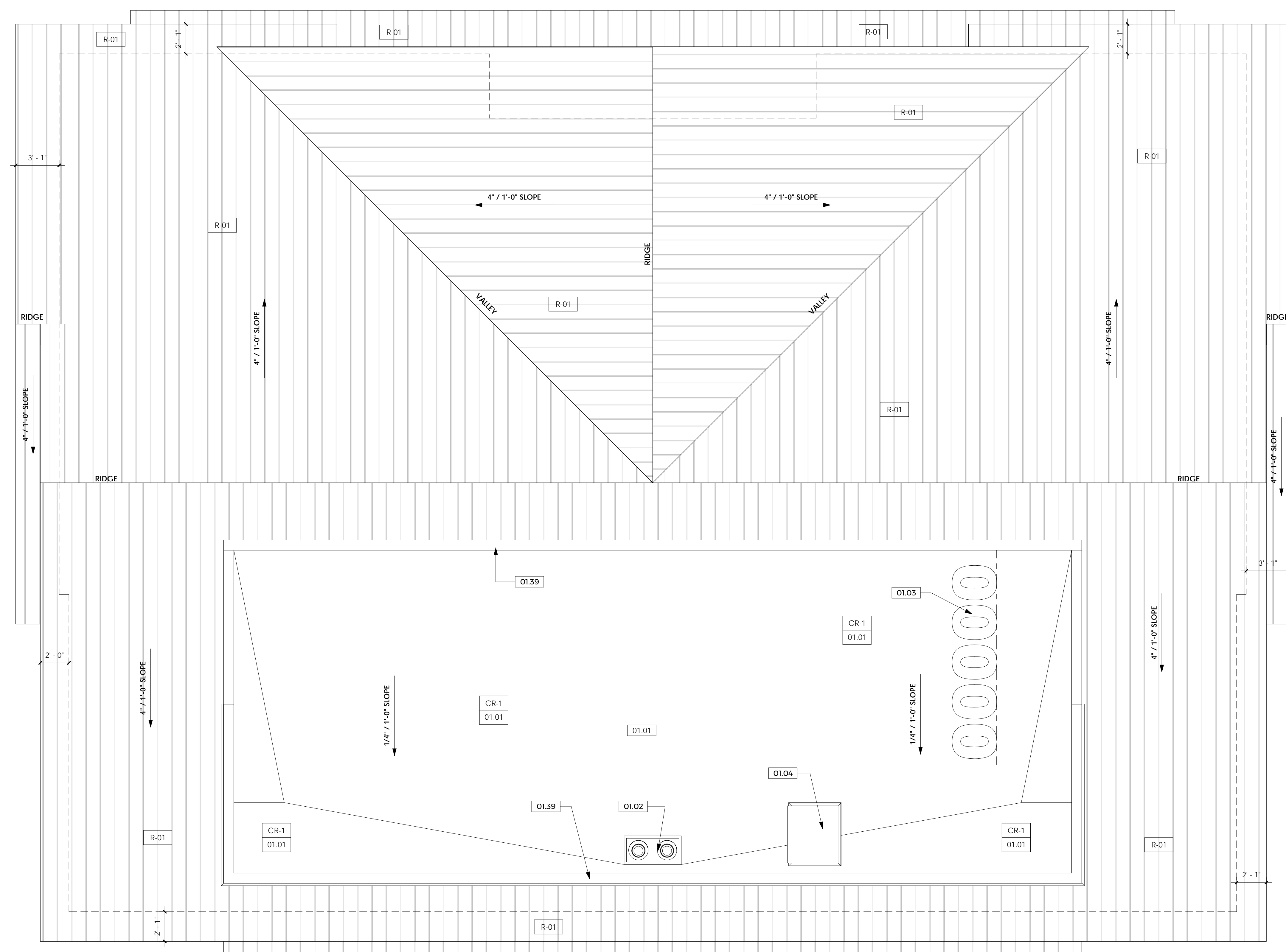
C-STORE FLOOR PLAN

SHEET NO:

A 1.0



1 C-STORE FLOOR PLAN  
1/4" = 1'-0"



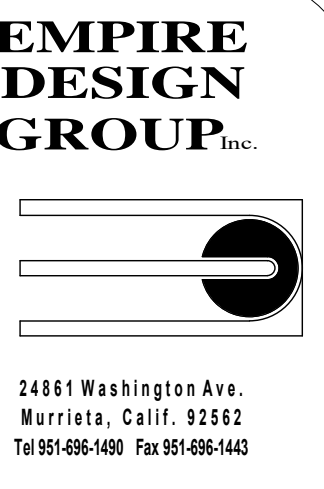
① C-STORE ROOF PLAN  
1/4" = 1'-0"

**KEYED NOTES:**

- 01.01 COOL ROOFING 60 MIL TPO SINGLE PLY ROOF BY FIRESTONE OR EQUAL. INSTALLATION OF ROOFING SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS
- 01.02 JAY R. SMITH MFG. CO. 1800 ROOF AND OVERFLOW DRAIN WITH DECK PLATE
- 01.03 36 INCH HIGH NUMBERS x 12 INCH WIDE STROKE BUILDING ADDRESS PARALLEL TO GROVE AVENUE. TO BE PAINTED WITH REFLECTIVE WHITE PAINT ON A FLAT BLACK BACKGROUND. NO EQUAL.
- 01.04 ROOF ACCESS HATCH
- 01.39 42 INCH MIN. PARAPET WALL TO BE USED TO SCREEN ALL ROOF MOUNTED MECHANICAL EQUIPMENT

**ROOFING:**

- CR-1 COOL ROOFING 60 MIL TPO SINGLE PLY ROOF BY FIRESTONE, ICC-ESR-3174
- 01.01
- R-01 METAL STANDING SEAM ROOF. MFR: BRIDGESTEEEL. SERIES: NAIL STRIP. COLOR: OLD ZINC GRAY

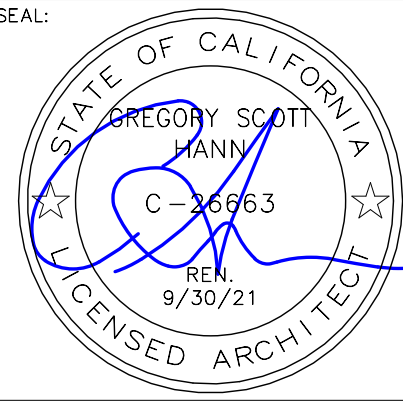


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Date: MARCH 17, 2020  
Project Number: EDG#04672

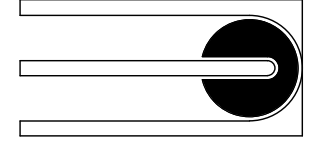
NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
CHECKED BY: GH  
DRAWN BY: RM

DRAWING TITLE:  
**C-STORE ROOF PLAN**

SHEET NO:

**A1.1**



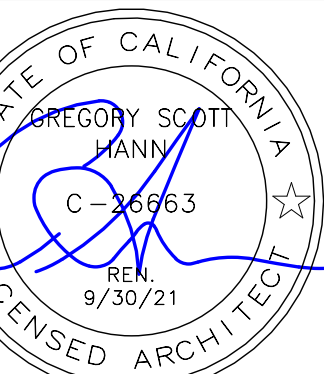
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CLIENT:

**RAFID  
HAMIKA**

**PROPOSED NTI GASOLINE SERVICE STATION/SHOPPING CENTER**  
3233 OLD HIGHWAY 395  
COUNTY OF SAN DIEGO - FALLBROOK, CA 92028

Architect of Record:  
GREGORY S. HANN, AIA  
24861 WASHINGTON AVE.  
MURRIETA, CA 92562  
TEL: 951-899-7601  
CELL: 951-899-7601  
FAX: 951-899-1443  
E-MAIL: gshann@empiregr.biz



Date: MARCH 17, 2020

Project Number: EDG04672

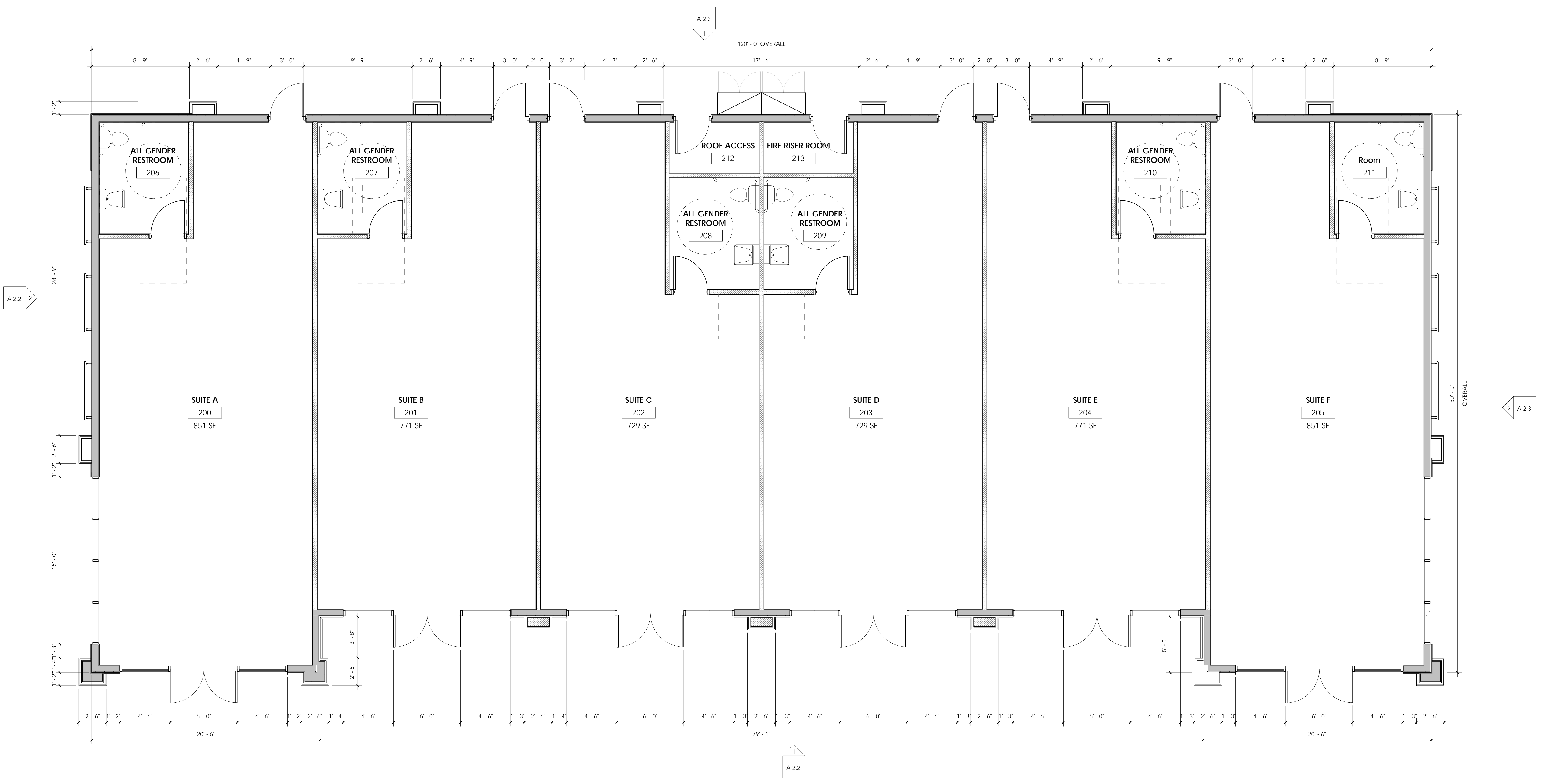
NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
CHECKED BY: GH  
DRAWN BY: RM

**BUILDING D  
FLOOR PLAN**

SHEET NO:

**A 1.2**

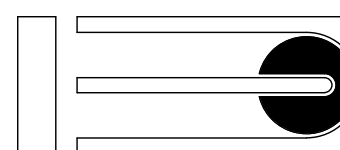


1 BUILDING D FLOOR PLAN  
1/4" = 1'-0"









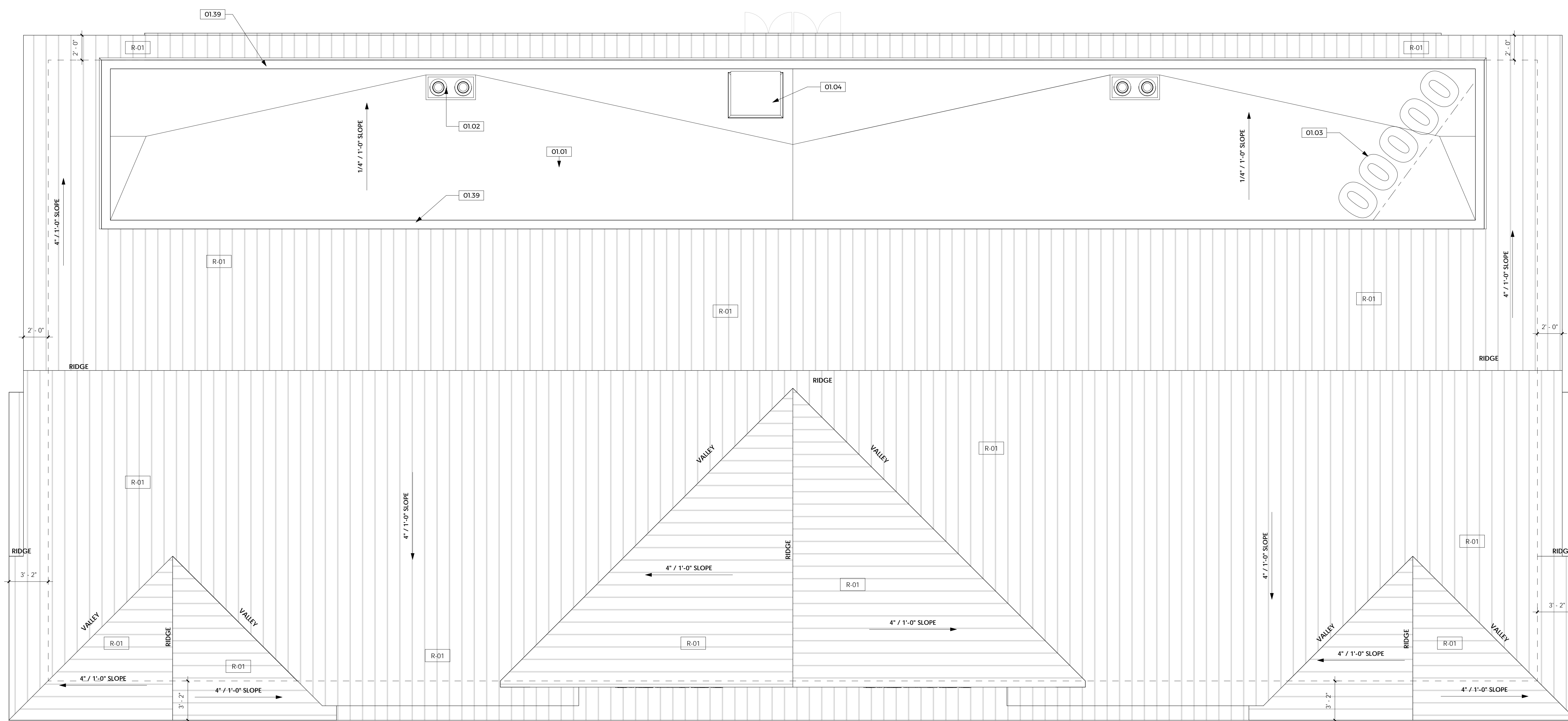
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**PROPOSED NTI GASOLINE SERVICE STATION/SHOPPING CENTER**  
3233 OLD HIGHWAY 395  
COUNTY OF SAN DIEGO - FALLBROOK, CA 92028

RAFID HAMIKA



**1** BUILDING E ROOF PLAN  
1/4" = 1'-0"

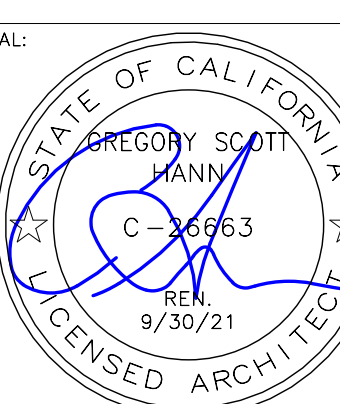
**KEYED NOTES:**

- 01.01 COOL ROOFING 60 MIL TPO SINGLE PLY ROOF BY FIRESTONE OR EQUAL. INSTALLATION OF ROOFING SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS
- 01.02 JAY R. SMITH MFG. CO. 1800 ROOF AND OVERFLOW DRAIN WITH DECK PLATE
- 01.03 36 INCH HIGH NUMBERS x 12 INCH WIDE STROKE BUILDING ADDRESS PARALLEL TO GROVE AVENUE. TO BE PAINTED WITH REFLECTIVE WHITE PAINT ON A FLAT BLACK BACKGROUND. NO EQUAL.
- 01.04 ROOF ACCESS HATCH
- 01.39 42 INCH MIN. PARAPET WALL TO BE USED TO SCREEN ALL ROOF MOUNTED MECHANICAL EQUIPMENT

**ROOFING:**

- CR-1** COOL ROOFING 60 MIL TPO SINGLE PLY ROOF BY FIRESTONE, ICC-ESR-3174
- 01.01**
- R-01** METAL STANDING SEAM ROOF. MFR: BRIDGESTEE. SERIES: NAIL STRIP COLOR: OLD ZINC GRAY

Architect of Record:  
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E-MAIL: gshann@empiregr.biz



Date: MARCH 17, 2020

Project Number: EDG#04672

NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: **RM**  
CHECKED BY: **GH**  
DRAWN BY: **RM**

DRAWING TITLE:

**BUILDING E ROOF PLAN**

SHEET NO:

**A 1.5**

**GENERAL NOTES:**

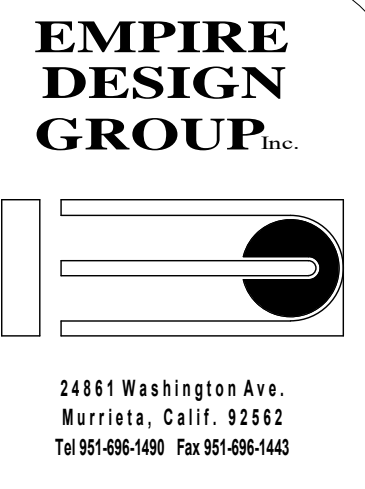
1. STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.

**KEYED NOTES:**

- 02.00 EXTERIOR LIGHT FIXTURE, PER OWNER
- 02.02 WALL MOUNTED EXTERIOR SIGNGAGE, BY OTHERS
- 02.03 STOREFRONT WINDOWS AND DOORS TO BE INSTALLED BY OTHERS

**EXTERIOR FINISHES:**

- [STCO] 7/8" CEMENT PLASTER, TEXTURE: FINE SAND FINISH
- [CR] FOAM CORNICE, TEXTURE: FINE SAND FINISH, COLOR: WHITE
- [ST] STONE VENEER, MFR: REAL STONE, SERIES: LEDGESTONE, COLOR: CHARCOAL
- [LS] LAP SIDING, MFR: ALLURA, SERIES: TRADITIONAL LAP, COLOR: FLAGSTONE
- [SS] SHINGLE SIDING, MFR: ALLURA, SERIES: STAGGERED EDGE, COLOR: WHITE
- [TF] FENCING, MFR: TRES FENCING, COLOR: WOODLAND BROWN
- [WD] STAINED WOOD BEAM, MFR: BEHR, COLOR: ANTIQUE WALNUT
- [R-01] METAL STANDING SEAM ROOF, MFR: BRIDGESTEL, SERIES: MECHANICAL LOCK, COLOR: OLD ZINC GRAY
- [TR] TRIM & FASCIA BOARDS, MFR: LP SMART SIDE, SERIES: TRIM & FASCIA, TEXTURE: CEDAR TEXTURE, COLOR: WHITE

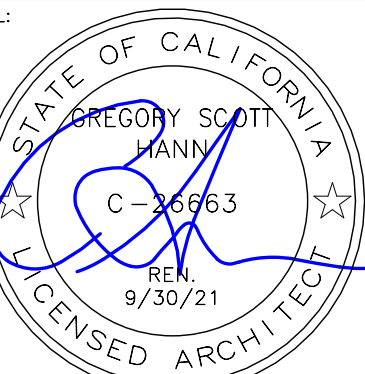


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Date: MARCH 17, 2020  
Project Number: EDG#04672

NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
CHECKED BY: GH  
DRAWN BY: RM

DRAWING TITLE:  
**C-STORE EXTERIOR ELEVATIONS**

SHEET NO:

**A 2.0**



① C-STORE NORTH ELEVATION  
1/4" = 1'-0"



② C-STORE EAST ELEVATION  
1/4" = 1'-0"



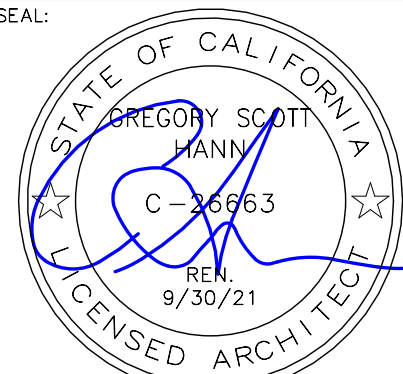


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Date: MARCH 17, 2020

Project Number: EDG#04672

NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
CHECKED BY: GH  
DRAWN BY: RM  
DRAWING TITLE:

**RETAIL BUILDING D ELEVATIONS**

SHEET NO:

**A 2.3**



1 RETAIL D REAR ELEVATION  
1/4" = 1'-0"



2 RETAIL D RIGHT ELEVATION  
1/4" = 1'-0"

**EXTERIOR FINISHES:**

- STICO** 7/8" CEMENT PLASTER. TEXTURE: FINE SAND FINISH
- CR** FOAM CORNICE. TEXTURE: FINE SAND FINISH. COLOR: WHITE
- ST** STONE VENEER. MFR: REAL STONE. SERIES: LEDGESTONE. COLOR: CHARCOAL
- LS** LAP SIDING. MFR: ALLURA. SERIES: TRADITIONAL LAP. COLOR: FLAGSTONE
- SS** SHINGLE SIDING. MFR: ALLURA. SERIES: STAGGERED EDGE. COLOR: WHITE
- TF** FENCING. MFR: TRES FENCING. COLOR: WOODLAND BROWN.
- WD** STAINED WOOD BEAM. MFR: BEHR. COLOR: ANTIQUE WALNUT
- R-01** METAL STANDING SEAM ROOF. MFR: BRIDERSTEEL. SERIES: MECHANICAL LOCK. COLOR: OLD ZINC GRAY
- TR** TRIM & FASCIA BOARDS. MFR: LP SMART SIDE. SERIES: TRIM & FASCIA. TEXTURE: CEDAR TEXTURE. COLOR: WHITE

**GENERAL NOTES:**

- STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.

**KEYED NOTES:**

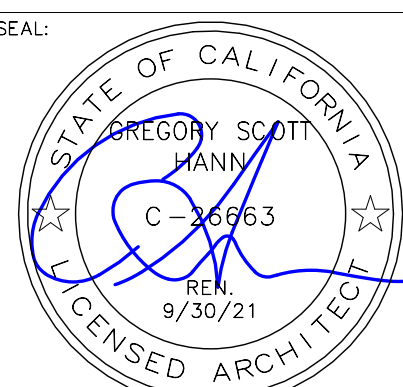
- 02.00 EXTERIOR LIGHT FIXTURE. PER OWNER
- 02.01 BOXED COLUMNS TO BE FRAMED PER STRUCTURAL PLANS
- 02.02 WALL MOUNTED EXTERIOR SIGNGAGE. BY OTHERS
- 02.03 STOREFRONT WINDOWS AND DOORS TO BE INSTALLED BY OTHERS
- 02.04 WALL MOUNTED METAL LANDSCAPE SCREEN

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Date: MARCH 17, 2020  
 Project Number: EDG#04672

NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
 CHECKED BY: GH  
 DRAWN BY: RM

DRAWING TITLE:  
**RETAIL BUILDING E ELEVATIONS**

SHEET NO:

**A 2.4**



1 RETAIL E FRONT ELEVATION  
 1/4" = 1'-0"



2 RETAIL E LEFT ELEVATION  
 1/4" = 1'-0"

**EXTERIOR FINISHES:**

STCO	7/8" CEMENT PLASTER, TEXTURE: FINE SAND FINISH
CR	FOAM CORNICE, TEXTURE: FINE SAND FINISH, COLOR: WHITE
ST	STONE VENEER, MFR: REAL STONE, SERIES: LEDGESTONE, COLOR: CHARCOAL
LS	LAP SIDING, MFR: ALLURA, SERIES: TRADITIONAL LAP, COLOR: FLAGSTONE
SS	SHINGLE SIDING, MFR: ALLURA, SERIES: STAGGERED EDGE, COLOR: WHITE
TF	FENCING, MFR: TRES FENCING, COLOR: WOODLAND BROWN
WD	STAINED WOOD BEAM, MFR: BEHR, COLOR: ANTIQUE WALNUT
R-01	METAL STANDING SEAM ROOF, MFR: BRIDERSTEEL, SERIES: MECHANICAL LOCK, COLOR: OLD ZINC GRAY
TR	TRIM & FASCIA BOARDS, MFR: LP SMART SIDE, SERIES: TRIM & FASCIA, TEXTURE: CEDAR TEXTURE, COLOR: WHITE

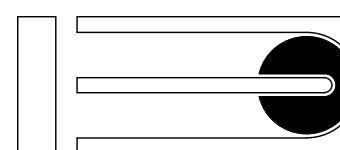
**GENERAL NOTES:**

- STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.

**KEYED NOTES:**

- |       |  |
|-------|--|
| 02.00 | EXTERIOR LIGHT FIXTURE, PER OWNER                      |
| 02.01 | BOXED COLUMNS TO BE FRAMED PER STRUCTURAL PLANS        |
| 02.02 | WALL MOUNTED EXTERIOR SIGNAGE, BY OTHERS               |
| 02.03 | STOREFRONT WINDOWS AND DOORS TO BE INSTALLED BY OTHERS |
| 02.04 | WALL MOUNTED METAL LANDSCAPE SCREEN                    |





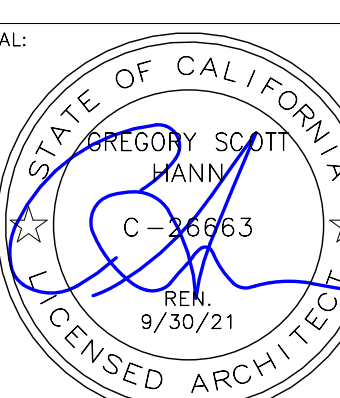
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E-MAIL: gshann@empiredg.com



Date: MARCH 17, 2020

Project Number: EDG#04672

NO. DATE REVISION DESCRIPTION

DESIGNED BY: RM

CHECKED BY: GH

DRAWN BY: RM

DRAWING TITLE:

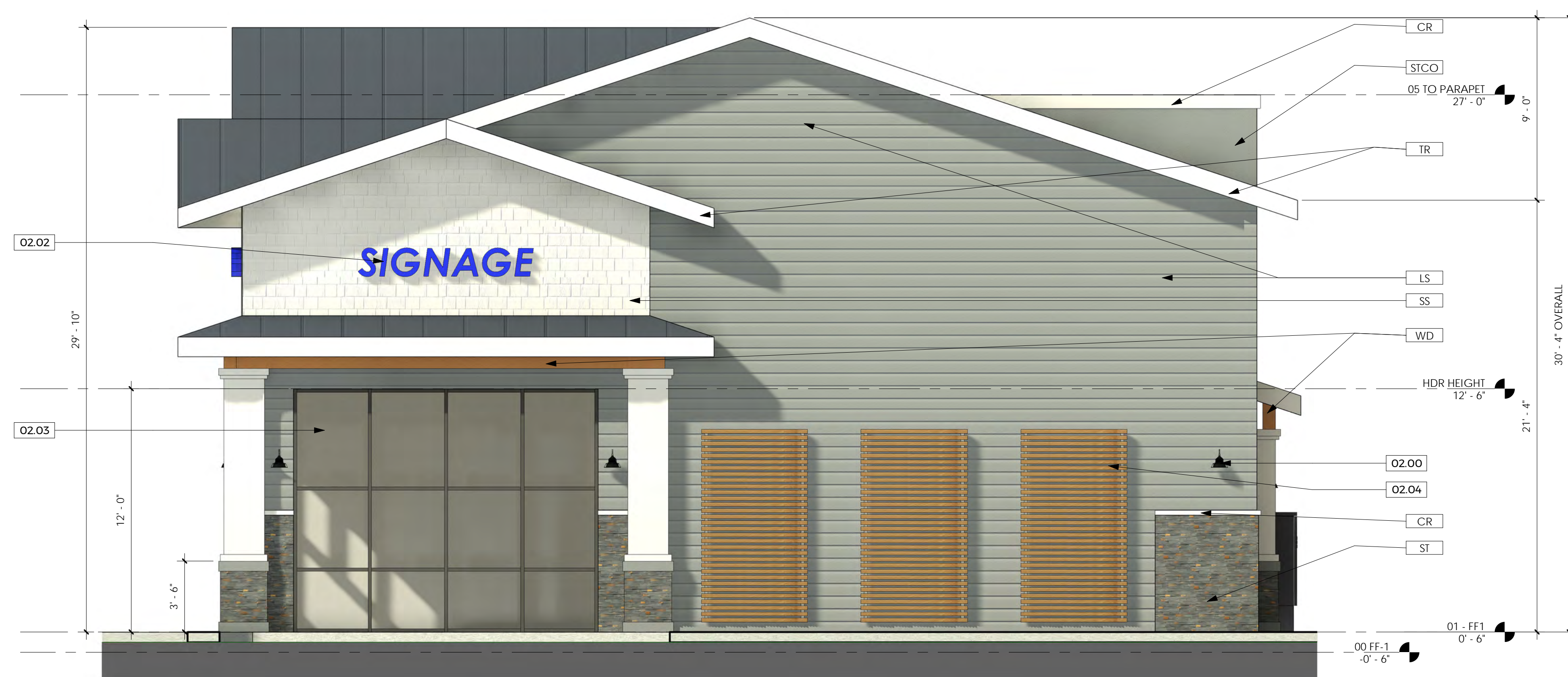
RETAIL BUILDING E ELEVATIONS

SHEET NO:

A 2.5



1 RETAIL E REAR ELEVATION  
1/4" = 1'-0"



2 RETAIL E RIGHT ELEVATION  
1/4" = 1'-0"

**EXTERIOR FINISHES:**

STICO	7/8" CEMENT PLASTER. TEXTURE: FINE SAND FINISH
CR	FOAM CORNICE. TEXTURE: FINE SAND FINISH. COLOR: WHITE
ST	STONE VENEER. MFR: REAL STONE. SERIES: LEDGESTONE. COLOR: CHARCOAL
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TF	FENCING. MFR: TRES FENCING. COLOR: WOODLAND BROWN.
WD	STAINED WOOD BEAM. MFR: BEHR. COLOR: ANTIQUE WALNUT
R.01	METAL STANDING SEAM ROOF. MFR: BRIDGESTEL. SERIES: MECHANICAL LOCK. COLOR: OLD ZINC GRAY
TR	TRIM & FASCIA BOARDS. MFR: LP SMART SIDE. SERIES: TRIM & FASCIA. TEXTURE: CEDAR. COLOR: WHITE

**GENERAL NOTES:**

- STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.

**KEYED NOTES:**

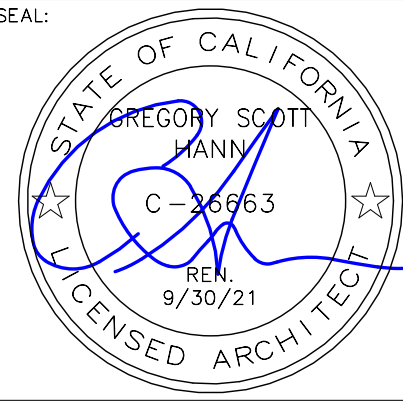
- |       |  |
|-------|--|
| 02.00 | EXTERIOR LIGHT FIXTURE. PER OWNER                      |
| 02.01 | BOXED COLUMNS TO BE FRAMED PER STRUCTURAL PLANS        |
| 02.02 | WALL MOUNTED EXTERIOR SIGNAGE. BY OTHERS               |
| 02.03 | STOREFRONT WINDOWS AND DOORS TO BE INSTALLED BY OTHERS |
| 02.04 | WALL MOUNTED METAL LANDSCAPE SCREEN                    |

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 E-MAIL: ghann@empiregr.biz



Date: MARCH 17, 2020  
 Project Number: EDG#04672

NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
 CHECKED BY: GH  
 DRAWN BY: RM

DRAWING TITLE:  
**MARKET FRONT ELEVATION**

SHEET NO:

**A 2.6**



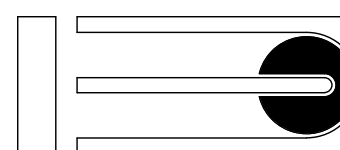
1 MARKET FRONT ELEVATION  
 3/16" = 1'-0"

**EXTERIOR FINISHES:**

- STCO 7/8" CEMENT PLASTER. TEXTURE: FINE SAND FINISH
- CR FOAM CORNICE. TEXTURE: FINE SAND FINISH. COLOR: WHITE
- ST STONE VENEER. MFR: REAL STONE. SERIES: LEDGESTONE. COLOR: CHARCOAL
- LS LAP SIDING. MFR: ALLURA. SERIES: TRADITIONAL LAP. COLOR: FLAGSTONE
- SS SHINGLE SIDING. MFR: ALLURA. SERIES: STAGGERED EDGE. COLOR: WHITE
- TF FENCING. MFR: TRES FENCING. COLOR: WOODLAND BROWN.
- WD STAINED WOOD BEAM. MFR: BEHR. COLOR: ANTIQUE WALNUT
- R-01 METAL STANDING SEAM ROOF. MFR: BRIDGESTEEL. SERIES: MECHANICAL LOCK. COLOR: OLD ZINC GRAY
- TR TRIM & FASCIA BOARDS. MFR: LP SMART SIDE. SERIES: TRIM & FASCIA. TEXTURE: CEDAR. COLOR: WHITE

**KEYED NOTES:**

1. STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.
  2. EXISTING MARKET STRUCTURE TO BE REMODELED ONLY
- KEYED NOTES:**
- 02.01 BOXED COLUMNS TO BE FRAMED PER STRUCTURAL PLANS
  - 02.02 WALL MOUNTED EXTERIOR SIGNAGE, BY OTHERS
  - 02.03 STOREFRONT WINDOWS AND DOORS TO BE INSTALLED BY OTHERS



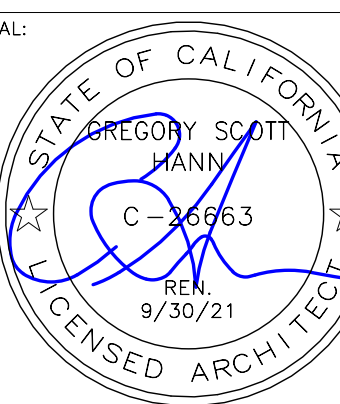
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Date: MARCH 17, 2020

Project Number: EDG#04672

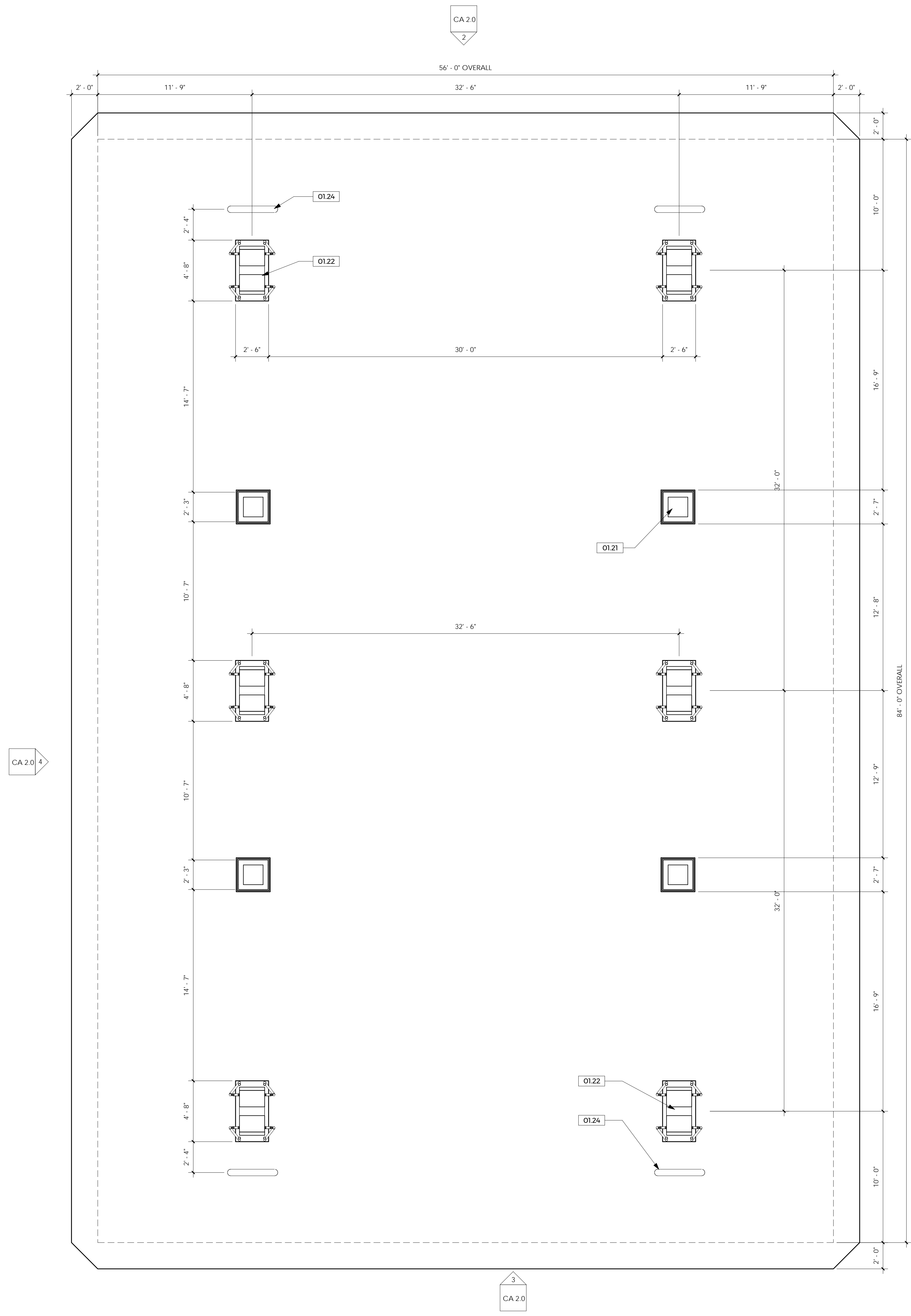
NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
 CHECKED BY: GH  
 DRAWN BY: RM

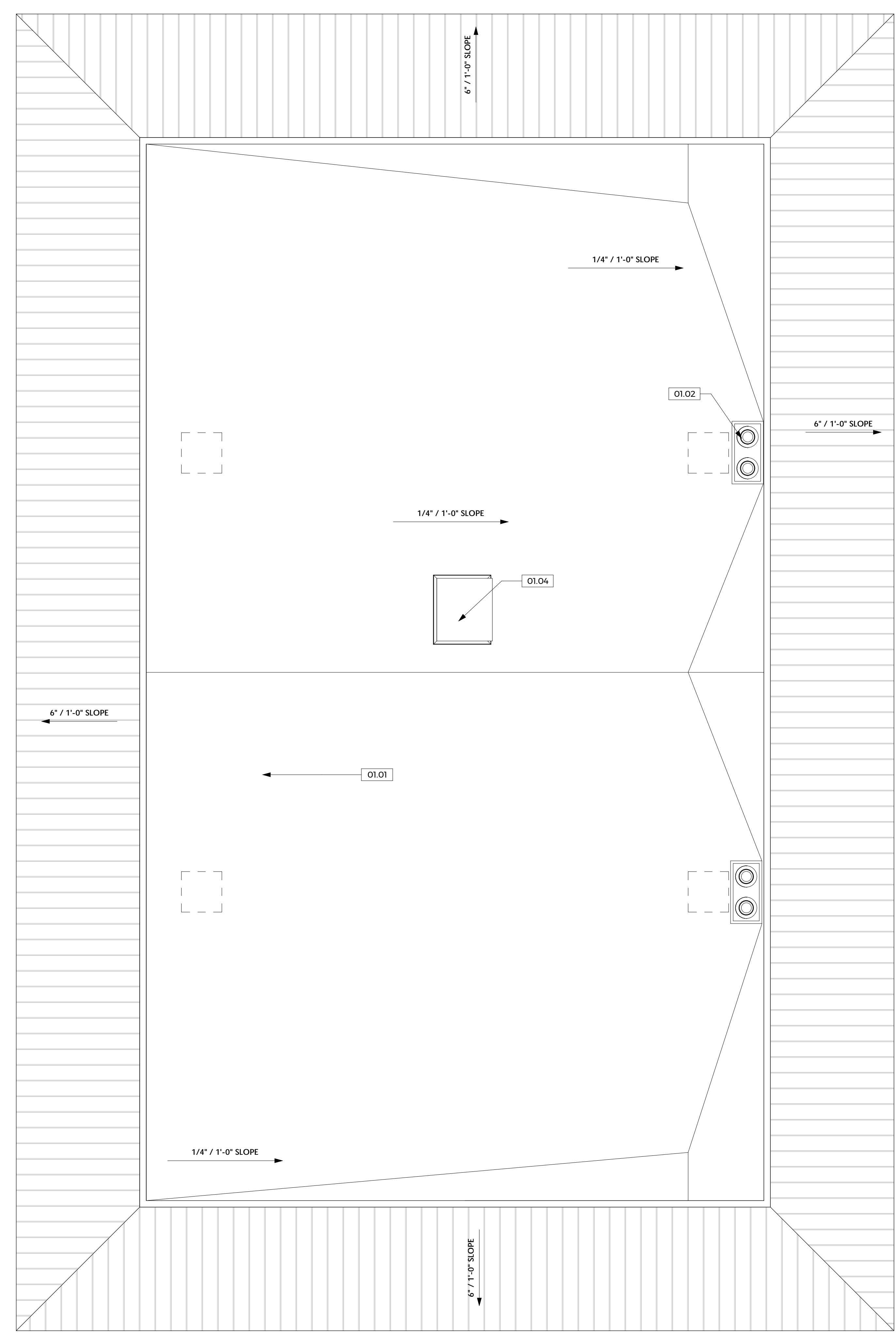
DRAWING TITLE:  
**CANOPY FLOOR & ROOF PLAN**

SHEET NO:

**CA 1.0**



1 CANOPY FLOOR PLAN  
 1/4" = 1'-0"



2 CANOPY ROOF PLAN  
 1/4" = 1'-0"

**KEYED NOTES:**

- 01.01 COOL ROOFING 60 MIL TPO SINGLE PLY ROOF BY FIRESTRON OR EQUAL. INSTALLATION OF ROOFING SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS
- 01.02 DISPENSERS WITH ILLUMINATED SIDE SKIRTS AND PUMP TOPPERS, TYP. OF B
- 01.04 ROOF ACCESS HATCH
- 01.21 METAL FRAMED COLUMNS, TYP. OF 6
- 01.22 PROTECTIVE BOLLARDS
- 01.24

**ROOFING:**

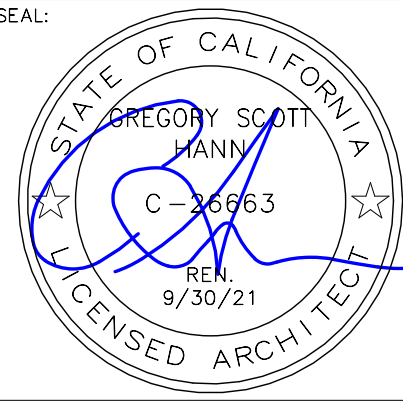
- CR-1 COOL ROOFING 60 MIL TPO SINGLE PLY ROOF BY FIRESTONE, ICC-ESR-3174
- R-01 METAL STANDING SEAM ROOF, MFR: BRIDGESTEEEL SERIES: NAIL STRIP COLOR: OLD ZINC GRAY

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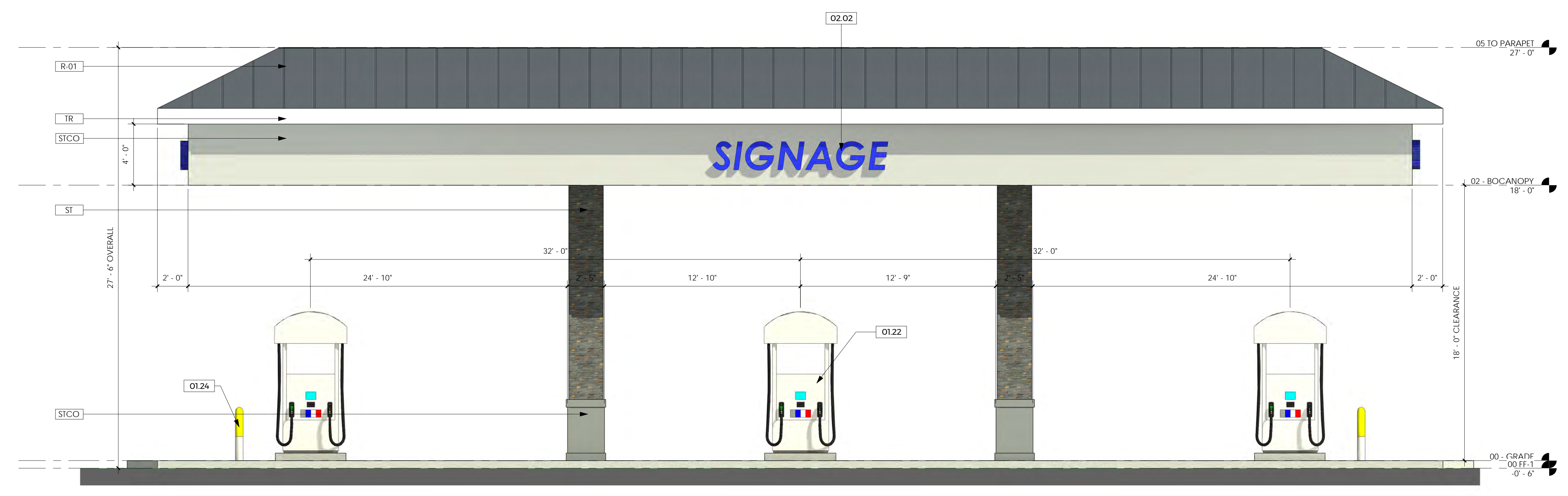
NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: RM  
 CHECKED BY: GH  
 DRAWN BY: RM

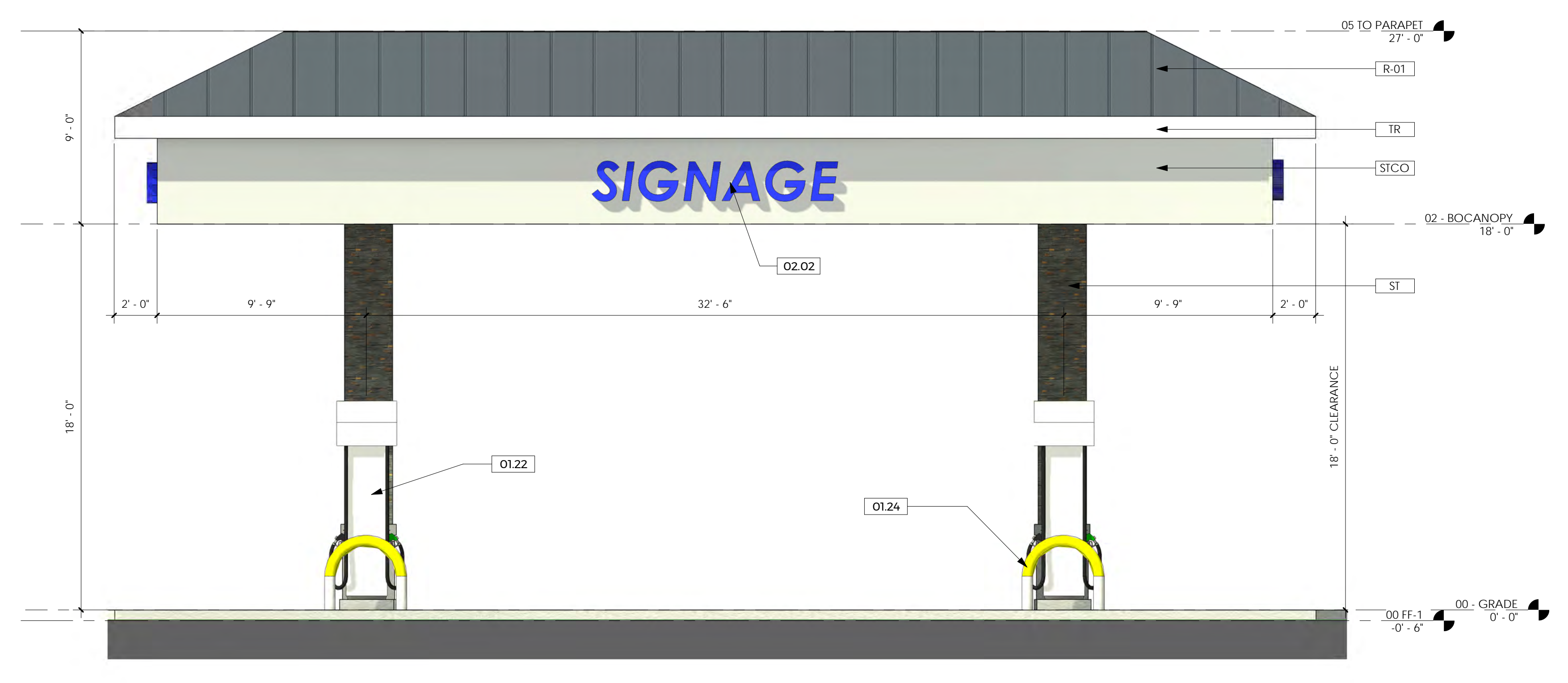
DRAWING TITLE:  
**CANOPY ELEVATIONS**

SHEET NO:

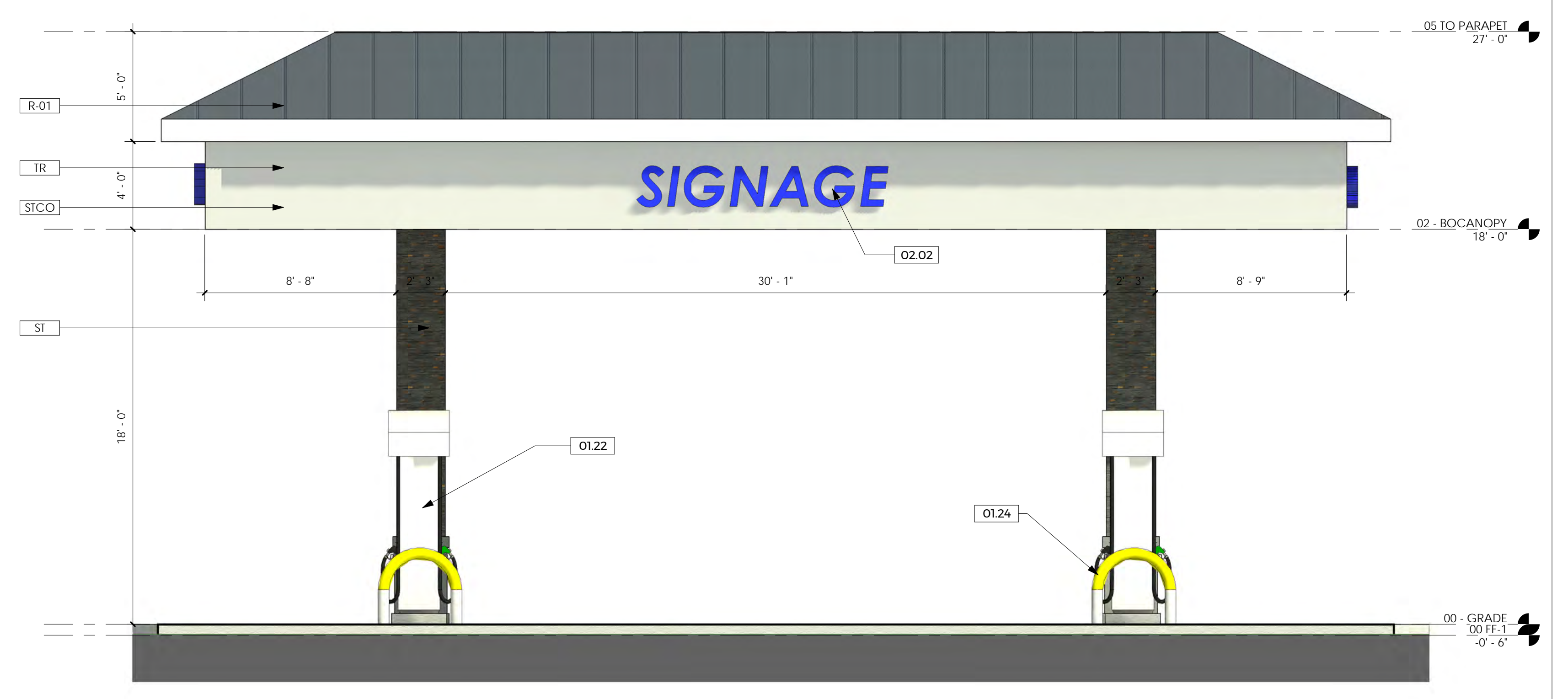
**CA 2.0**



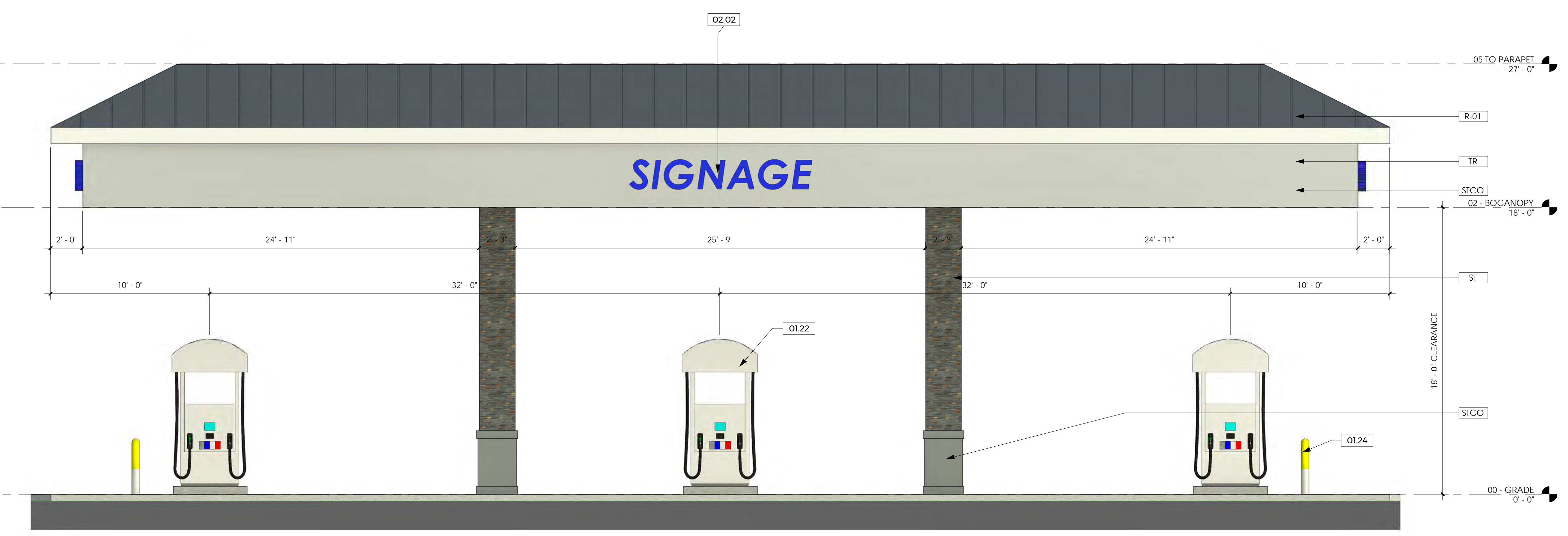
1 CANOPY EAST ELEVATION  
 1/4" = 1'-0"



2 CANOPY NORTH ELEVATION  
 1/4" = 1'-0"



3 CANOPY SOUTH ELEVATION  
 1/4" = 1'-0"

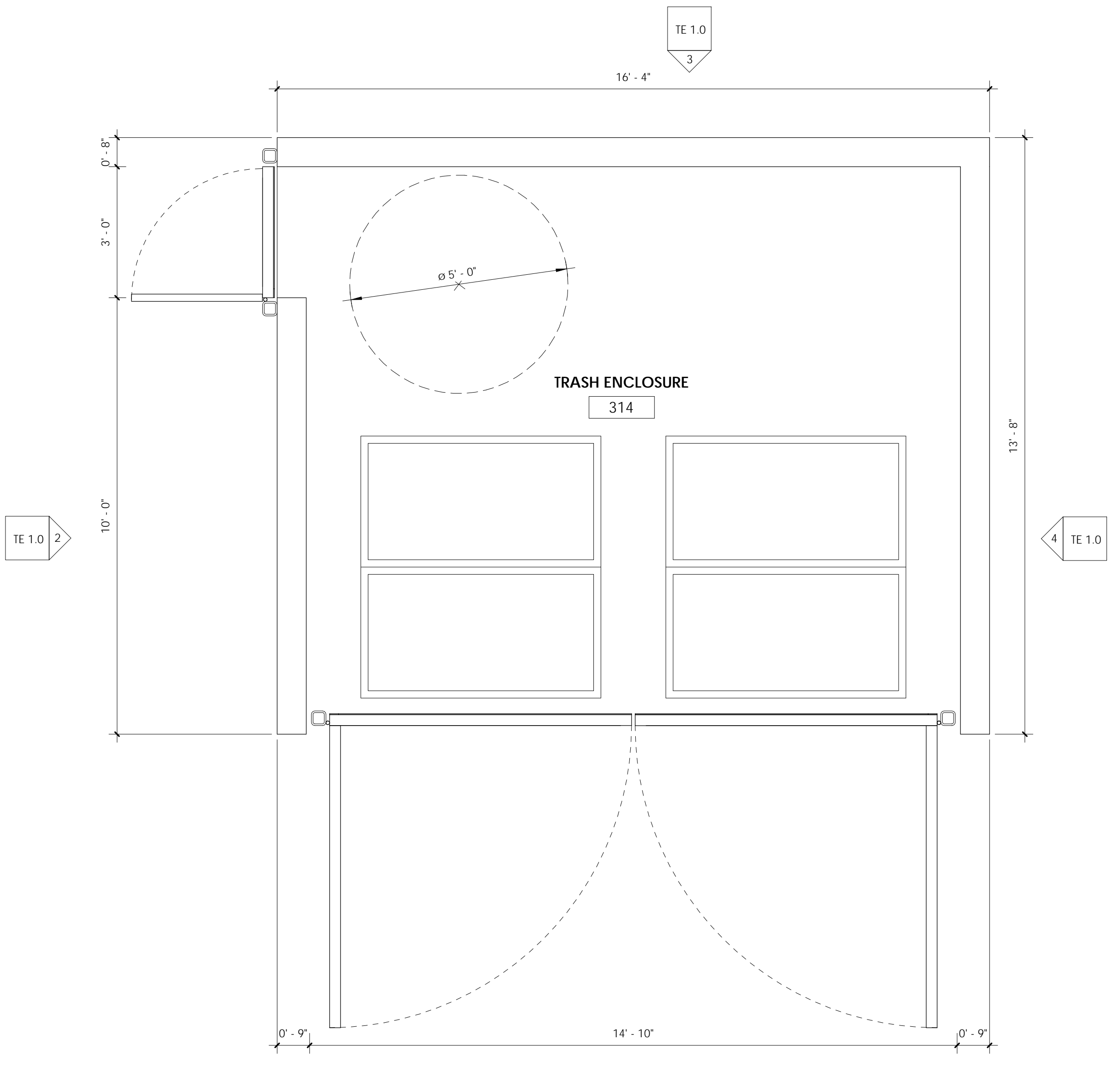


4 CANOPY WEST ELEVATION  
 1/4" = 1'-0"

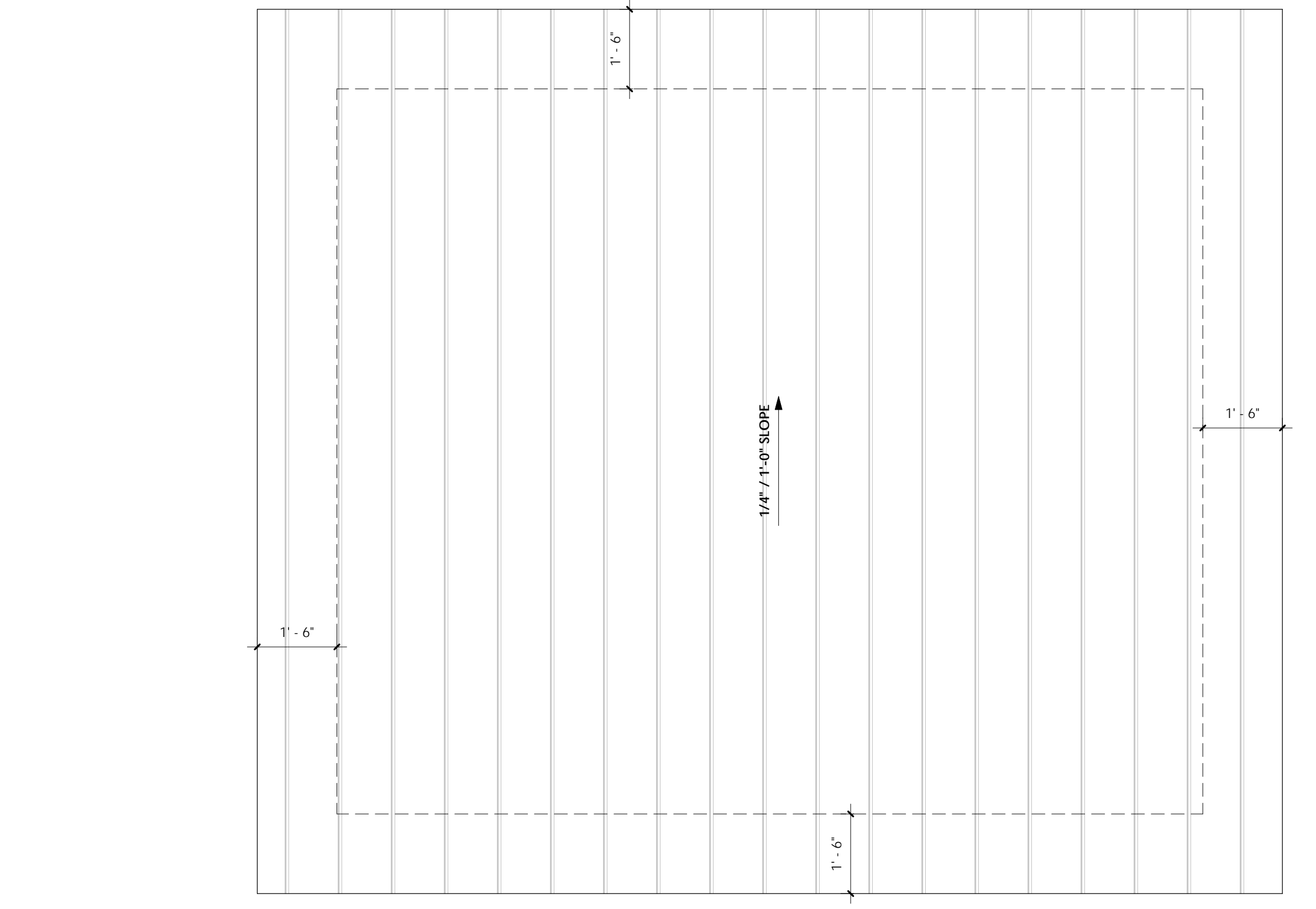
**EXTERIOR FINISHES:**

STCO	7/8" CEMENT PLASTER, TEXTURE: FINE SAND FINISH
CR	FOAM CORNICE, TEXTURE: FINE SAND FINISH, COLOR: WHITE
ST	STONE VENEER, MFR: REAL STONE, SERIES: LEDGESTONE, COLOR: CHARCOAL
LS	LAP SIDING, MFR: ALLURA, SERIES: TRADITIONAL LAP, COLOR: FLAGSTONE
SS	SHINGLE SIDING, MFR: ALLURA, SERIES: STAGGERED EDGE, COLOR: WHITE
TF	FENCING, MFR: TRES FENCING, COLOR: WOODLAND BROWN
WD	STAINED WOOD BEAM, MFR: BEHR, COLOR: ANTIQUE WALNUT
R-01	METAL STANDING SEAM ROOF, MFR: BRIDGESTEEL, SERIES: MECHANICAL LOCK, COLOR: OLD ZINC GRAY
TR	TRIM & FASCIA BOARDS, MFR: LP SMART SIDE, SERIES: TRIM & FASCIA, TEXTURE: CEDAR TEXTURE, COLOR: WHITE

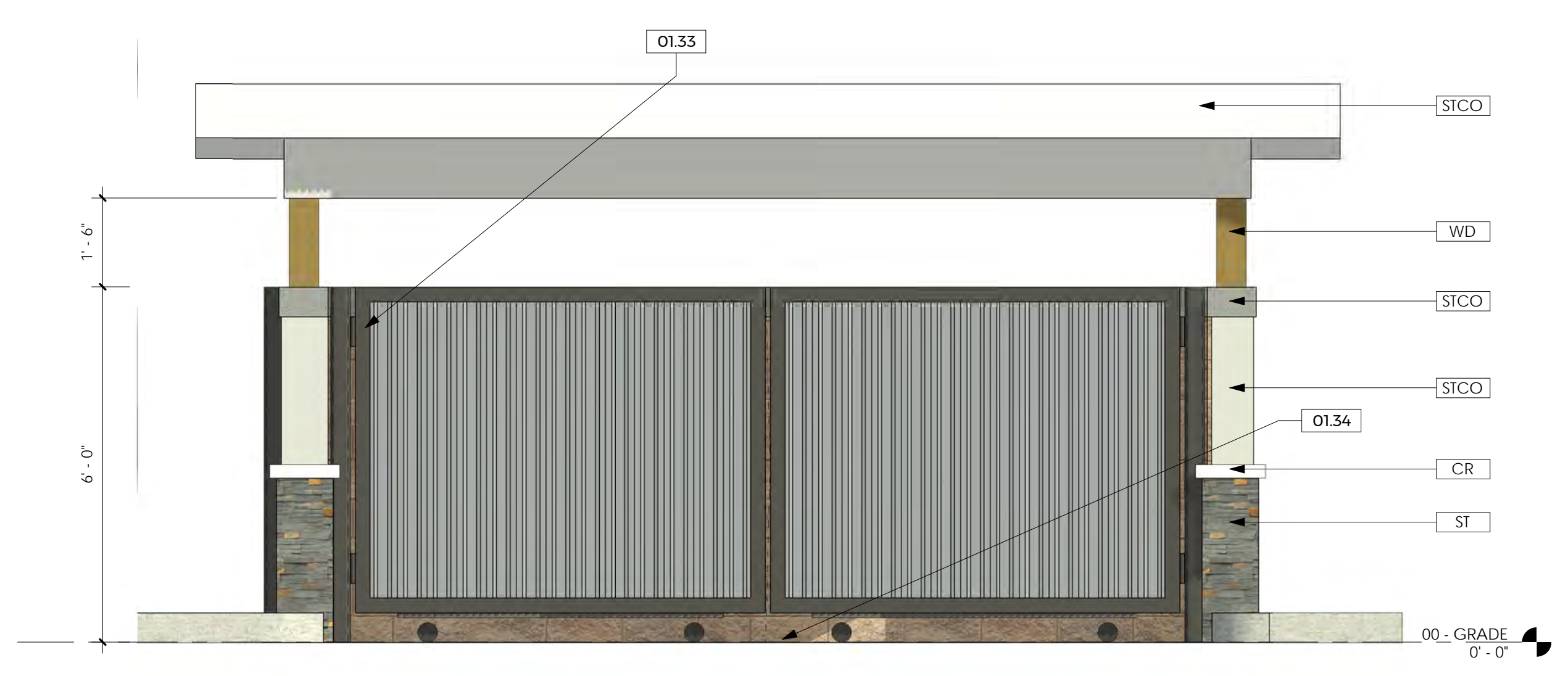
- GENERAL NOTES:**
- STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.
- KEYED NOTES:**
- 01.22 DISPENSERS WITH ILLUMINATED SIDE SKIRTS AND PUMP TOPPERS, TYP. OF 8
  - 01.24 PROTECTIVE BOLLARDS
  - 02.02 WALL MOUNTED EXTERIOR SIGNAGE, BY OTHERS



5 TE FLOOR PLAN  
1/2" = 1'-0"



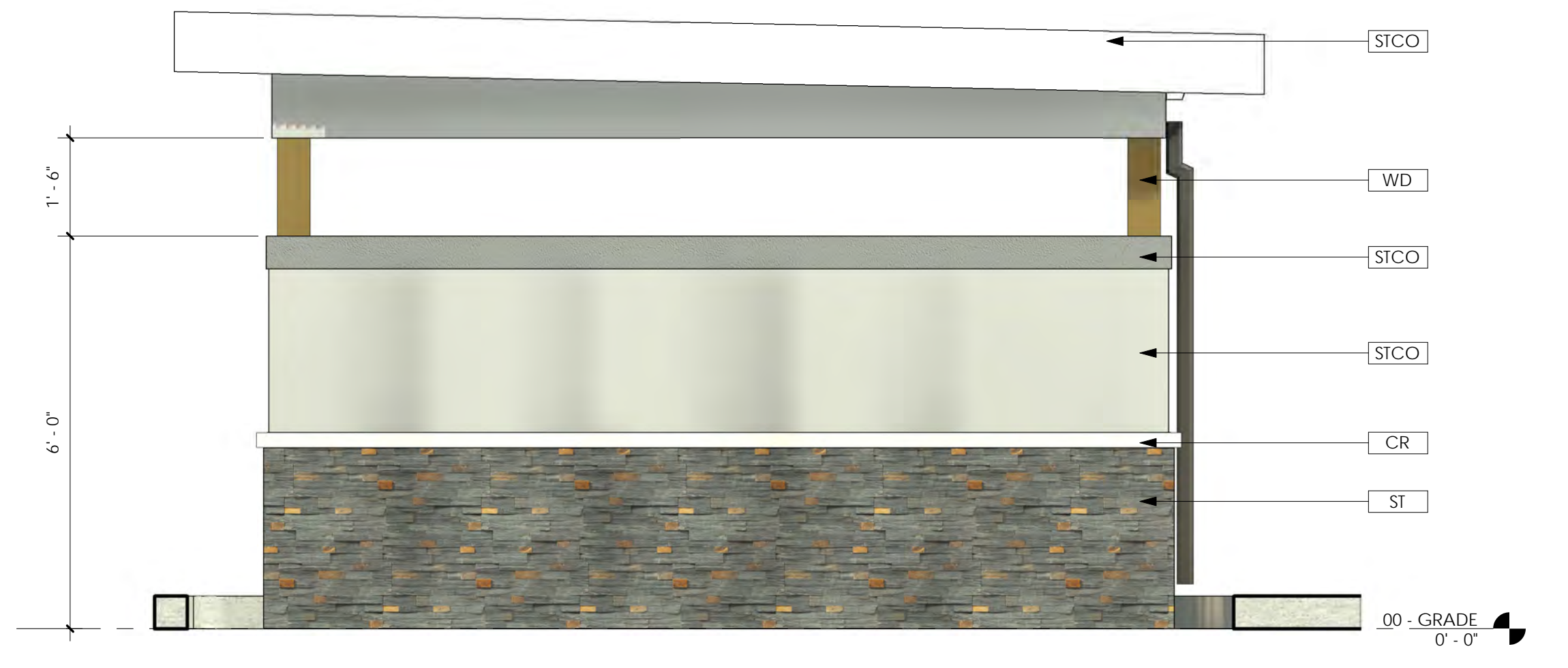
6 TE ROOF PLAN  
1/2" = 1'-0"



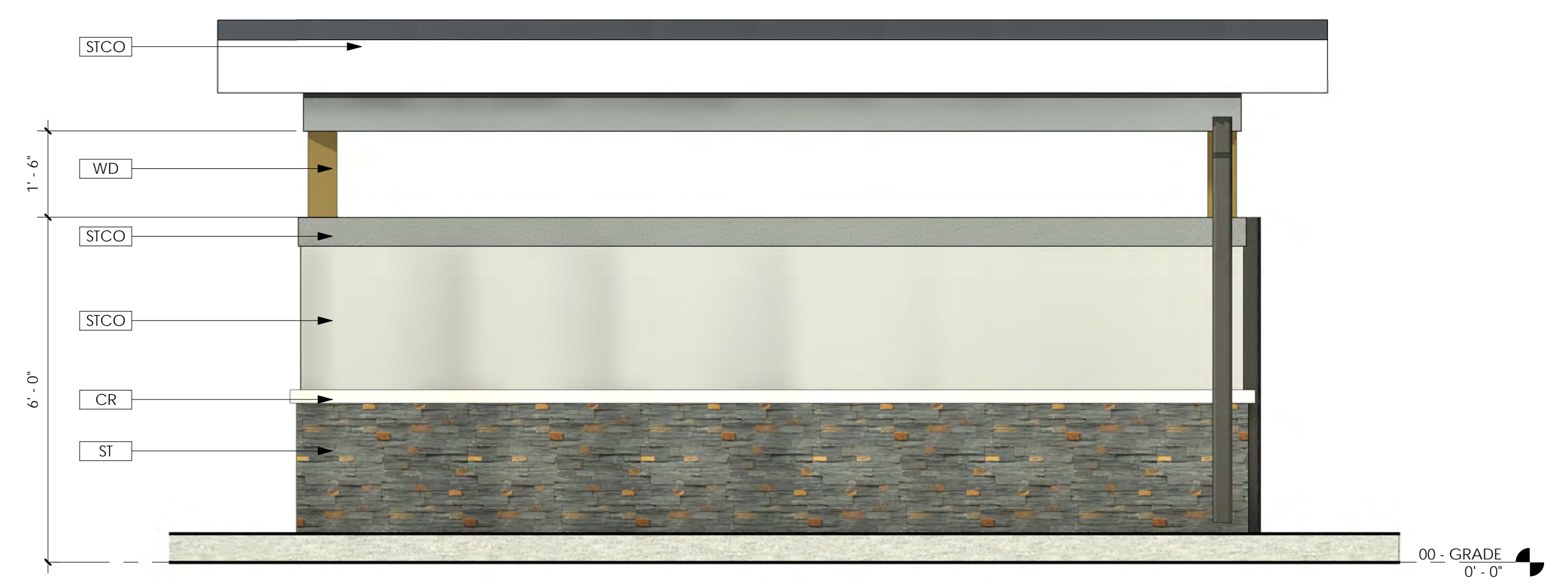
1 T.E. FRONT ELEVATION  
1/2" = 1'-0"



2 T.E. LEFT ELEVATION  
1/2" = 1'-0"



4 T.E. RIGHT ELEVATION  
1/2" = 1'-0"



3 T.E. REAR ELEVATION  
1/2" = 1'-0"

**GENERAL NOTES:**

1. STUCCO REVEAL LOCATIONS IN EXTERIOR FINISH ARE SHOWN TO ALIGN CLOSELY AS POSSIBLE TO ELEVATIONS.

**KEYED NOTES:**

01.33 2 - HEAVY DUTY STEEL HINGES, TYP. @ EACH GATE  
01.34 STEEL CANE BOLT W/ SLEEVE, LOCATED ON OUTSIDE OF ONE GATE, TYP. @ EACH SIDE

**EXTERIOR FINISHES:**

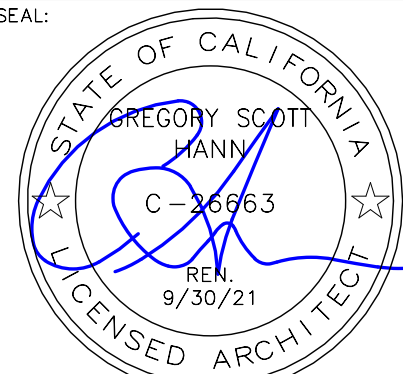
- [STCO] 7/8" CEMENT PLASTER, TEXTURE: FINE SAND FINISH
- [CR] FOAM CORNICE, TEXTURE: FINE SAND FINISH, COLOR: WHITE
- [SI] STONE VENEER, MFR: REAL STONE, SERIES: LEDGESTONE, COLOR: CHARCOAL
- [LS] LAP SIDING, MFR: ALLURA, SERIES: TRADITIONAL LAP, COLOR: FLAGSTONE
- [SS] SHINGLE SIDING, MFR: ALLURA, SERIES: STAGGERED EDGE, COLOR: WHITE
- [TF] FENCING, MFR: TRES FENCING, COLOR: WOODLAND BROWN
- [WD] STAINED WOOD BEAM, MFR: BEHR, COLOR: ANTIQUE WALNUT
- [R-01] METAL STANDING SEAM ROOF, MFR: BRIDGESTEL, SERIES: MECHANICAL LOCK, COLOR: OLD ZINC GRAY
- [TR] TRIM & FASCIA BOARDS, MFR: LP SMART SIDE, SERIES: TRIM & FASCIA, TEXTURE: CEDAR TEXTURE, COLOR: WHITE

**EMPIRE DESIGN GROUP, INC.**  
24861 Washington Ave.  
Murrieta, Calif. 92562  
TEL: 951-696-4000 FAX: 951-696-4040

**RAFID HAMIKA**

PROPOSED NTI GASOLINE SERVICE STATION/SHOPPING CENTER  
3233 OLD HIGHWAY 395  
COUNTY OF SAN DIEGO - FALLBROOK, CA 92028

Architect of Record:  
GREGORY S. HANN, AIA  
24861 WASHINGTON AVE.  
MURRIETA, CA 92562  
TEL: 951-696-1490  
CEL: 951-809-7601  
FAX: 951-696-1443  
E-MAIL: ghann@empiregr.biz



NO.	DATE	REVISION DESCRIPTION

DESIGNED BY: **RM**  
CHECKED BY: **GH**  
DRAWN BY: **RM**

**TRASH ENCLOSURE PLANS & ELEVATIONS**

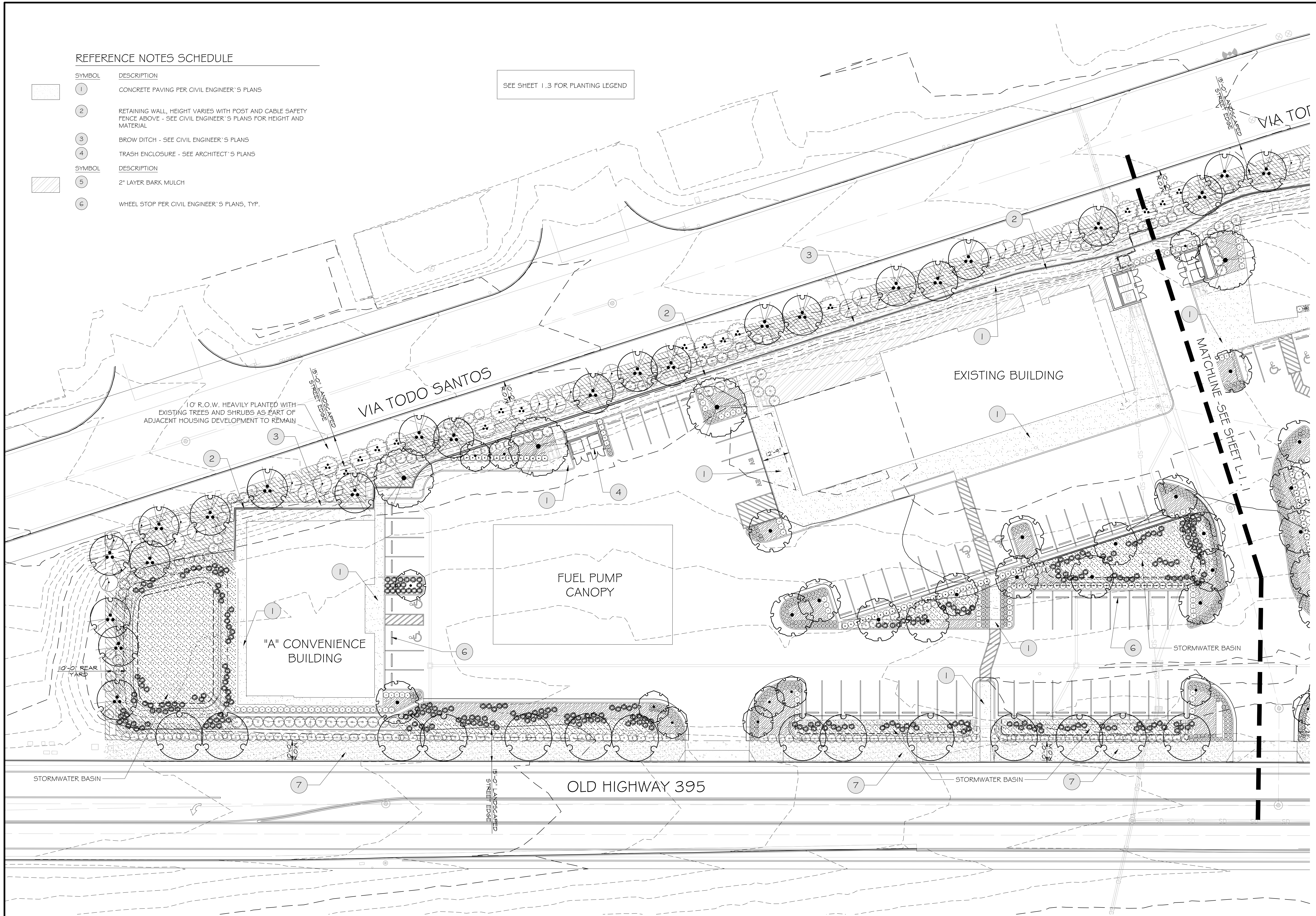
SHEET NO:

**TE 1.0**

REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION
①	CONCRETE PAVING PER CIVIL ENGINEER'S PLANS
②	RETAINING WALL, HEIGHT VARIES WITH POST AND CABLE SAFETY FENCE ABOVE - SEE CIVIL ENGINEER'S PLANS FOR HEIGHT AND MATERIAL
③	BROW DITCH - SEE CIVIL ENGINEER'S PLANS
④	TRASH ENCLOSURE - SEE ARCHITECT'S PLANS
SYMBOL	DESCRIPTION
⑤	2" LAYER BARK MULCH
⑥	WHEEL STOP PER CIVIL ENGINEER'S PLANS, TYP.

SEE SHEET 1.3 FOR PLANTING LEGEND



WYNN-SMITH  
LANDSCAPE  
ARCHITECTURE  
INC.

12315 OAK KNOLL RD, SUITE 300  
P.O. BOX 9, CA 92064  
TEL: 858.513.0030  
FAX: 858.513.0032

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OLD HIGHWAY 395  
RETAIL CENTER  
3233-3235 OLD HIGHWAY 395  
FALLBROOK, CA 92028

CONCEPTUAL  
PLANTING PLAN

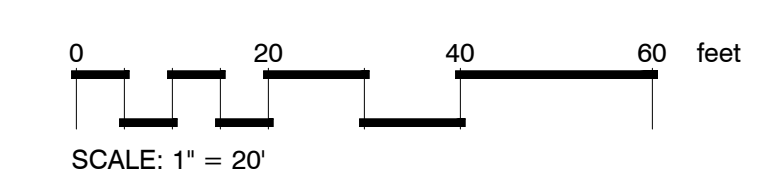
Revisions	By
3-18-2021	

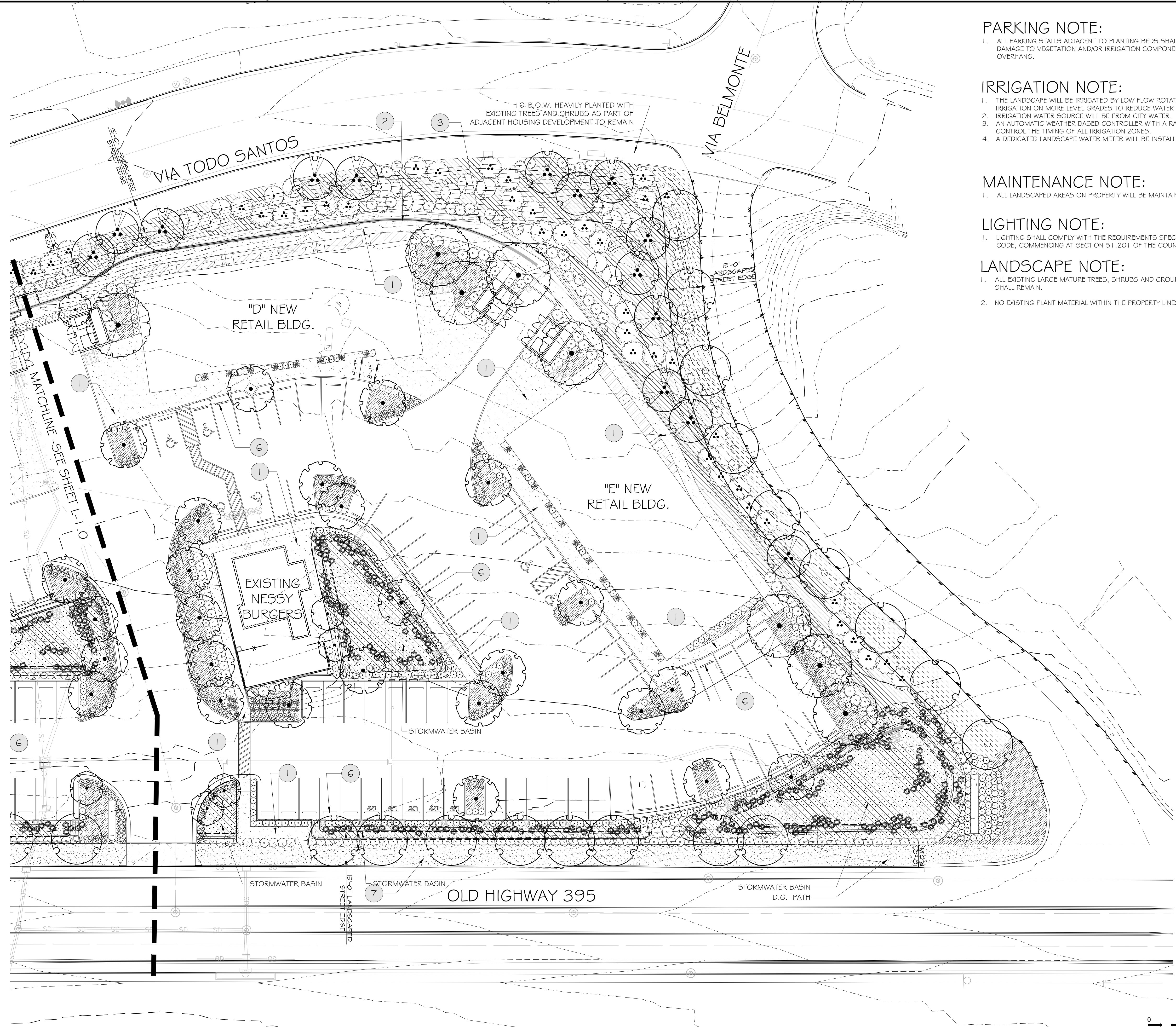
Drawn By: TEAM  
Checked By:  
Date Issued: 12-8-2020  
Project No.: 20-013  
Scale:

Sheet

L-1.0

Of Sheets





**PARKING NOTE:**

1. ALL PARKING STALLS ADJACENT TO PLANTING BEDS SHALL PROVIDE A WHEEL STOP TO PREVENT DAMAGE TO VEGETATION AND/OR IRRIGATION COMPONENTS FROM VEHICULAR BUMPER OVERHANG.

**IRRIGATION NOTE:**

1. THE LANDSCAPE WILL BE IRRIGATED BY LOW FLOW ROTATOR NOZZLES ON SLOPES AND DRIP IRRIGATION ON MORE LEVEL GRADES TO REDUCE WATER CONSUMPTION.
2. IRRIGATION WATER SOURCE WILL BE FROM CITY WATER.
3. AN AUTOMATIC WEATHER BASED CONTROLLER WITH A RAIN SENSING SHUT OFF DEVICE WILL CONTROL THE TIMING OF ALL IRRIGATION ZONES.
4. A DEDICATED LANDSCAPE WATER METER WILL BE INSTALLED.

**MAINTENANCE NOTE:**

1. ALL LANDSCAPED AREAS ON PROPERTY WILL BE MAINTAINED BY OWNER.

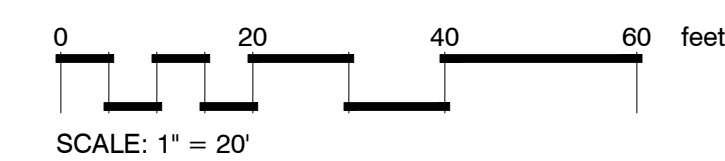
**LIGHTING NOTE:**

1. LIGHTING SHALL COMPLY WITH THE REQUIREMENTS SPECIFIED IN THE COUNTY LIGHT POLLUTION CODE, COMMENCING AT SECTION 51.201 OF THE COUNTY CODE REGULATORY ORDINANCES.

**LANDSCAPE NOTE:**

1. ALL EXISTING LARGE MATURE TREES, SHRUBS AND GROUNDCOVERS ALONG VIA TODO SANTOS SHALL REMAIN.
2. NO EXISTING PLANT MATERIAL WITHIN THE PROPERTY LINES REMAINS.

SEE SHEET 1.1 FOR REFERENCE NOTES SCHEDULE  
SEE SHEET 1.3 FOR PLANTING LEGEND



**WYNN-SMITH  
LANDSCAPE  
ARCHITECTURE  
INC.**

12315 OAK KNOLL RD, SUITE 300  
P.O. BOX 94, FALLBROOK, CA 92028  
TEL: 858.513.0030  
FAX: 858.513.0032

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**OLD HIGHWAY 395  
RETAIL CENTER  
3233-3235 OLD HIGHWAY 395  
FALLBROOK, CA 92028**

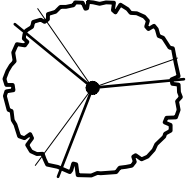
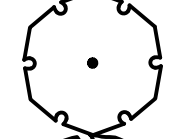
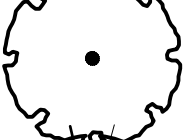
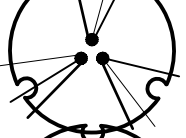
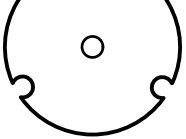














**CONCEPTUAL  
PLANTING PLAN**

Revisions	By
3-18-2021	

Drawn By: TEAM  
Checked By: \_\_\_\_\_  
Date Issued: 12-8-2020  
Project No.: 20-013  
Scale: \_\_\_\_\_

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**L-1.1**  
Of \_\_\_\_\_ Sheets

CONCEPT PLANT SCHEDULE

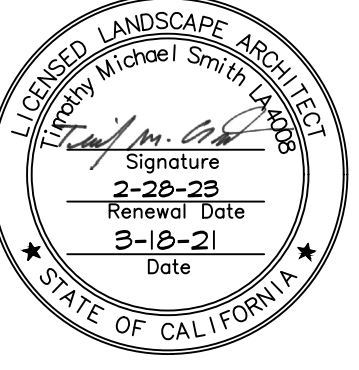
	<b>LARGE TREE</b> ARBUTUS X 'MARINA' / ARBUTUS STANDARD PISTACIA CHINENSIS / CHINESE PISTACHE PISTACIA CHINENSIS 'KEITH DAVEY' / KEITH DAVEY CHINESE PISTACHE SAPIUM SEBIFERUM / CHINESE TALLOW TREE	11	15 GAL 24"BOX 24"BOX 24"BOX
	<b>SMALL INTERIOR ACCENT TREE</b> FRUNUS CERASIFERA 'KRAUTER VESUVIUS' / PURPLE LEAF PLUM X CHITALPA TASHKENTENSIS 'PINK DAWN' / PINK DAWN CHITALPA	14	24"BOX 24"BOX
	<b>MEDIUM INTERIOR PARKING LOT TREE</b> KOELREUTERIA BIPINNATA / CHINESE FLAME TREE LOPHOSTEMON CONFERTUS / BRISBANE BOX	37	24"BOX 24"BOX
	<b>SLOPE TREE</b> PROSOPIA X 'AZT' / AZT THORNLESS MESQUITE RHUS LANCEA / AFRICAN SUMAC MULTI-TRUNK	34	15 GAL 15 GAL
	<b>STREET TREE</b> X CHITALPA TASHKENTENSIS 'PINK DAWN' / PINK DAWN CHITALPA	31	24"BOX
	<b>ACCENT SHRUB</b> CORDYLINA X 'DESIGN-A-LINE BURGUNDY' / CORDYLINA PENNISSETUM ADVENA 'FIREWORKS' / VARIEGATED PURPLE FOUNTAIN GRASS PHORMIUM X 'DAZZLER' / DAZZLER NEW ZEALAND FLAX	40	5 GAL 5 GAL 5 GAL
	<b>LARGE SLOPE SHRUB</b> ECHINUM CANDICANS / PRIDE OF MADEIRA RHAMNUS CALIFORNICA / CALIFORNIA COFFEEBERRY RHUS INTEGRIFOLIA / LEMONADE BERRY	41	5 GAL 5 GAL 5 GAL
	<b>MEDIUM SCREEN SHRUB</b> BUXUS MICROPHYLLA JAPONICA 'GREEN BEAUTY' / GREEN BEAUTY BOXWOOD LIGUSTRUM JAPONICUM 'TEXANUM' / WAX LEAF PRIVET WESTRINGIA FRUTICOSA 'MORNING LIGHT' / MORNING LIGHT COAST ROSEMARY	261	5 GAL 5 GAL 5 GAL
	<b>LARGE SLOPE FLOWERING SHRUB</b> CALLISTEMON CITRINUS / LEMON BOTTLEBRUSH SHRUB GREVILLEA NOELII / NOEL'S GREVILLEA MELALEUCA NESOPHILA / PINK MELALEUCA	53	5 GAL 5 GAL 5 GAL
	<b>GRASSES</b> CHONDROPETALUM TECTORUM / CAPE RUSH MUHLENBERGIA CAPILLARIS 'REGAL MIST' TM / REGAL MIST DEER GRASS MUHLENBERGIA RIGENS / DEER GRASS	550	1 GAL 1 GAL 1 GAL
	<b>LOW FLOWERING SHRUBS</b> DIANELLA REVOLUTA 'LITTLE REV' / LITTLE REV FLAX LILY DIETES X 'LEMON DROP' / FORTNIGHT LILY GALVEZIA SPECIOSA 'FIRECRACKER' / BUSH SNAPDRAGON	242	1 GAL 1 GAL 1 GAL
	<b>PERENNIAL COLOR</b> LAVANDULA STOECHAS / SPANISH LAVENDER LAVANDULA STOECHAS 'ALBA' / WHITE SPANISH LAVENDER	721	1 GAL 1 GAL
	<b>COLORFUL SCREENING SHRUB</b> CALLIANDRA CALIFORNICA / RED BAJA FAIRY DUSTER CARPENTERIA CALIFORNICA / BUSH ANEMONE	207	5 GAL 5 GAL
	<b>SMALL SHRUB</b> MYRTUS COMMUNIS 'COMPACTA' / DWARF MYRTLE PITOSPORUM TOBIRA 'WHEELER'S DWARF' / WHEELER'S DWARF PITOSPORUM	266	5 GAL 5 GAL
	<b>COLORFUL SIGNAGE SHRUBS</b> GALVEZIA SPECIOSA 'FIRECRACKER' / BUSH SNAPDRAGON JUSTICIA SPICIGERA / MOHINTLI	75	5 GAL 5 GAL
	<b>BASIN PLANTINGS</b> CAREX SPICATA / SAN DIEGO SEDGE JUNCUS PATENS 'ELK BLUE' / SPREADING RUSH	15,798 SF 4,105 4,105	1 GAL 1 GAL 24" oc 24" oc
	<b>SLOPE GROUNDCOVER</b> BACCHARIS PILULARIS 'TWIN PEAKS' / TWIN PEAKS COYOTE BRUSH MYOPORUM PARVIFOLIUM 'PINK' / TRAILING MYOPORUM	11,488 SF 11,939 747	FLAT 1 GAL 12" oc 48" oc
	<b>COLORFUL GROUNDCOVER</b> LANTANA MONTEVIDENSIS / TRAILING LANTANA LANTANA MONTEVIDENSIS 'SPREADING RADIATION' / TRAILING LANTANA	6,234 SF 405 405	1 GAL 1 GAL 48" oc 48" oc
	<b>STREET GROUNDCOVER</b> MYOPORUM PARVIFOLIUM 'PINK' / TRAILING MYOPORUM MYOPORUM PARVIFOLIUM 'WHITE' / WHITE TRAILING MYOPORUM	2,813 SF 183 183	1 GAL 1 GAL 48" oc 48" oc



WYNN-SMITH  
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INC.

12315 OAK KNOLL RD, SUITE 300  
P O BOX 92084  
TEL. 858.513.0030  
FAX. 858.513.0032

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OLD HIGHWAY 395  
RETAIL CENTER  
3233-3235 OLD HIGHWAY 395  
FALLBROOK, CA 92028

CONCEPTUAL  
PLANTING PLAN

Revisions	By
3-18-2021	

Drawn By: TEAM  
Checked By: \_\_\_\_\_  
Date Issued: 12-8-2020  
Project No.: 20-013  
Scale: \_\_\_\_\_

Sheet

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Of \_\_\_\_\_ Sheets





# Fallbrook Commercial Additional Photo Simulation

Project Address: 3233 Old Highway 395, Fallbrook, CA 92028  
APN 125-050-54



VISUAL SIMULATION: EXCEL ENGINEERING  
GRADING CONCEPT: EXCEL ENGINEERING  
ARCHITECTURE: EMPIRE DESIGN GROUP, INC  
LANDSCAPING CONCEPT: WYNN SMITH LANDSCAPE ARCHITECTURE, INC

# Photo Simulation 01



*Current View*



*Project Simulation View*

REMARKS: PROPOSED PROJECT VISIBLE AS SHOWN.

# Photo Simulation 02



*Current View*



*Project Simulation View*

REMARKS: PROPOSED PROJECT VISIBLE AS SHOWN.

# Photo Simulation 03



PHOTO FROM GOOGLE STREET VIEW  
IMAGERY DATE: 03/2021

*Current View*



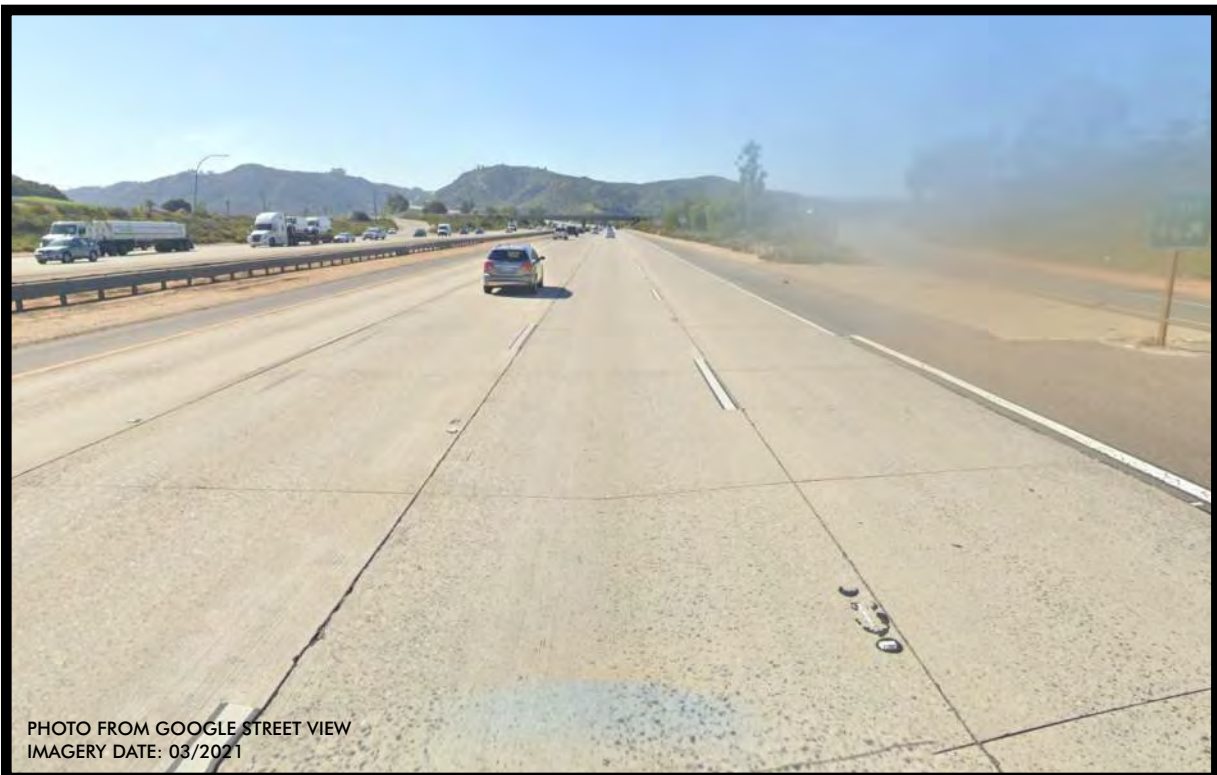
PHOTO FROM GOOGLE STREET VIEW  
IMAGERY DATE: 03/2021

**Project Simulation View**  
REMARKS: PROPOSED PROJECT NOT VISIBLE.

# Photo Simulation 04



*Current View*



**Project Simulation View**  
REMARKS: PROPOSED PROJECT NOT VISIBLE.

# Photo Simulation 05



CAMERA 35 MM EQUIVALENT FOCAL LENGTH = 50MM  
PHOTO TAKEN: 10/18/2021

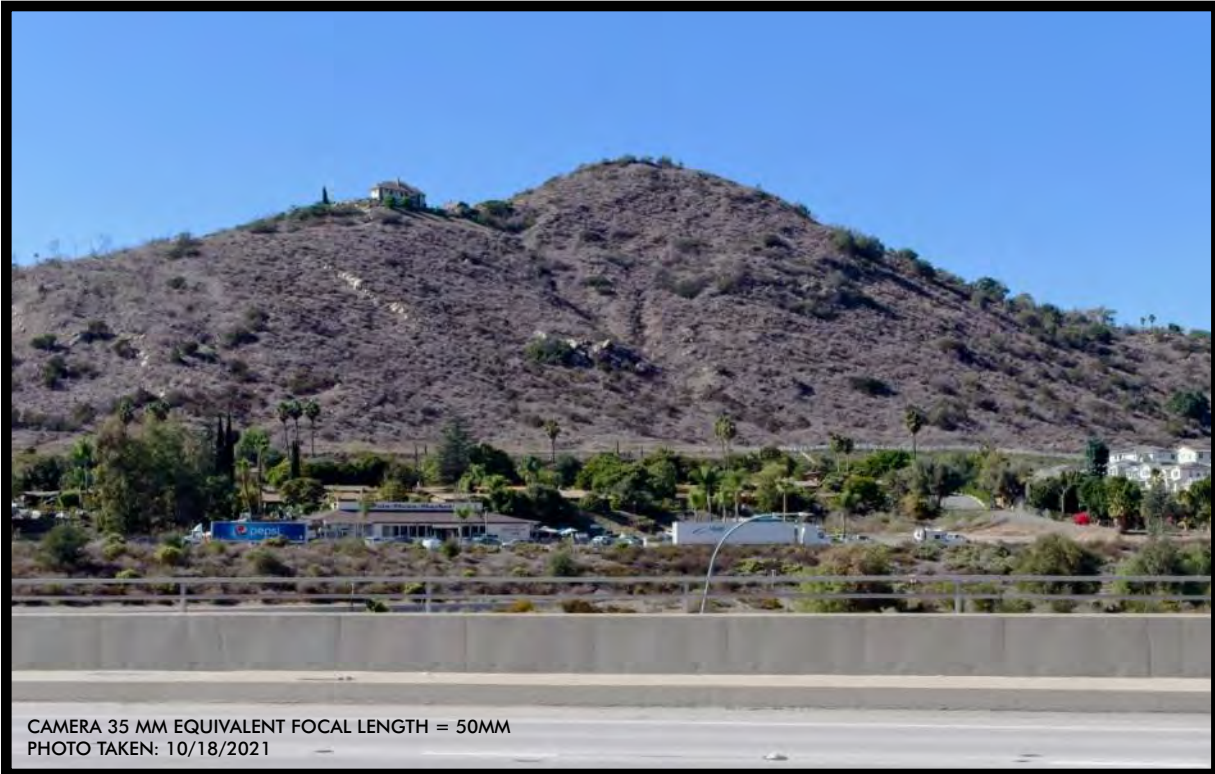
*Current View*



CAMERA 35 MM EQUIVALENT FOCAL LENGTH = 50MM  
PHOTO TAKEN: 10/18/2021

**Project Simulation View**  
REMARKS: PROPOSED PROJECT NOT VISIBLE.

# Photo Simulation 06



*Current View*



*Project Simulation View*

REMARKS: PROPOSED PROJECT VISIBLE AS SHOWN.



# Photo Simulation 07



*Current View*



*Project Simulation View*

REMARKS: PROPOSED PROJECT VISIBLE AS SHOWN.

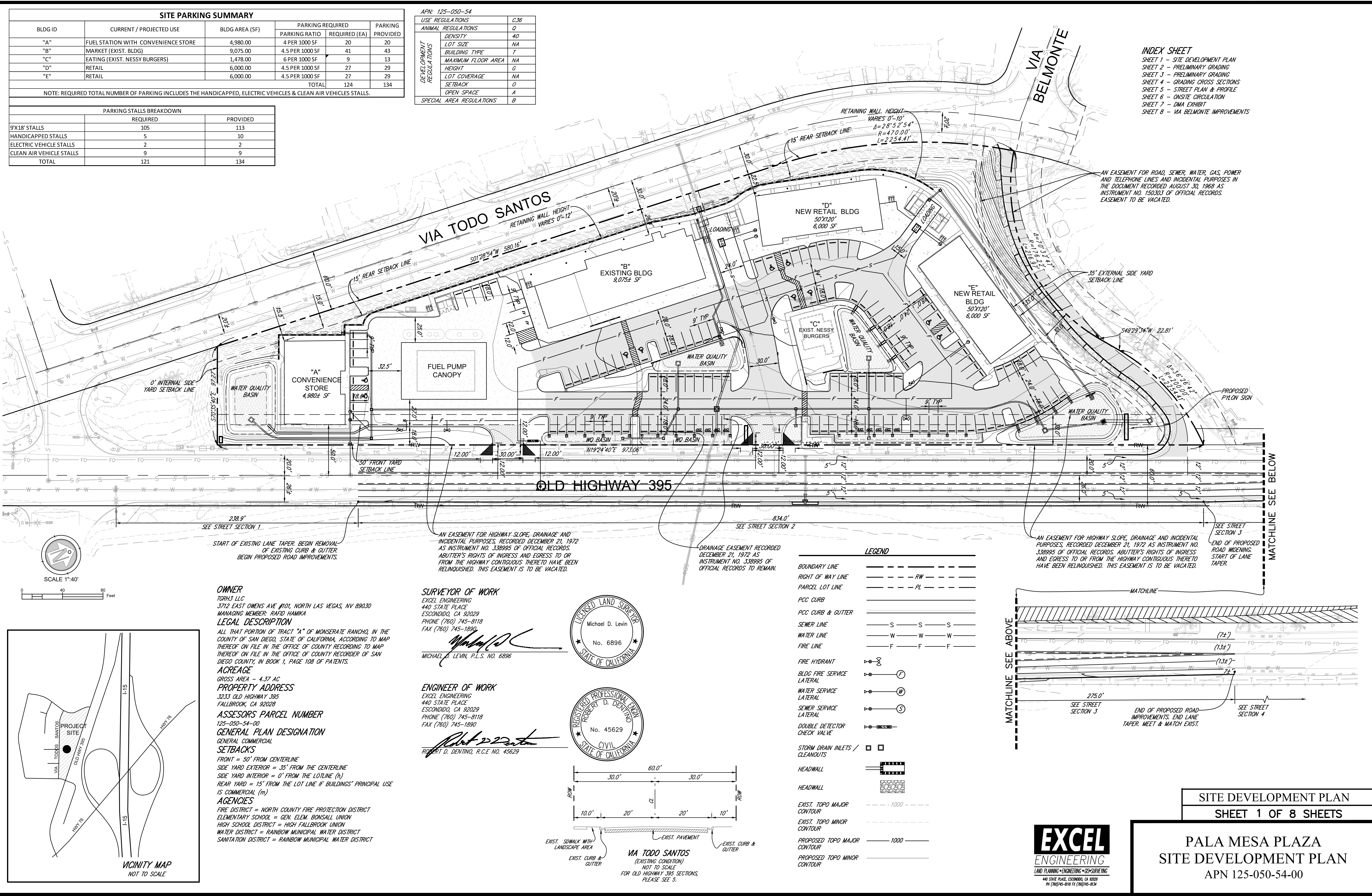
SITE PARKING SUMMARY					
BLDG ID	CURRENT / PROJECTED USE	BLDG AREA (SF)	PARKING REQUIRED		PARKING PROVIDED
			PARKING RATIO	REQUIRED (EA)	
"A"	FUEL STATION WITH CONVENIENCE STORE	4,980.00	4 PER 1000 SF	20	20
"B"	MARKET (EXIST. BLDG)	9,075.00	4.5 PER 1000 SF	41	43
"C"	EATING (EXIST. NESSY BURGERS)	1,478.00	6 PER 1000 SF	9	13
"D"	RETAIL	6,000.00	4.5 PER 1000 SF	27	29
"E"	RETAIL	6,000.00	4.5 PER 1000 SF	27	29
			TOTAL	124	134

NOTE: REQUIRED TOTAL NUMBER OF PARKING INCLUDES THE HANDICAPPED, ELECTRIC VEHICLES & CLEAN AIR VEHICLES STALLS.

PARKING STALLS BREAKDOWN		
	REQUIRED	PROVIDED
9'X18' STALLS	105	113
HANDICAPPED STALLS	5	10
ELECTRIC VEHICLE STALLS	2	2
CLEAN AIR VEHICLE STALLS	9	9
TOTAL	121	134

APN: 125-050-54	
USE REGULATIONS	C.36
ANIMAL REGULATIONS	0
DEVELOPMENT REGULATIONS	40
DENSITY	NA
LOT SIZE	NA
BUILDING TYPE	T
MAXIMUM FLOOR AREA	NA
HEIGHT	C
LOT COVERAGE	NA
SETBACK	0
OPEN SPACE	A
SPECIAL AREA REGULATIONS	B

**INDEX SHEET**  
 SHEET 1 - SITE DEVELOPMENT PLAN  
 SHEET 2 - PRELIMINARY GRADING  
 SHEET 3 - PRELIMINARY GRADING  
 SHEET 4 - GRADING CROSS SECTIONS  
 SHEET 5 - STREET PLAN & PROFILE  
 SHEET 6 - ON-SITE CIRCULATION  
 SHEET 7 - DMA EXHIBIT  
 SHEET 8 - VIA BELMONTE IMPROVEMENTS



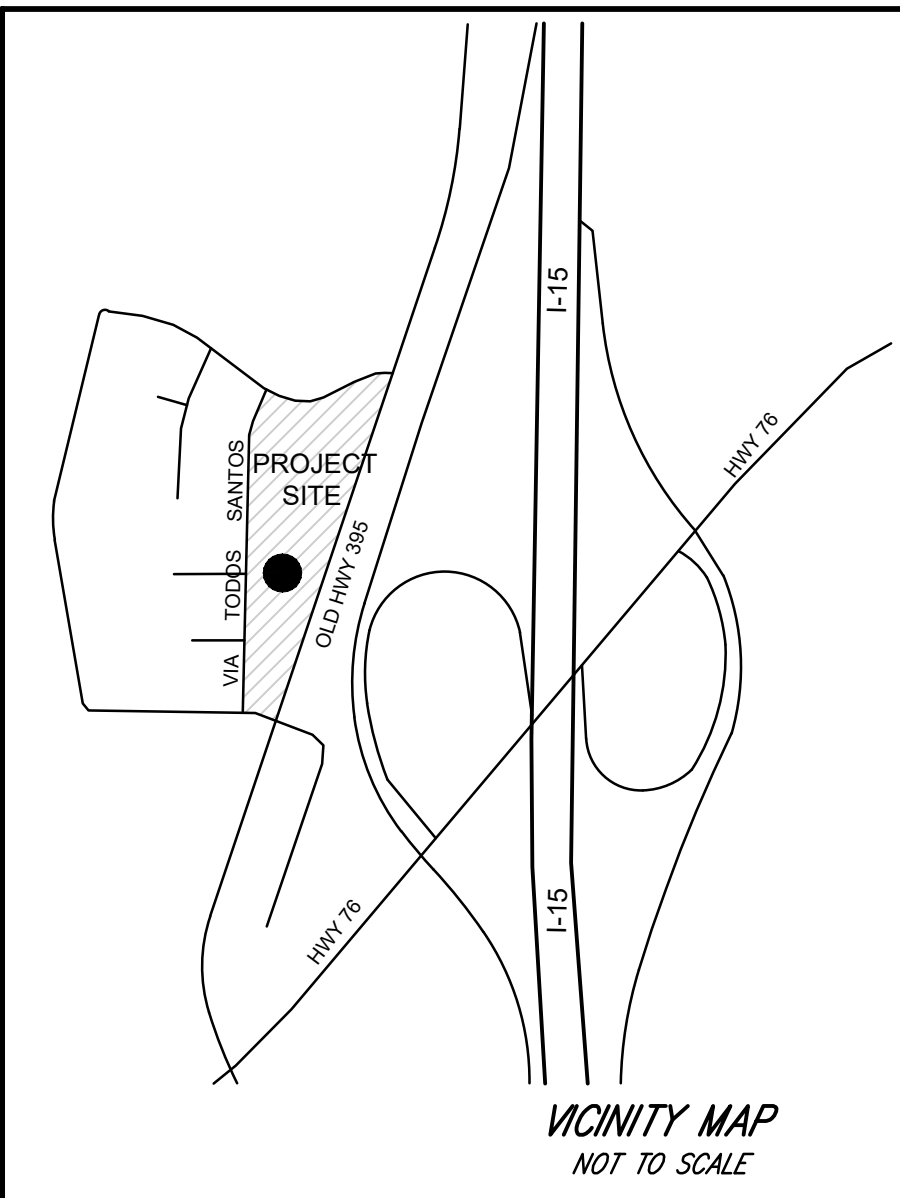
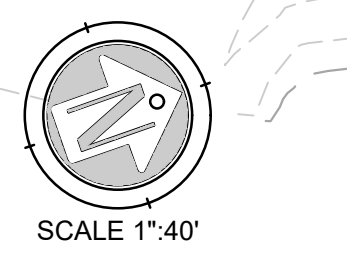
START OF EXISTING LANE TAPER. BEGIN REMOVAL OF EXISTING CURB & GUTTER. BEGIN PROPOSED ROAD IMPROVEMENTS.

AN EASEMENT FOR HIGHWAY SLOPE, DRAINAGE AND INCIDENTAL PURPOSES, RECORDED DECEMBER 21, 1972 AS INSTRUMENT NO. 338995 OF OFFICIAL RECORDS. ADJUTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM THE HIGHWAY CONTIGUOUS THERETO HAVE BEEN RELINQUISHED. THIS EASEMENT IS TO BE VACATED.

DRAINAGE EASEMENT RECORDED DECEMBER 21, 1972 AS INSTRUMENT NO. 338995 OF OFFICIAL RECORDS TO REMAIN.

AN EASEMENT FOR HIGHWAY SLOPE, DRAINAGE AND INCIDENTAL PURPOSES, RECORDED DECEMBER 21, 1972 AS INSTRUMENT NO. 338995 OF OFFICIAL RECORDS. ADJUTER'S RIGHTS OF INGRESS AND EGRESS TO OR FROM THE HIGHWAY CONTIGUOUS THERETO HAVE BEEN RELINQUISHED. THIS EASEMENT IS TO BE VACATED.

END OF PROPOSED ROAD WIDENING. START OF LANE TAPER.



**OWNER**  
 TORH3 LLC  
 3712 EAST OWENS AVE #101, NORTH LAS VEGAS, NV 89030  
 MANAGING MEMBER: RAFID HAMKA

**LEGAL DESCRIPTION**  
 ALL THAT PORTION OF TRACT "A" OF MONSERATE RANCHO, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF ON FILE IN THE OFFICE OF COUNTY RECORDER TO MAP THEREOF ON FILE IN THE OFFICE OF COUNTY RECORDER OF SAN DIEGO COUNTY, IN BOOK 1, PAGE 108 OF PATENTS.

**ACREAGE**  
 GROSS AREA - 4.37 AC

**PROPERTY ADDRESS**  
 3233 OLD HIGHWAY 395  
 FALLBROOK, CA 92028

**ASSESSORS PARCEL NUMBER**  
 125-050-54-00

**GENERAL PLAN DESIGNATION**  
 GENERAL COMMERCIAL

**SETBACKS**  
 FRONT = 50' FROM CENTERLINE  
 SIDE YARD EXTERIOR = 35' FROM THE CENTERLINE  
 SIDE YARD INTERIOR = 0' FROM THE LOTLINE (h)  
 REAR YARD = 15' FROM THE LOT LINE IF BUILDINGS' PRINCIPAL USE IS COMMERCIAL (m)

**AGENCIES**  
 FIRE DISTRICT = NORTH COUNTY FIRE PROTECTION DISTRICT  
 ELEMENTARY SCHOOL = GEN. ELEM. BONSAI UNION  
 HIGH SCHOOL DISTRICT = HIGH FALLBROOK UNION  
 WATER DISTRICT = RAINBOW MUNICIPAL WATER DISTRICT  
 SANITATION DISTRICT = RAINBOW MUNICIPAL WATER DISTRICT

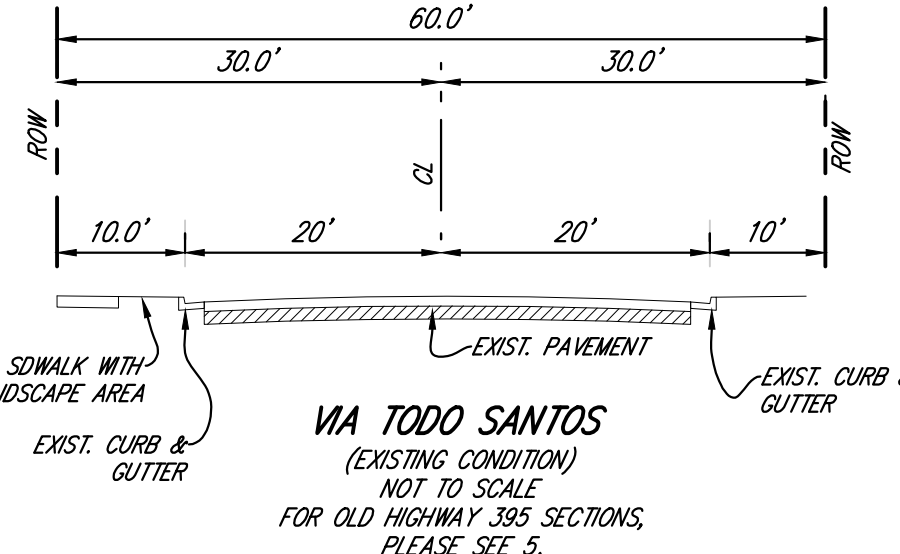
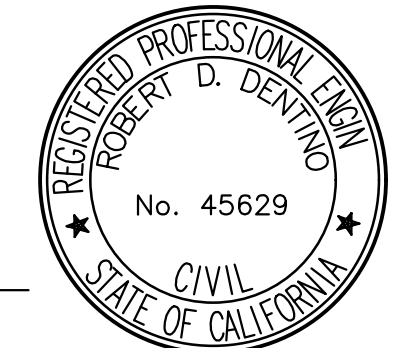
**SURVEYOR OF WORK**  
 EXCEL ENGINEERING  
 440 STATE PLACE  
 ESCONDI DO, CA 92029  
 PHONE (760) 745-8118  
 FAX (760) 745-1890

MICHAEL D. LEVIN, P.L.S. NO. 6896

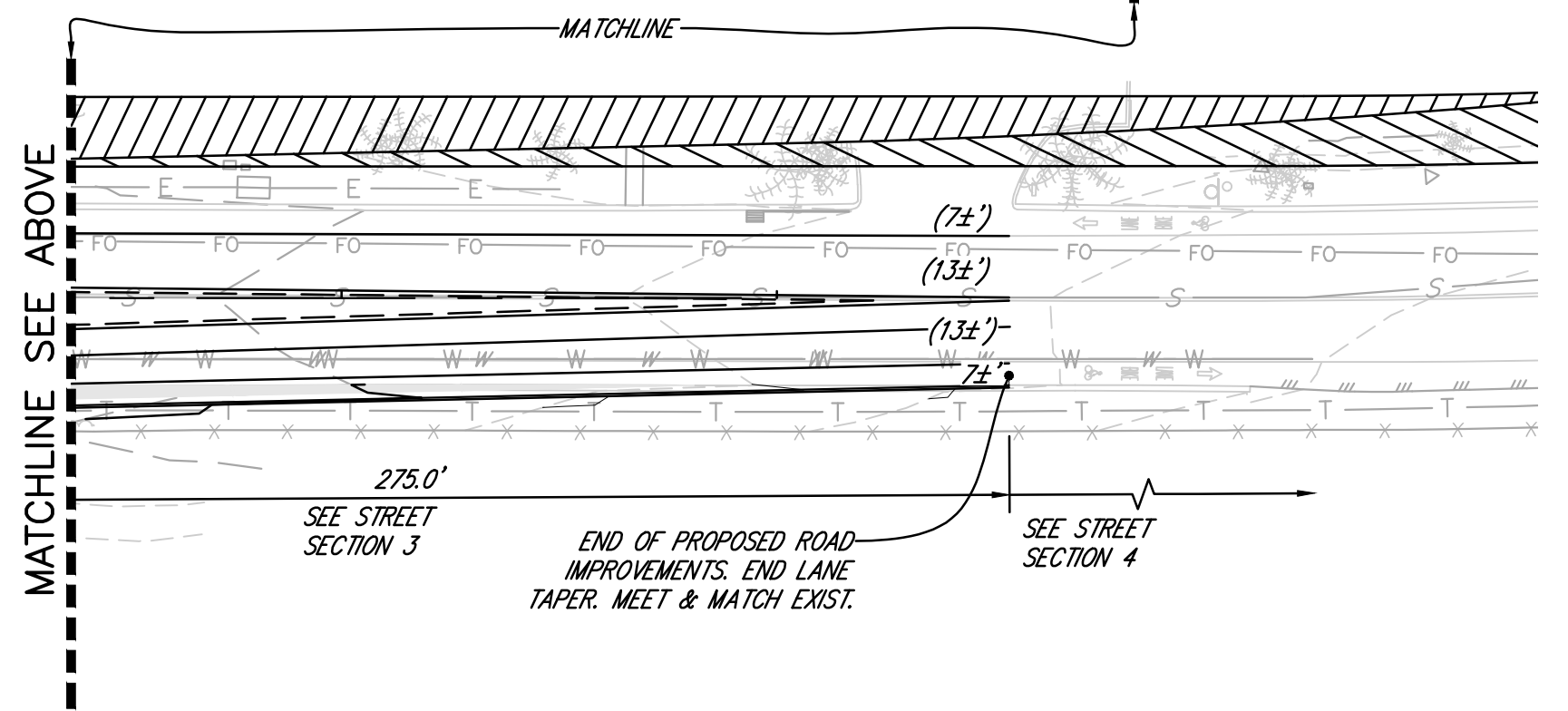


**ENGINEER OF WORK**  
 EXCEL ENGINEERING  
 440 STATE PLACE  
 ESCONDI DO, CA 92029  
 PHONE (760) 745-8118  
 FAX (760) 745-1890

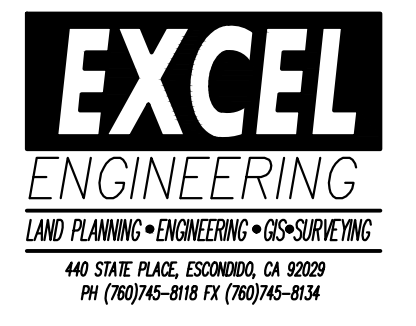
ROBERT D. DENVING, R.C.E. NO. 45629



- LEGEND**
- BOUNDARY LINE
  - RIGHT OF WAY LINE
  - PARCEL LOT LINE
  - PCC CURB
  - PCC CURB & GUTTER
  - SEWER LINE
  - WATER LINE
  - FIRE LINE
  - FIRE HYDRANT
  - BLDG FIRE SERVICE LATERAL
  - WATER SERVICE LATERAL
  - SEWER SERVICE LATERAL
  - DOUBLE DETECTOR CHECK VALVE
  - STORM DRAIN INLETS / CLEANOUTS
  - HEADWALL
  - EXIST. TOPO MAJOR CONTOUR
  - EXIST. TOPO MINOR CONTOUR
  - PROPOSED TOPO MAJOR CONTOUR
  - PROPOSED TOPO MINOR CONTOUR



SITE DEVELOPMENT PLAN  
 SHEET 1 OF 8 SHEETS



PALA MESA PLAZA  
 SITE DEVELOPMENT PLAN  
 APN 125-050-54-00

K:\19\190225\Engineering\SDP\SDP\_01\SDP\_190225\_SDP\_TITL.dwg 5/7/2024 5:00 PM ORIGINAL PLOT SIZE: 11x17

**EARTHWORK QUANTITIES:**  
THE PRISMATICAL VOLUME METHOD WAS USED TO CALCULATE THE VALUES SHOWN HEREON. SECTIONS OF STREETS, BIO BASINS & UTILITIES, NO BULKING OR SHRINKAGE WERE CONSIDERED IN THIS EARTHWORK QUANTITIES.

ADJUSTED CUT VOLUME = 7,725 CY  
ADJUSTED FILL VOLUME = 5,575 CY  
EXPORT = 2,150 CY

**PRELIMINARY GRADING NOTE:**  
THIS PLAN IS PROVIDED TO ALLOW FOR FULL & ADEQUATE DISCRETIONARY REVIEW OF A PROPOSED DEVELOPMENT PROJECT. THE PROPERTY OWNER ACKNOWLEDGES THAT ACCEPTANCE OR APPROVAL OF THIS PLAN DOES NOT CONSTITUTE AN APPROVAL TO PERFORM ANY GRADING SHOWN HEREON, AND AGREES TO OBTAIN VALID GRADING PERMITS BEFORE COMMENCING SUCH ACTIVITY.

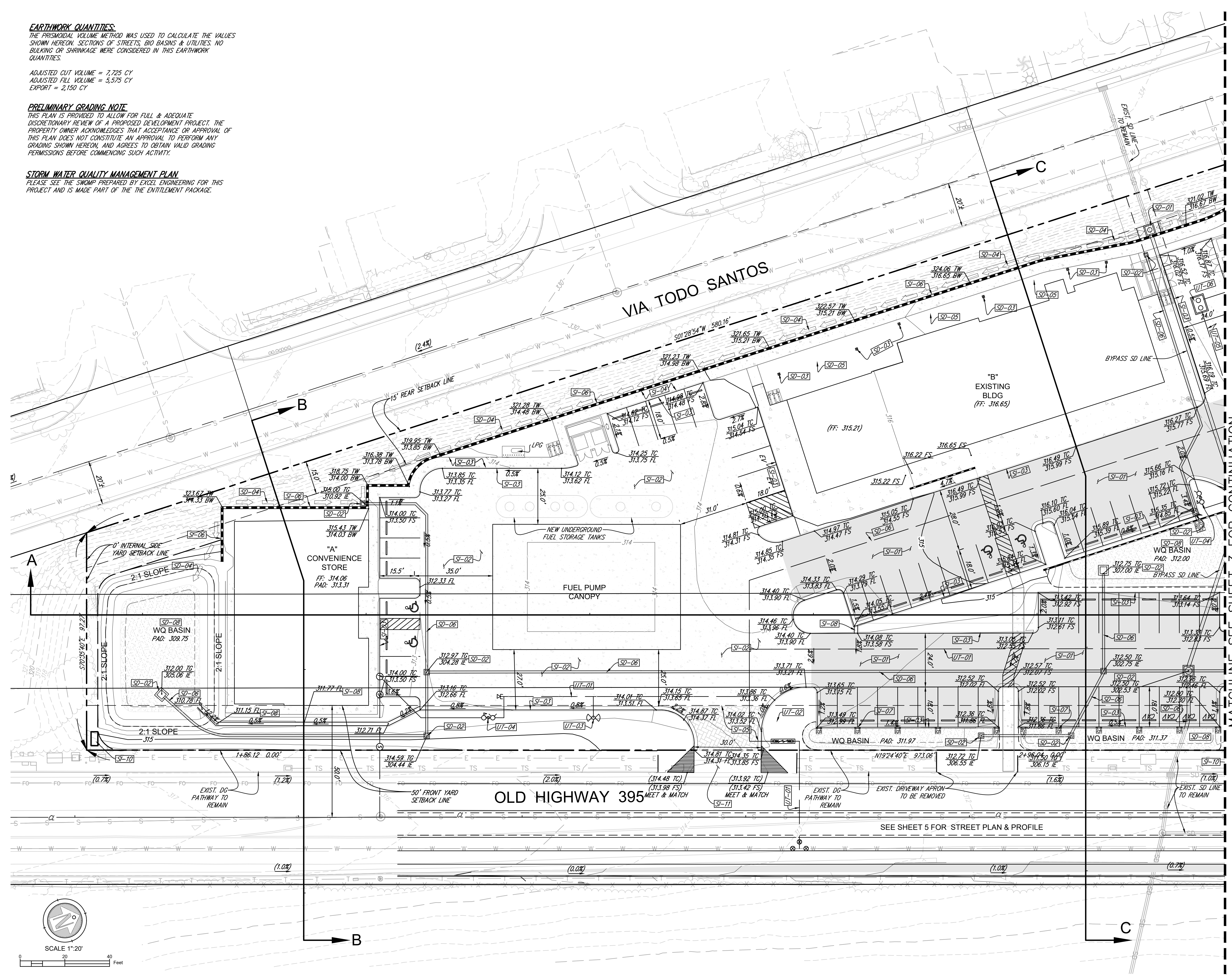
**STORM WATER QUALITY MANAGEMENT PLAN:**  
PLEASE SEE THE SWAMP PREPARED BY EXCEL ENGINEERING FOR THIS PROJECT AND IS MADE PART OF THE ENTITLEMENT PACKAGE.

**LEGEND**

- BOUNDARY LINE ————
- RIGHT OF WAY LINE ———— RW ————
- PARCEL LOT LINE ———— PL ————
- PCC CURB ————
- PCC CURB & GUTTER ————
- SEWER LINE — S — S — S —
- WATER LINE — W — W — W —
- FIRE LINE — F — F — F —
- FIRE HYDRANT — (Symbol)
- BLDG FIRE SERVICE LATERAL — (Symbol)
- WATER SERVICE LATERAL — (Symbol)
- SEWER SERVICE LATERAL — (Symbol)
- DOUBLE DETECTOR CHECK VALVE — (Symbol)
- STORM DRAIN INLETS / CLEANOUTS — (Symbol)
- HEADWALL — (Symbol)
- HEADWALL — (Symbol)
- EXIST. TOPO MAJOR CONTOUR ———— 1000 ————
- EXIST. TOPO MINOR CONTOUR ————
- PROPOSED TOPO MAJOR CONTOUR ———— 1000 ————
- PROPOSED TOPO MINOR CONTOUR ————

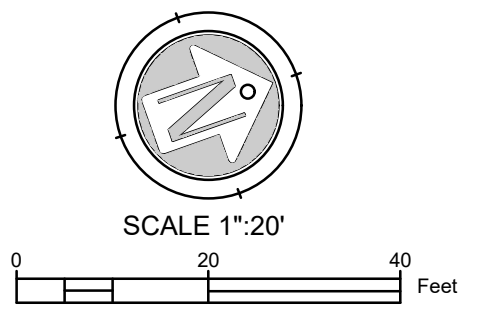
**CONSTRUCTION NOTES**

- SI-01 AC PAVEMENT
- SI-02 PCC PAVEMENT
- SI-03 PCC CURB / CURB & GUTTER
- SI-04 PCC SIDEWALK
- SI-05 DECORATIVE PAVERS
- SI-06 RETAINING WALL
- SI-07 1.5" WIDE CURB/CLUT 18" O.C. ALONG THE LENGTH OF THE CURB & GUTTER WITH HIP/RAP PROTECTION INSTALLED ALL THE WAY TO THE BASIN.
- SI-08 1.5" WIDE PCC CHANNEL
- SI-09 SIDEWALK UNDERDRAIN
- SI-10 MONUMENT SIGN
- SI-11 30" WIDE PCC DRIVEWAY PER SDRSD G-14
- SD-01 BROOKS BOX OR EQUIVALENT
- SD-02 AREA DRAIN
- SD-03 BROW DITCH
- SD-04 8" PVC PIPE
- SD-05 PVC STORM DRAIN PIPE
- SD-06 18" RCP STORM DRAIN PIPE
- SD-07 WATER QUALITY BASIN
- UT-01 INSTALL FIRE LINE
- UT-02 INSTALL DDCV
- UT-03 INSTALL FIRE HYDRANT
- UT-04 INSTALL FDC/PIV
- UT-05 NEW SEWER LINE
- UT-06 GREASE TRAP



MATCHLINE SEE SHEET 3 FOR CONTINUATION

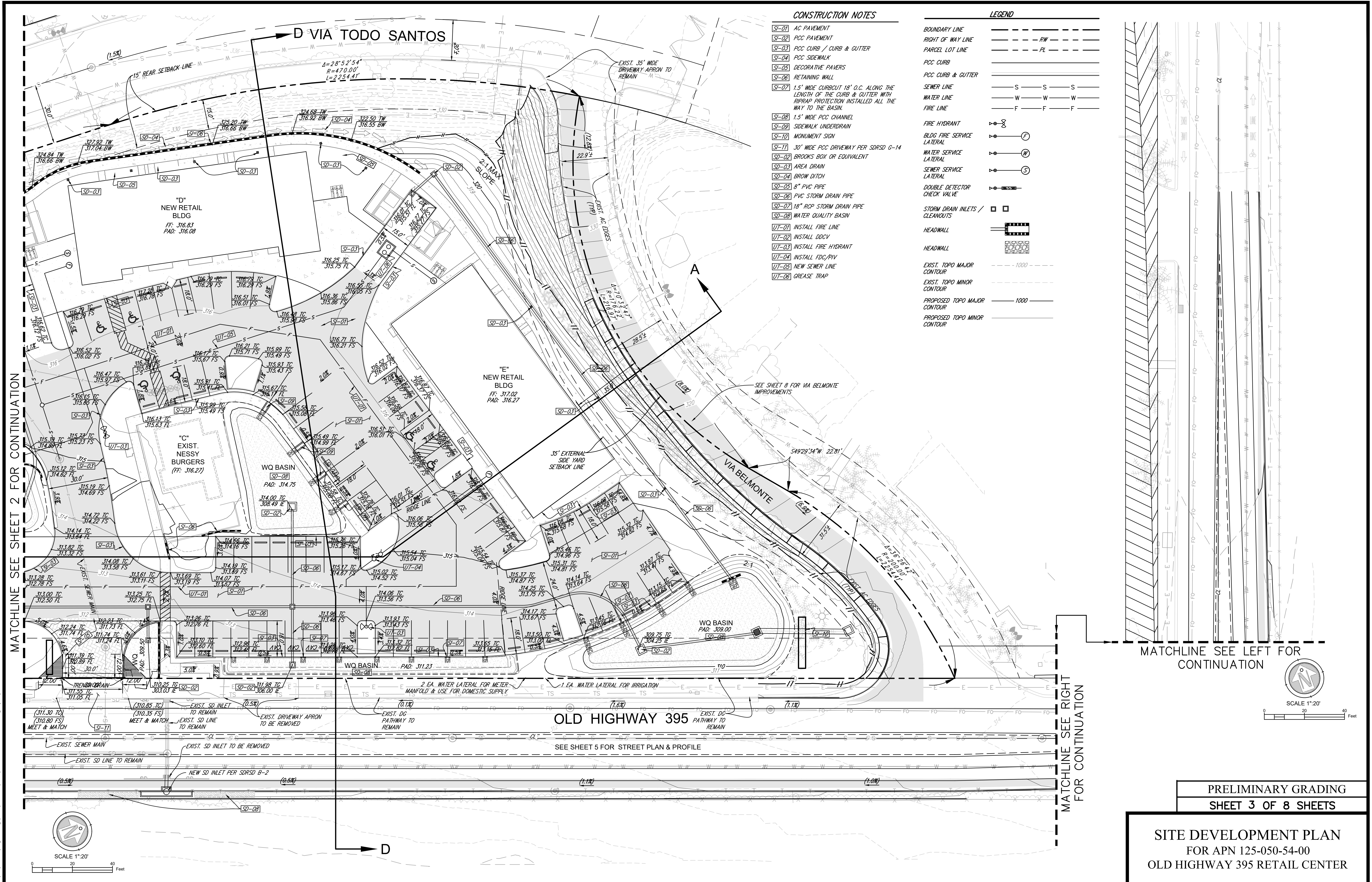
SEE SHEET 5 FOR STREET PLAN & PROFILE



**PRELIMINARY GRADING**  
**SHEET 2 OF 8 SHEETS**

**SITE DEVELOPMENT PLAN**  
FOR APN 125-050-54-00  
**OLD HIGHWAY 395 RETAIL CENTER**

1/19/2025 Engineering\SDP\SDP\_01\SDP\_19025\_SDP\_GRAVING.dwg 4/1/2024 4:51 PM ORIGINAL PLOT SIZE: ---



CONSTRUCTION NOTES

- SD-01 AC PAVEMENT
- SD-02 PCC PAVEMENT
- SD-03 PCC CURB / CURB & GUTTER
- SD-04 PCC SIDEWALK
- SD-05 DECORATIVE PAVERS
- SD-06 RETAINING WALL
- SD-07 1.5" WIDE CURB/CUT 18" O.C. ALONG THE LENGTH OF THE CURB & GUTTER WITH RIPRAP PROTECTION INSTALLED ALL THE WAY TO THE BASIN.
- SD-08 1.5" WIDE PCC CHANNEL
- SD-09 SIDEWALK UNDERDRAIN
- SD-10 MONUMENT SIGN
- SD-11 30" WIDE PCC DRIVEWAY PER SDRSD G-14
- SD-12 BROOKS BOX OR EQUIVALENT
- SD-13 AREA DRAIN
- SD-14 BROW DITCH
- SD-15 8" PVC PIPE
- SD-16 PVC STORM DRAIN PIPE
- SD-17 18" RCP STORM DRAIN PIPE
- SD-18 WATER QUALITY BASIN
- UT-01 INSTALL FIRE LINE
- UT-02 INSTALL DDCV
- UT-03 INSTALL FIRE HYDRANT
- UT-04 INSTALL FDC/PV
- UT-05 NEW SEWER LINE
- UT-06 GREASE TRAP

LEGEND

- BOUNDARY LINE
- RIGHT OF WAY LINE
- PARCEL LOT LINE
- PCC CURB
- PCC CURB & GUTTER
- SEWER LINE
- WATER LINE
- FIRE LINE
- FIRE HYDRANT
- BLDG FIRE SERVICE LATERAL
- WATER SERVICE LATERAL
- SEWER SERVICE LATERAL
- DOUBLE DETECTOR CHECK VALVE
- STORM DRAIN INLETS / CLEANOUTS
- HEADWALL
- HEADWALL
- EXIST. TOPO MAJOR CONTOUR
- EXIST. TOPO MINOR CONTOUR
- PROPOSED TOPO MAJOR CONTOUR
- PROPOSED TOPO MINOR CONTOUR

MATCHLINE SEE SHEET 2 FOR CONTINUATION

MATCHLINE SEE RIGHT FOR CONTINUATION

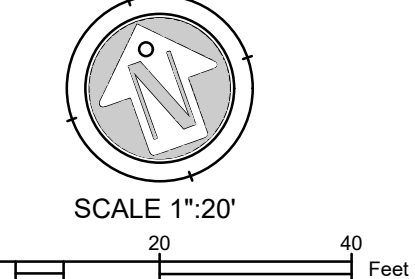
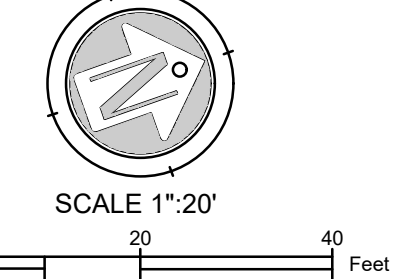
MATCHLINE SEE LEFT FOR CONTINUATION

OLD HIGHWAY 395

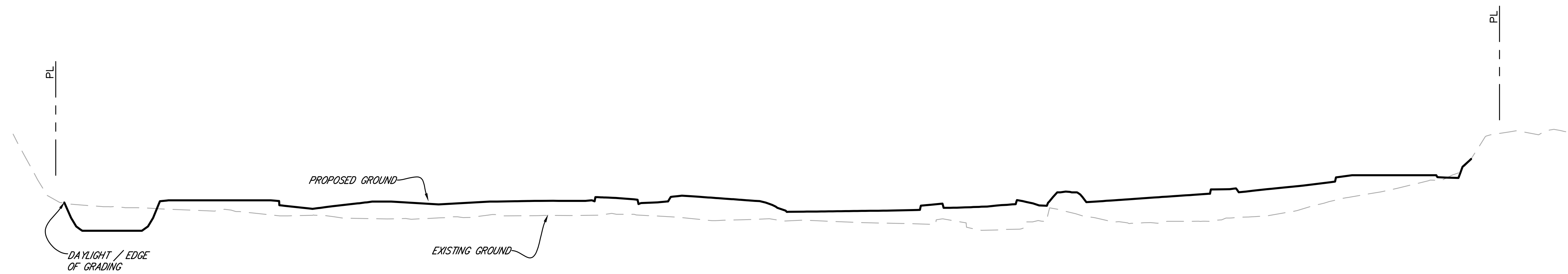
SEE SHEET 5 FOR STREET PLAN & PROFILE

PRELIMINARY GRADING SHEET 3 OF 8 SHEETS

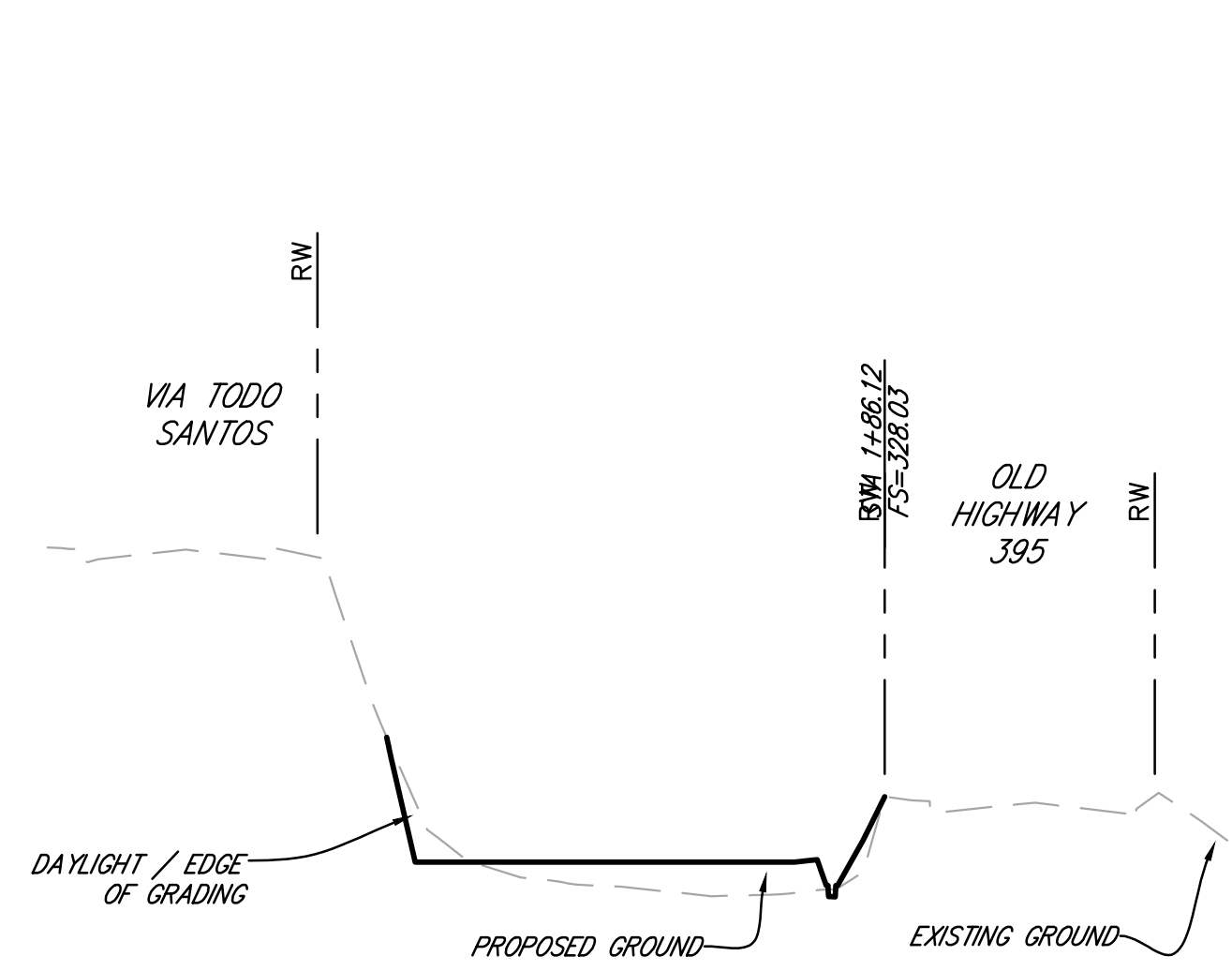
SITE DEVELOPMENT PLAN FOR APN 125-050-54-00 OLD HIGHWAY 395 RETAIL CENTER



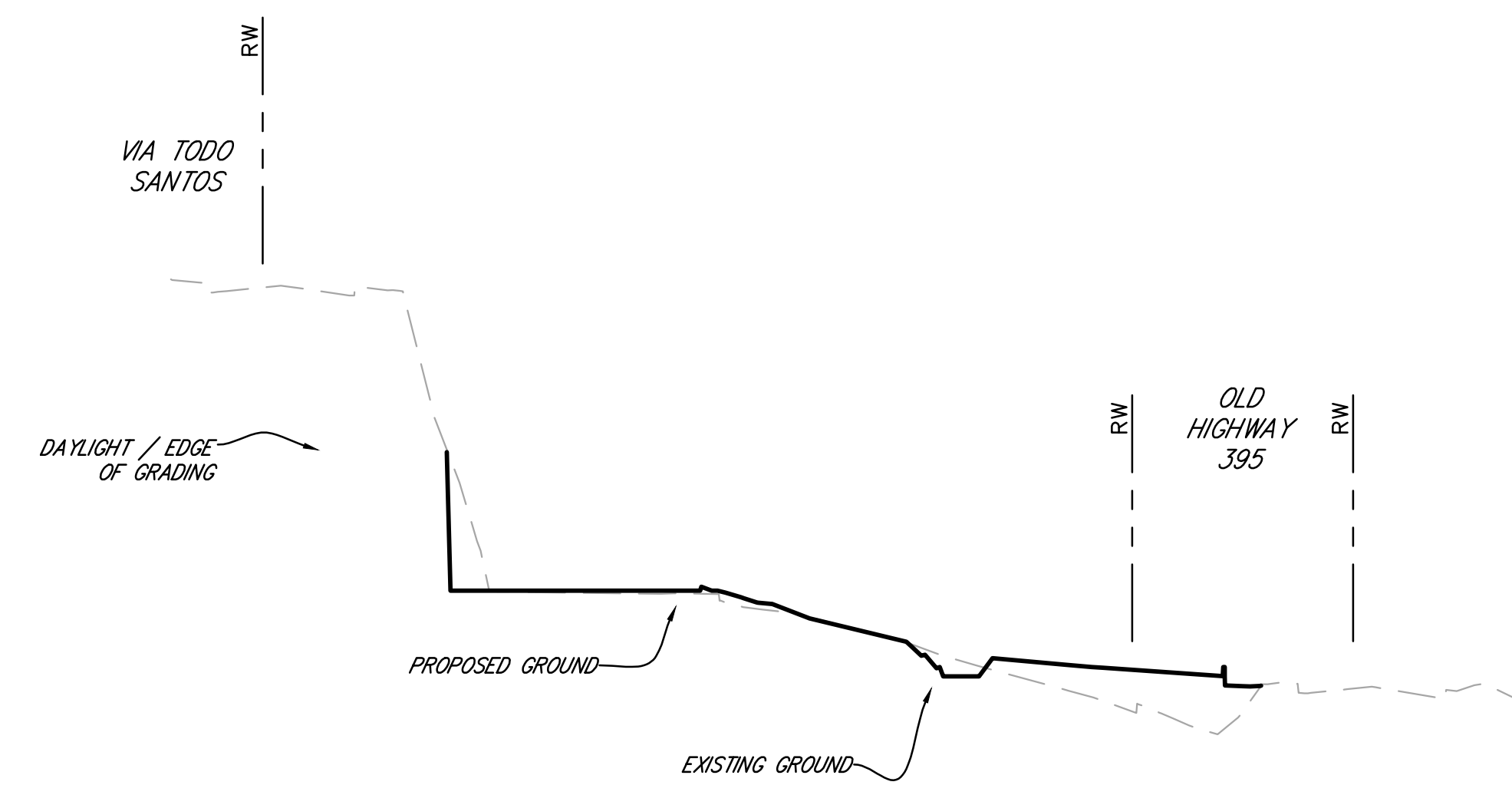
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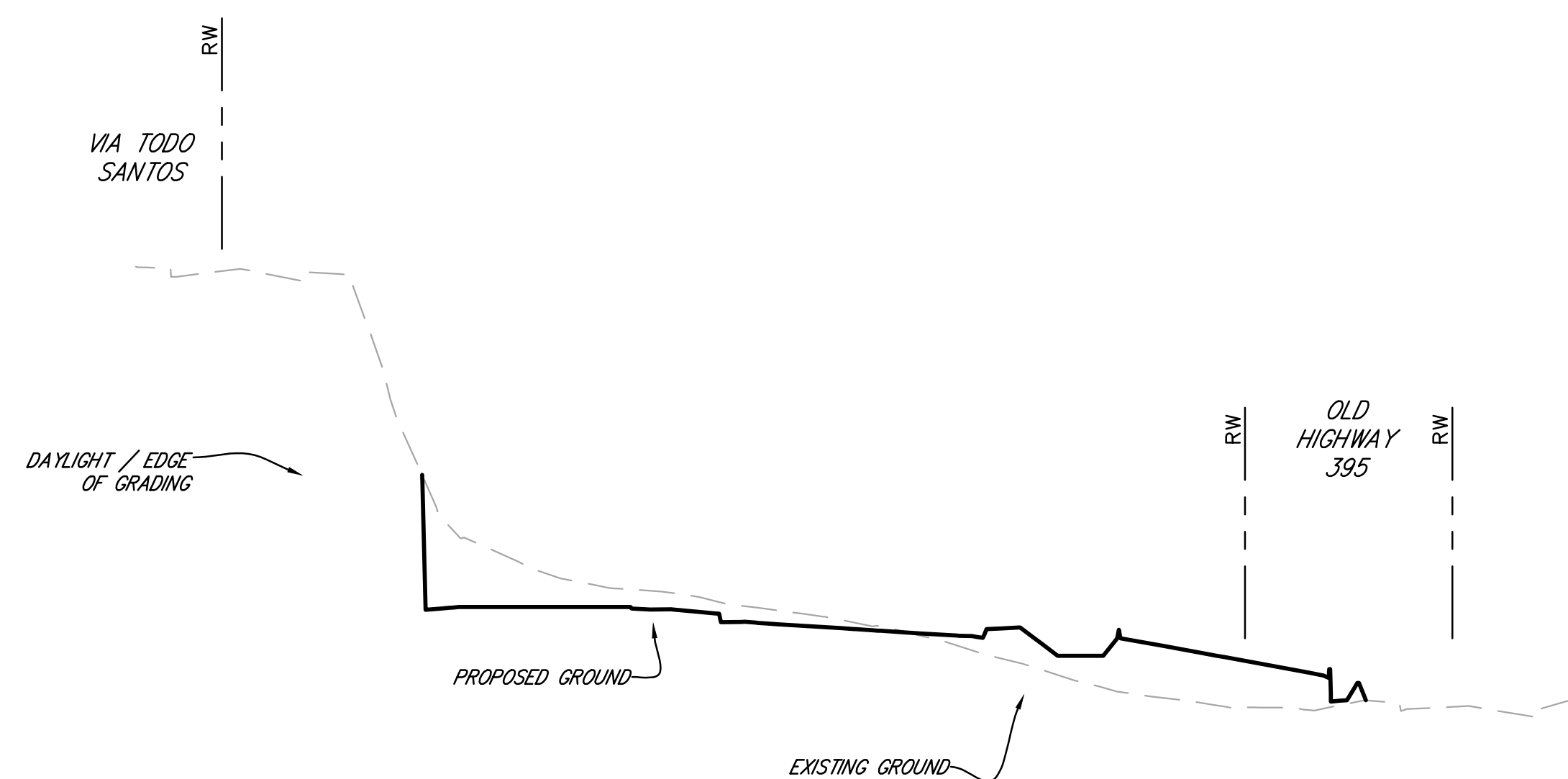
SECTION A-A  
 SCALE HORIZONTAL = 1":40'  
 VERTICAL = 1":8'  
 SEE SHEET 2 & 3



SECTION B-B  
 SCALE HORIZONTAL = 1":40'  
 VERTICAL = 1":8'  
 SEE SHEET 2



SECTION C-C  
 SCALE HORIZONTAL = 1":40'  
 VERTICAL = 1":8'  
 SEE SHEET 2

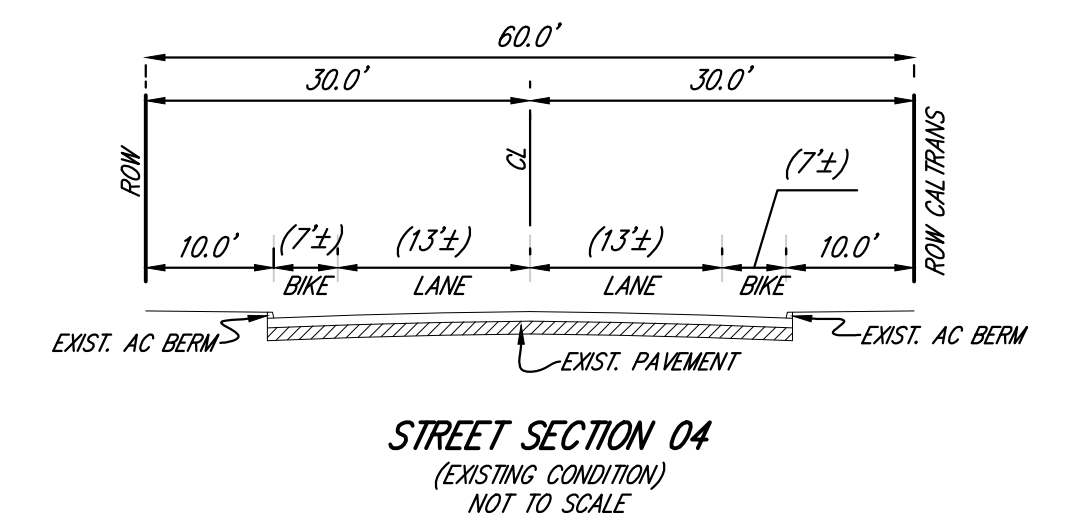
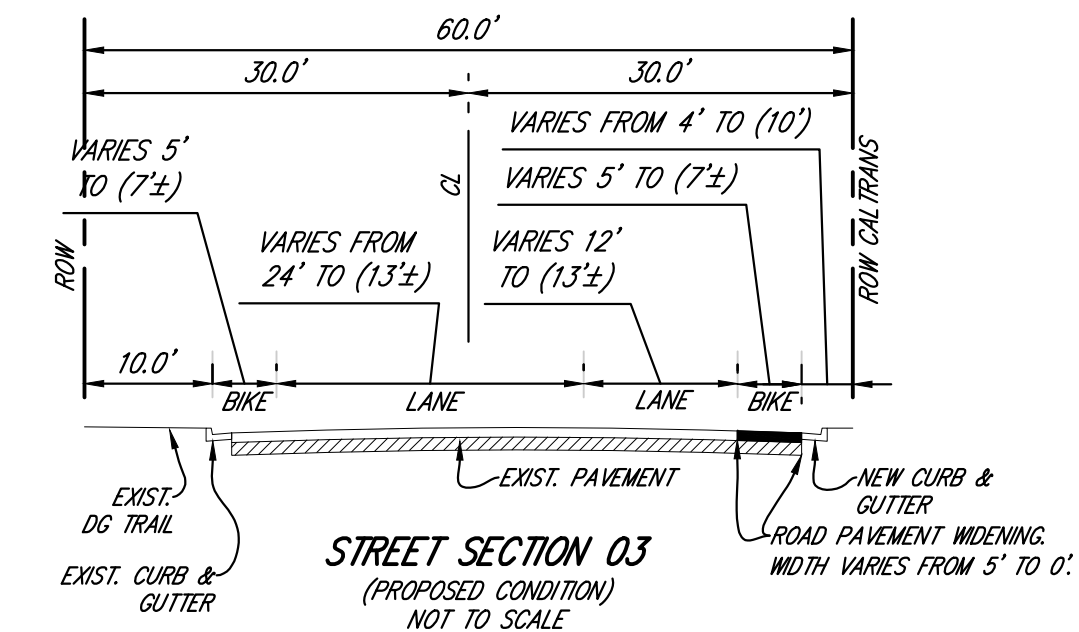
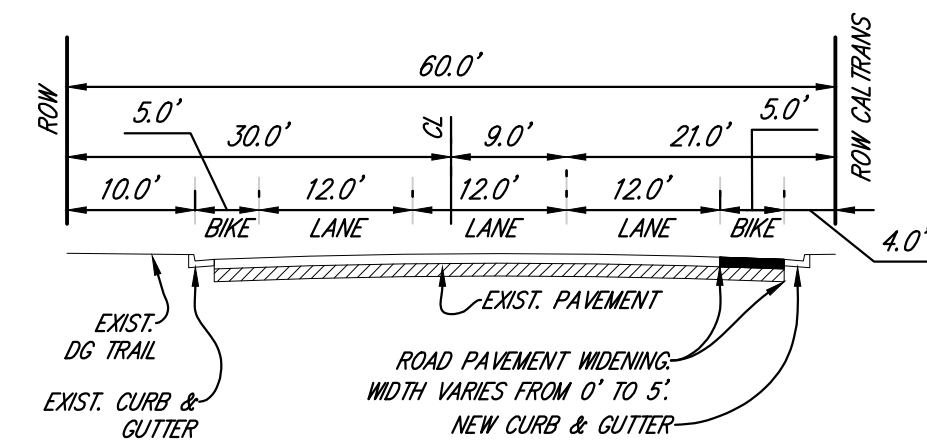
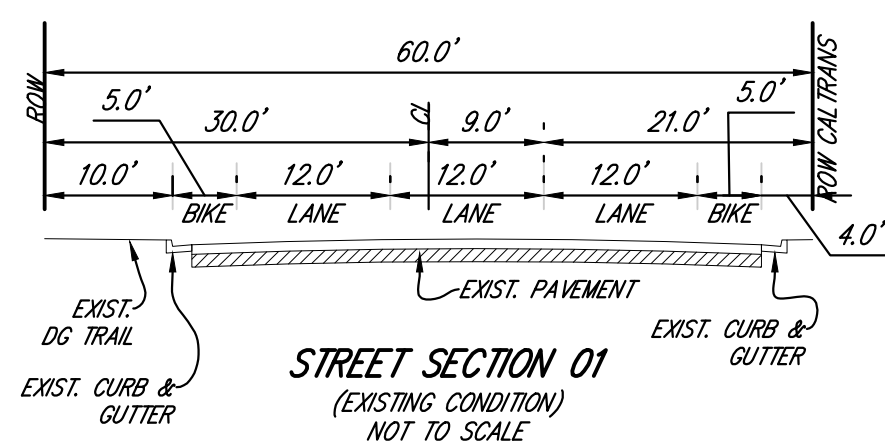
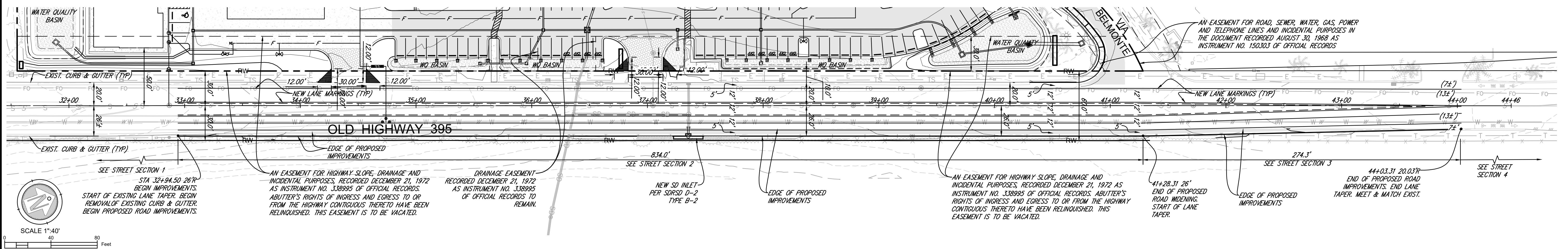
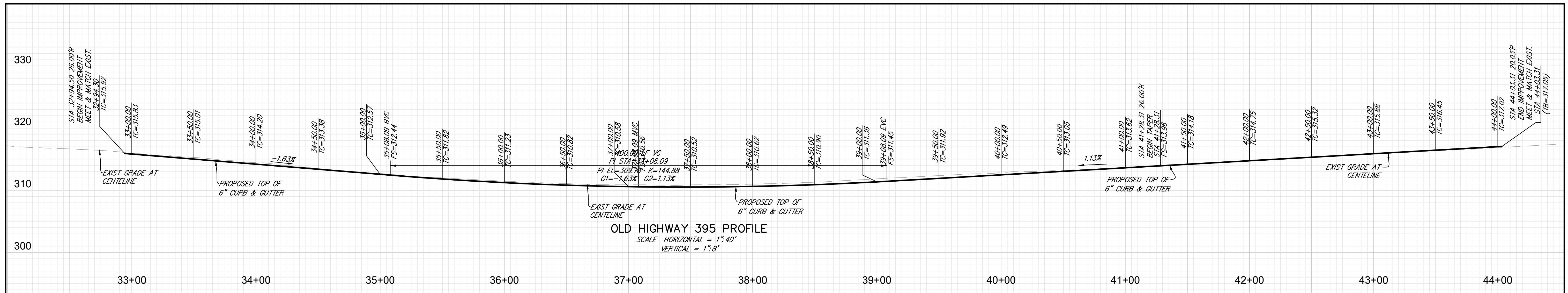


SECTION D-D  
 SCALE HORIZONTAL = 1":40'  
 VERTICAL = 1":8'  
 SEE SHEET 3

GRADING CROSS SECTIONS  
 SHEET 4 OF 8 SHEETS

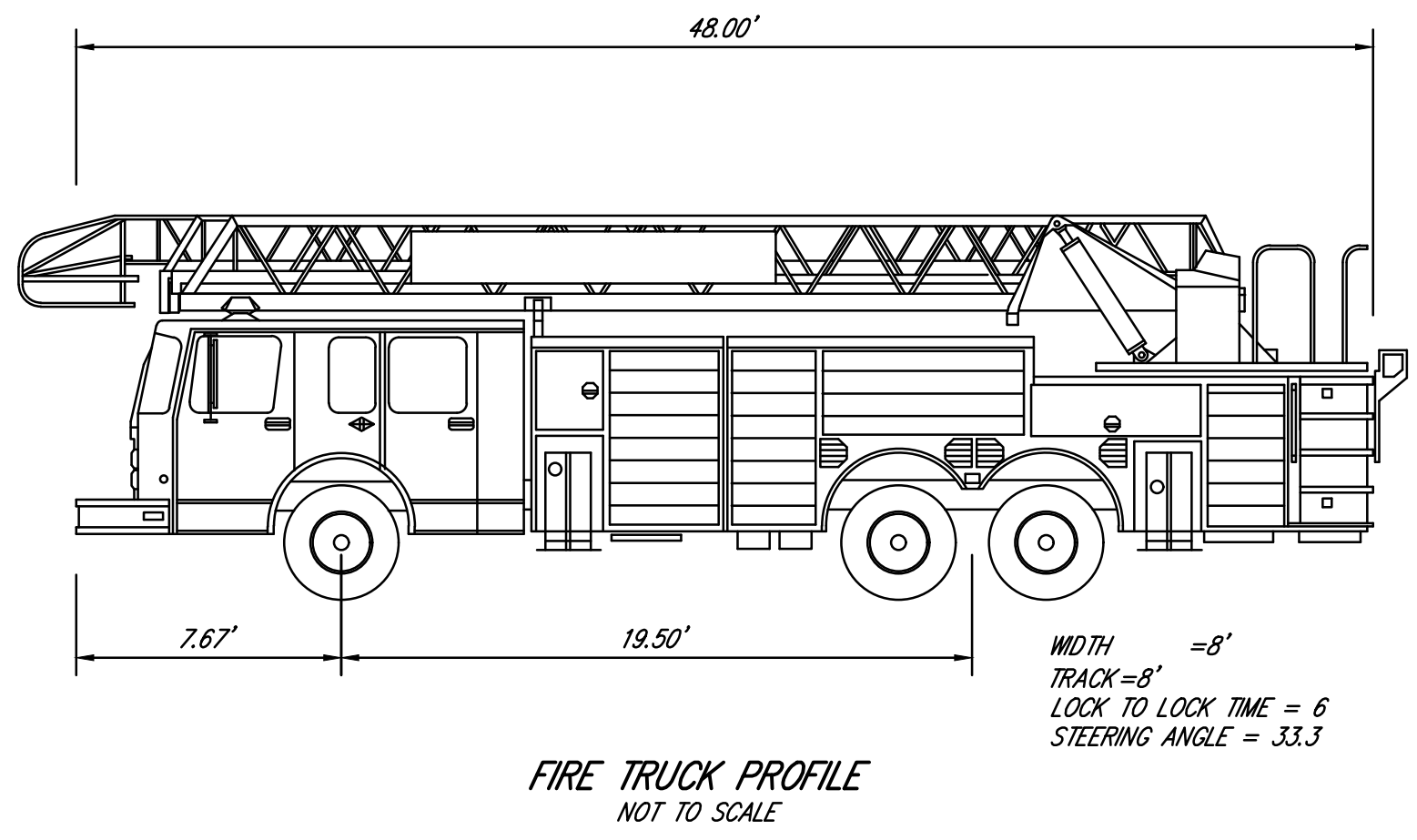
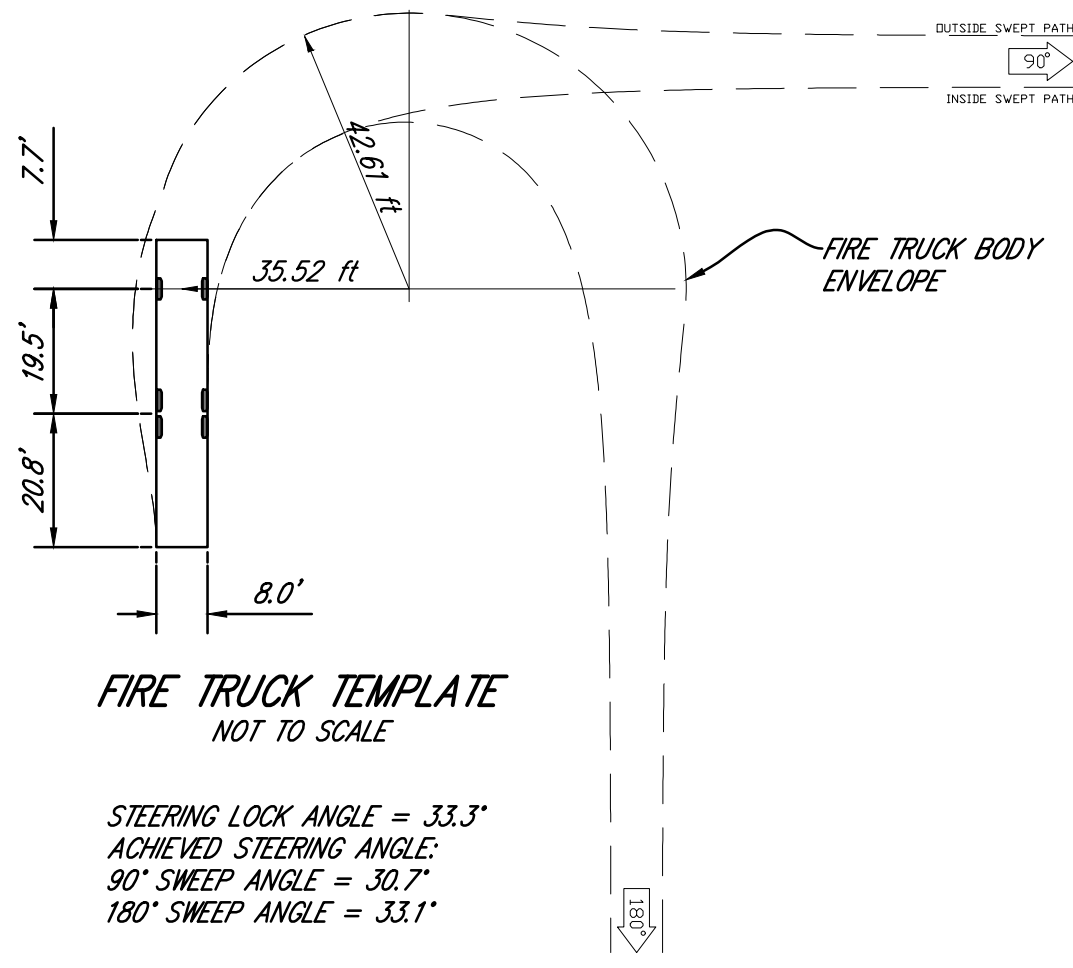
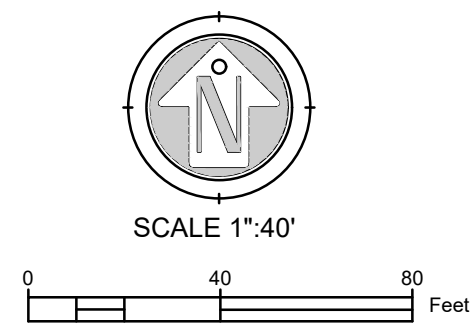
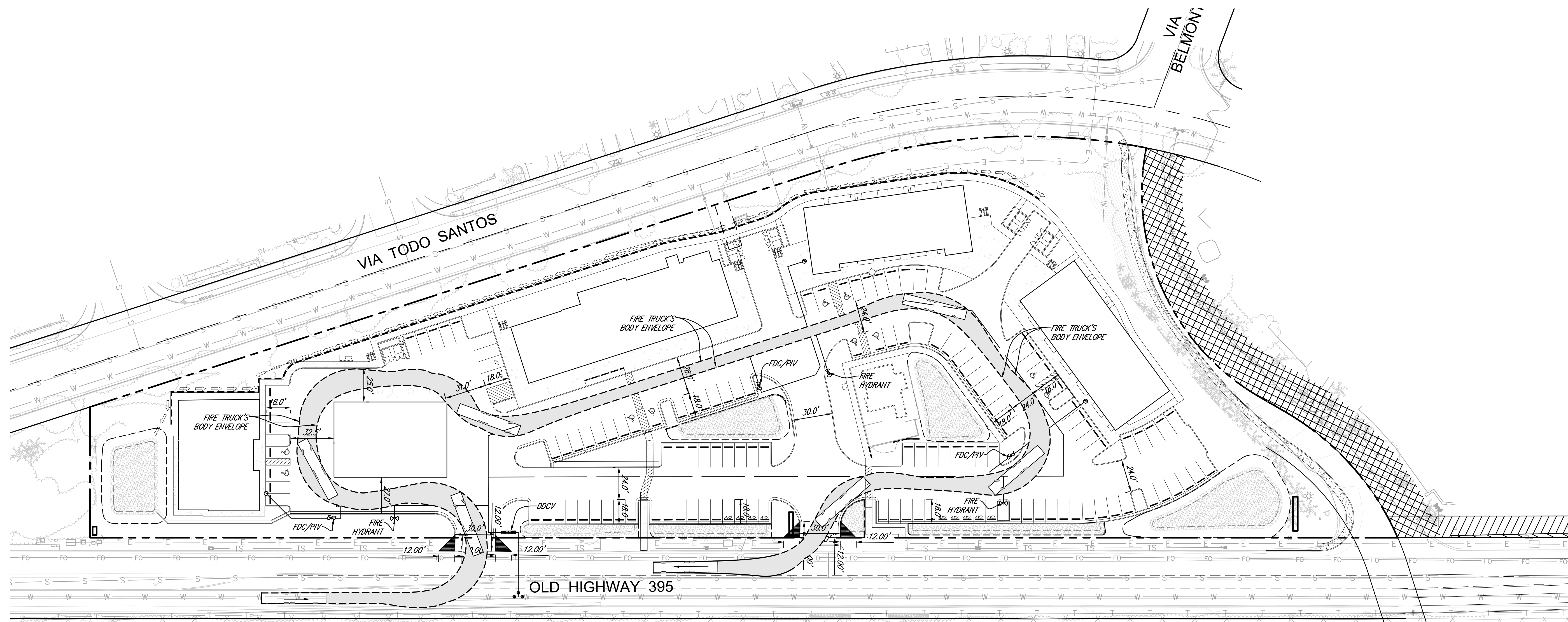
SITE DEVELOPMENT PLAN  
 FOR APN 125-050-54-00  
 OLD HIGHWAY 395 RETAIL CENTER

k:\19\19025\Engineering\SDP\SDP 01\SDP 19025 SDP GRADING.dwg 7/24/2024 4:06 PM ORIGINAL PLOT SIZE: -----



STREET PLAN & PROFILE  
SHEET 5 OF 8 SHEETS

PALA MESA PLAZA  
SITE DEVELOPMENT PLAN  
APN 125-050-54-00



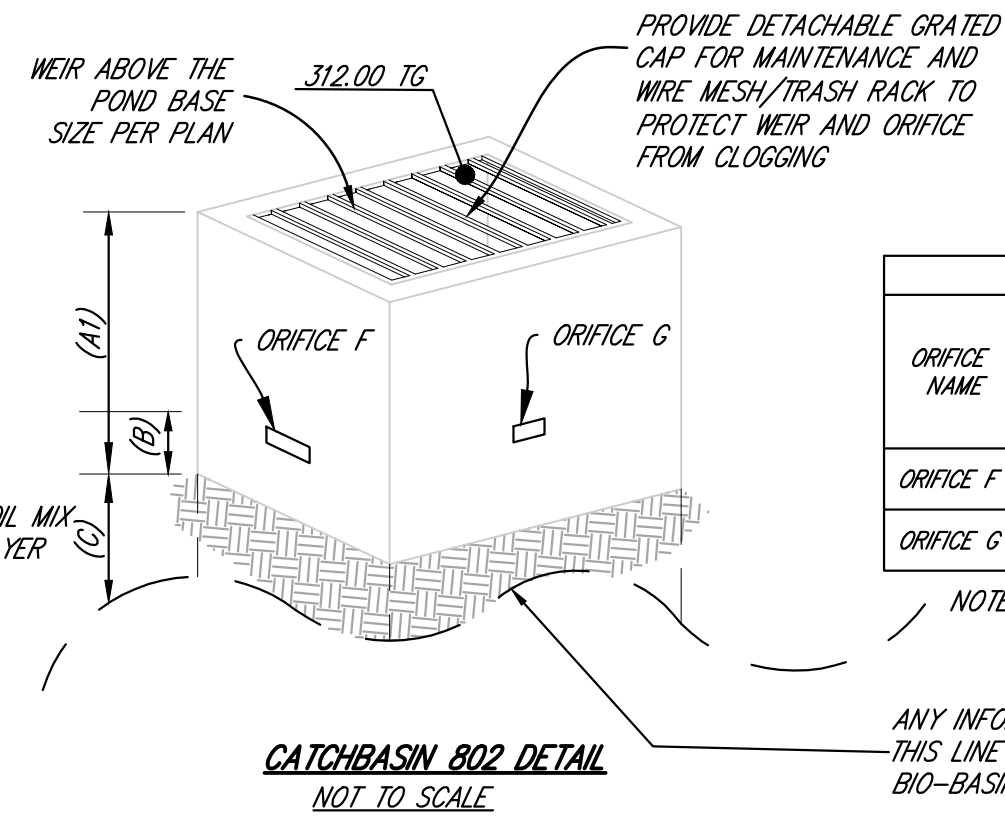
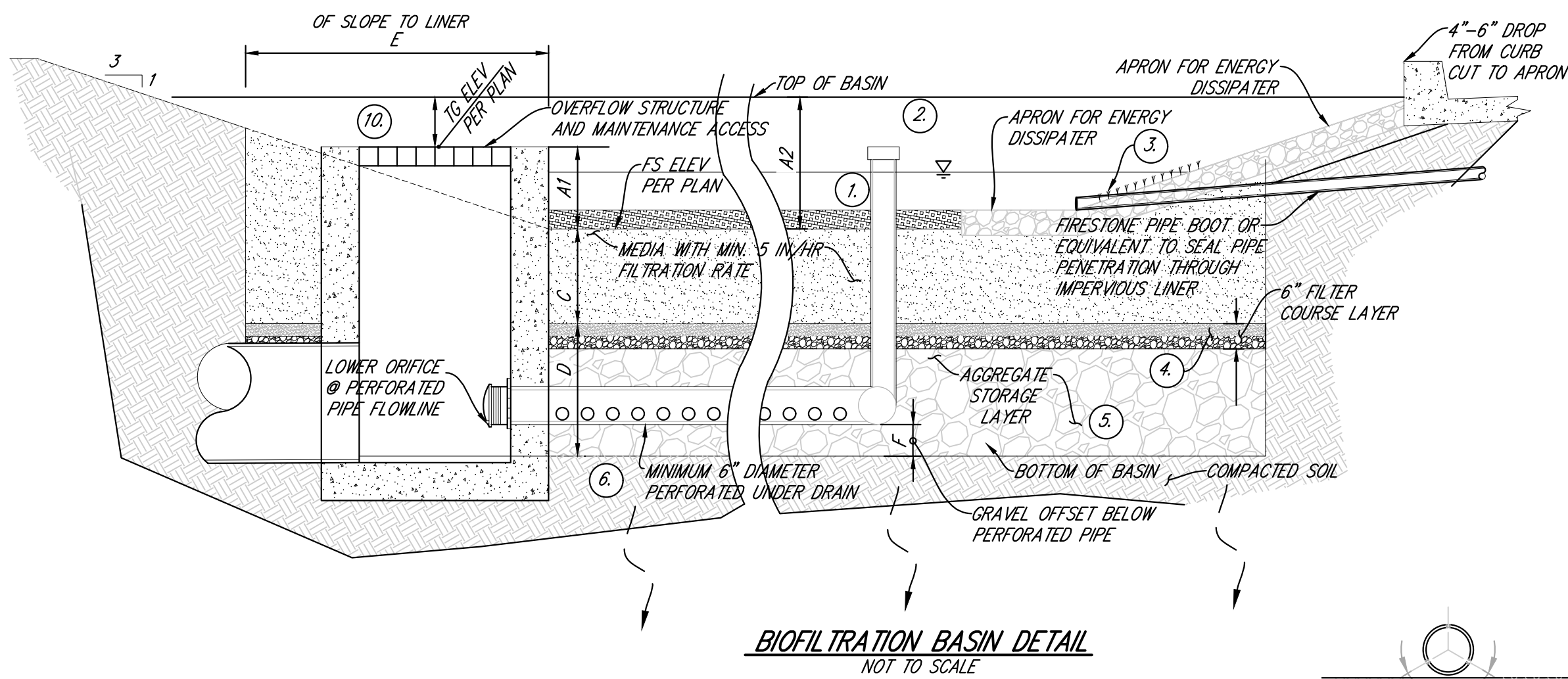
ONSITE CIRCULATION  
SHEET 6 OF 8 SHEETS

PALA MESA PLAZA  
SITE DEVELOPMENT PLAN  
APN 125-050-54-00

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**WATER QUALITY BASIN INSTALLATION NOTES:**

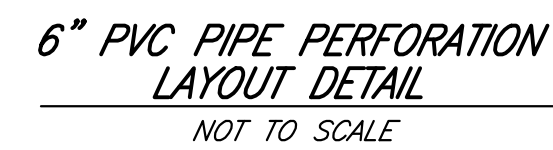
- 3 INCHES OF WELL-AGED, SHREDDED HARDWOOD MULCH.
- AN UNDERDRAIN CLEANOUT WITH A MINIMUM 6-INCH DIAMETER AND LOCKABLE CAP IS PLACED EVERY 250 TO 300 FEET AS REQUIRED BASED ON UNDERDRAIN LENGTH.
- VEGETATION USED SHOULD BE SUITABLE FOR THE CLIMATE PER LANDSCAPE PLANS.
- FILTER COARSE IS A MINIMUM OF 6 INCHES PROVIDED IN TWO SEPARATE 3 INCH LAYERS. THE TOP LAYER SHALL BE MADE OF ASTM C33 CHOKER SAND AND THE BOTTOM LAYER BE OF ASTM NO. 8 AGGREGATE. MARKERS STAKES SHALL BE USED TO ENSURE UNIFORM LIFT THICKNESS.
- AASHTO NO. 57 STONE OR CLASS 2 PERMEABLE PER CAL TRANS SPECIFICATION 68-1.025 IS RECOMMENDED FOR THE AGGREGATE STORAGE LAYER. WASHED, OPEN-GRADED CRUSHED ROCK MAY BE USED, HOWEVER, A 4 INCH MINIMUM WASHED PEA GRAVEL FILTER COURSE LAYER AT THE TOP OF THE CRUSHED ROCK IS REQUIRED.
- IMPERMEABLE LINER SHALL BE INSTALLED WHEN THE BIOFILTRATION BASIN IS WITHIN 10 FEET OF RETAINING WALLS OR BUILDING FOUNDATIONS, OR AS RECOMMENDED BY THE SOILS ENGINEER, OR REQUIRED BY THESE PLANS. IMPERMEABLE LINER SHALL BE 30 MIL THICK (PER COUNTY OF SAN DIEGO GREEN STREETS DESIGN STANDARD DRAWING GS-3.00 AND COUNTY GREEN STREETS SUPPLEMENT TO CAL TRANS SPECIFICATIONS 20-11.08B) CONFIGURED TO ENTIRELY ENCOMPASS THE SIDES OF THE WATER QUALITY BASIN.
- IMPERMEABLE LINER BE CONSTRUCTED IN COMPLIANCE WITH THE COUNTY OF SAN DIEGO GREEN STREETS SUPPLEMENT TO CAL TRANS SPECIFICATIONS 20-11.08B.
- BIOFILTRATION SOIL MEDIA LAYER (BSM) SHALL CONSIST OF 60% TO 80% BY VOLUME SAND, UP TO 20% BY VOLUME TOPSOIL, AND UP TO 20% BY VOLUME COMPOST (PER COUNTY OF SAN DIEGO BMP DESIGN MANUAL SEPTEMBER 2020 APPENDIX F.2 SECTION B05-2 BLENDED BSM CRITERIA AND TESTING REQUIREMENTS) PLACED IN 6" LIFTS AND COMPACTED WITH WATER PRIOR TO THE NEXT LIFT. INITIAL PERMEABILITY SHALL BE 6" PER HOUR (WITH ASSUMED STABILIZED PERMEABILITY OF 5" PER HOUR).
- THE AGGREGATE STORAGE LAYER SHALL BE COMPACTED IN ACCORDANCE WITH SOILS ENGINEER'S RECOMMENDATIONS.
- OVERFLOW STRUCTURE TO HAVE A MINIMUM OF 2 INCHES OF FREEBOARD.
- ALL LINER INSTALLATIONS, FIELD WELDING OF SEAMS, AND OBSERVATION OF SOIL MIX PLACEMENT SHALL REQUIRE SPECIAL INSPECTION BY THE PROJECT GEOTECHNICAL ENGINEER OR OTHER QUALIFIED PERSON. A LETTER CERTIFYING PROPER INSTALLATION SHALL BE PROVIDED TO THE ENGINEER OF RECORD TO ACCEPTANCE OF THE FACILITIES.
- SPECIAL INSPECTION SHALL BE REQUIRED FOR CONSTRUCTION OF ALL BIOFILTRATION BASINS. INSPECTION SHALL BE PERFORMED BY A QUALIFIED INDIVIDUAL (SUCH AS: ENGINEER OF RECORD, OSD). INSPECTION SHALL INCLUDE:
  - VERIFICATION OF OVERALL DIMENSIONS PRIOR TO PLACEMENT OF MATERIALS;
  - PLACEMENT OF THE LINER, IF REQUIRED; AND SEAMS OR PENETRATIONS
  - PLACEMENT OF THE GRAVEL, FILTER MATERIALS, AND FILTER MEDIA;
  - ALL INLET AND OUTLET STRUCTURES INCLUDING UNDERDRAINS, IF REQUIRED.
  - CONTRACTOR SHALL TAKE PICTURES AT EACH STAGE OF INSTALLATION AND SUBMITTED TO ENGINEER FOR VERIFICATION OF INSTALL.
- INSPECTOR SHALL BE GIVEN A MINIMUM OF 48 HOURS PRIOR TO INSPECTION. UPON COMPLETION THE INSPECTOR SHALL PROVIDE A CERTIFICATION TO THE ENGINEER OF WORK.
- PROPOSED MATERIALS, SUCH AS AGGREGATE, FILTER MATERIAL, AND FILTER MEDIA SHALL BE SUBMITTED TO THE ENGINEER OF WORK FOR APPROVAL.



ORIFICE NAME	INVERT ELEVATION (FT)	B (INCH)	A1 TOP OF RISER (INCH)	NUMBER OF BARRELS	SPAN (INCH)	RISE (INCH)	MAX WATER SURFACE ELEVATION (FT)	FREE BOARD ELEVATION (FT)
ORIFICE F	310.50	9.00	27	1	3	1	310.97	311.00
ORIFICE G	310.60	10.20		1	2	1		

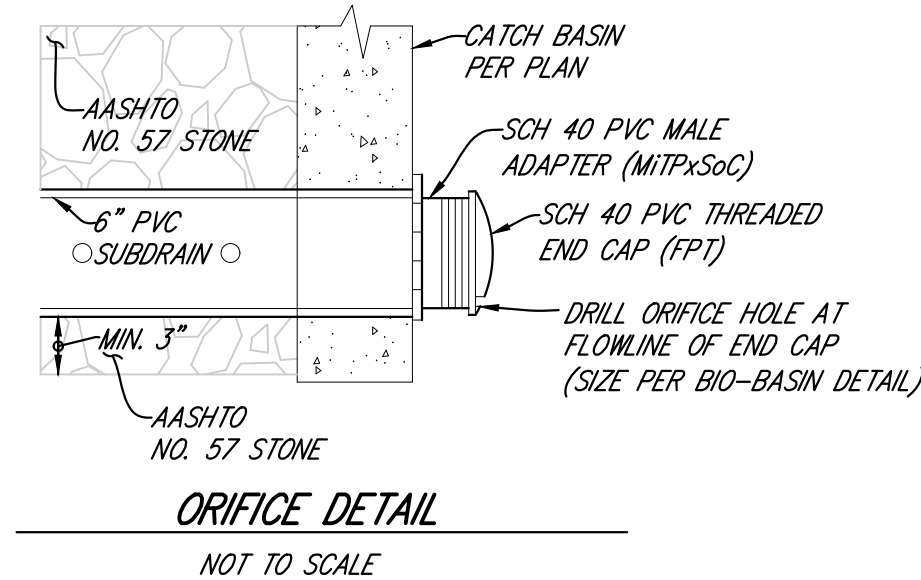
NOTE: MEASUREMENTS ARE FROM FINISH SURFACE OF BASIN

ANY INFORMATION BELOW THIS LINE SEE STRUCTURAL BIO-BASIN SUMMARY TABLE



BMP NAME	TYPE OF BMP	EFFECTIVE AREA (SQFT)	A1 (INCH) TOP OF RISER	A2 (INCH) TOP OF BASIN	A3 (INCH) CLEAN OUT	B (INCH) UPPER ORIFICE	C (INCH) MEDIA	D (INCH) GRAVEL	E (INCH) DISTANCE FROM TOE OF SLOPE TO LINER	F (INCH) PERFOR. PIPE IE OFFSET	BOX RISER OVERFLOW STRUCTURE SIZE (INCHES)	ORIFICES DIAMETER		IMPERMEABLE LINER ?
												UPPER (INCH)	LOWER (INCH)	
BMP-103	BIOFILTRATION	2767	9	36	9	N/A	18	30	27	18	36x36	N/A	0.75	NO
BMP-201	BIOFILTRATION	1712	9	18	9	N/A	18	18	27	6	36x36	N/A	0.5	NO
BMP-302	BIOFILTRATION	1020	9	18	9	N/A	18	18	27	6	24x24	N/A	0.5	NO
BMP-401	BIOFILTRATION	278	9	18	9	N/A	18	18	27	3	18x18	N/A	0.5	NO
BMP-504	BIOFILTRATION	1531	9	17.6	9	N/A	18	18	27	3	36x36	N/A	0.5	NO
BMP-602	BIOFILTRATION	558	9	12	9	N/A	18	18	27	3	18x18	N/A	0.5	NO
BMP-702	BIOFILTRATION	560	9	5.4	9	N/A	18	18	27	3	18x18	N/A	0.5	NO
BMP-802	BIOFILTRATION	1807	SEE CATCH BASIN DETAIL	24	9	SEE CATCH BASIN DETAIL	18	30	27	18	36x36	SEE CATCH BASIN DETAIL	0.75	NO

NOTE: MEASUREMENTS ARE FROM FINISH SURFACE OF BASIN



**DETAIL**  
**"NO DUMPING" AT CATCH BASINS**

NOTE: ALL CATCH BASINS WITH GRATES SHALL BE STENCILED WITH CITY REQUIRED ITEM PER ABOVE DETAIL:  
(DAS MANUFACTURING #SDO OR EQUIVALENT)

PERMANENT WATER QUALITY TREATMENT FACILITY

KEEPING OUR WATERWAYS CLEAN  
MAINTAIN WITH CARE - NO MODIFICATIONS WITHOUT AGENCY APPROVAL

**DETAIL**  
**WATER QUALITY SIGN- PLACED AT EACH BIOFILTRATION BASIN**

NOTE: ALL BIOFILTRATION AREAS WILL HAVE A SIGN POSTED TO BE VISIBLE AT ALL TIMES.

**HYDROLOGIC SOIL GROUP**

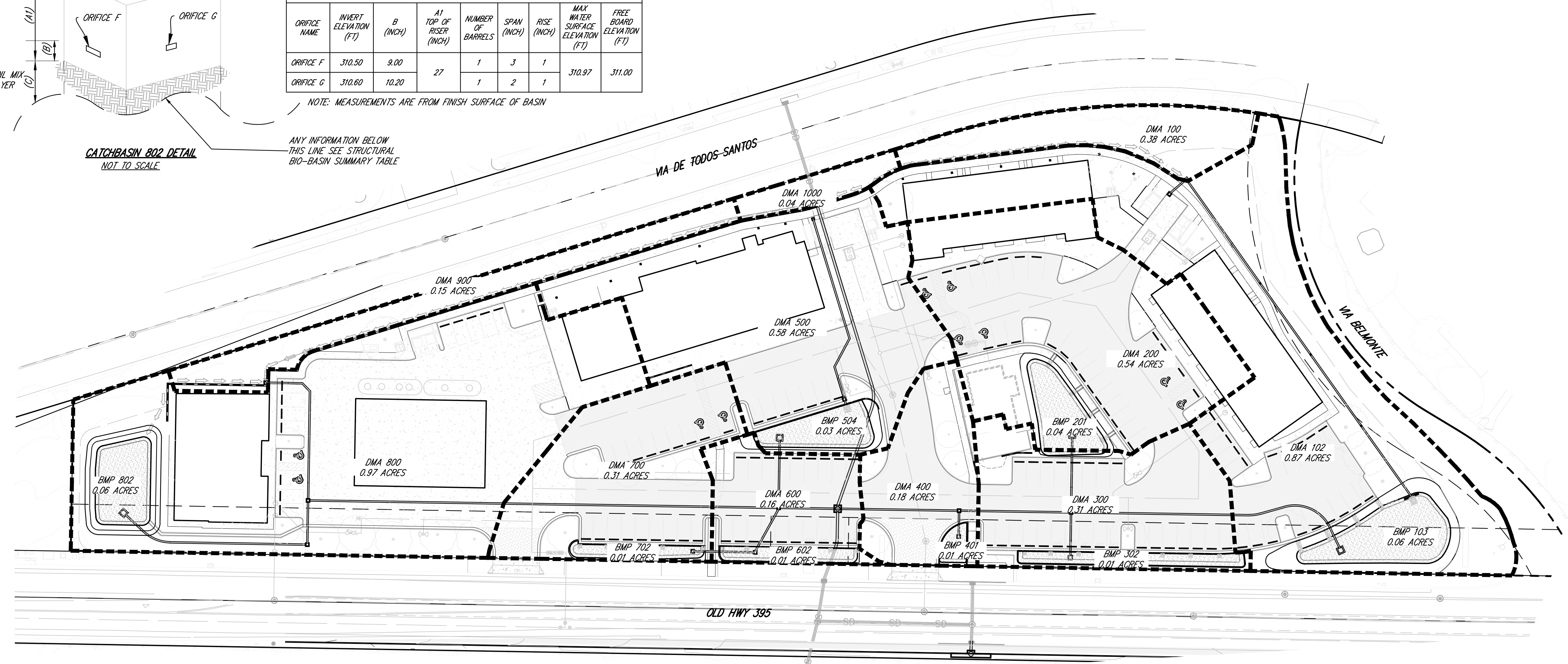
THE HYDROLOGIC SOIL GROUPS FOR THIS SITE ARE TYPES (A) AND (C).

**EXISTING SITE FEATURES**

- THE APPROXIMATE DEPTH TO GROUNDWATER IS GREATER THAN 25 FEET BASED ON THE EPA WEB SOIL SURVEY RESULTS
- THERE ARE NO NATURAL HYDROLOGIC FEATURES ON THE SITE.
- THE SITE PROPOSES TO CONNECT TO THE EXISTING PUBLIC STORMDRAIN SYSTEM LOCATED IN THE PUBLIC RIGHT-OF-WAY FOR OLD HIGHWAY 395.
- BASED ON WATERSHED MAPPING OF POTENTIAL CRITICAL COARSE SEDIMENT YIELD AREAS (CCSYA), THERE ARE NO CCSYA LOCATED WITHIN THE PROJECT BOUNDARY OR TRIBUTARY TO THE RUNOFF BYPASSED AROUND THE SITE.

**SELF-MITIGATING DMAS**

- VEGETATION IN THE NATURAL OR LANDSCAPED AREA IS NATIVE AND/OR NON-NATIVE/NON-INVASIVE DROUGHT TOLERANT SPECIES THAT DO NOT REQUIRE REGULAR APPLICATION OF FERTILIZERS AND PESTICIDES.
- SOILS ARE UNDISTURBED NATIVE TOPSOIL OR DISTURBED SOILS THAT HAVE BEEN AMENDED AND AERATED TO PROMOTE WATER RETENTION CHARACTERISTICS EQUIVALENT TO UNDISTURBED NATIVE TOPSOIL.
- THE INCIDENTAL IMPERVIOUS AREAS ARE LESS THAN 5 PERCENT OF THE SELF-MITIGATING AREA.
- IMPERVIOUS AREA WITHIN THE SELF-MITIGATING AREA SHOULD NOT BE HYDRAULICALLY CONNECTED TO OTHER IMPERVIOUS AREAS UNLESS IT IS A STORM WATER CONVEYANCE SYSTEM (SUCH AS A BROW DITCH).
- THE SELF-MITIGATING AREA IS HYDRAULICALLY SEPARATE FROM DMAS THAT CONTAIN PERMANENT STORM WATER POLLUTANT CONTROL BMPs.

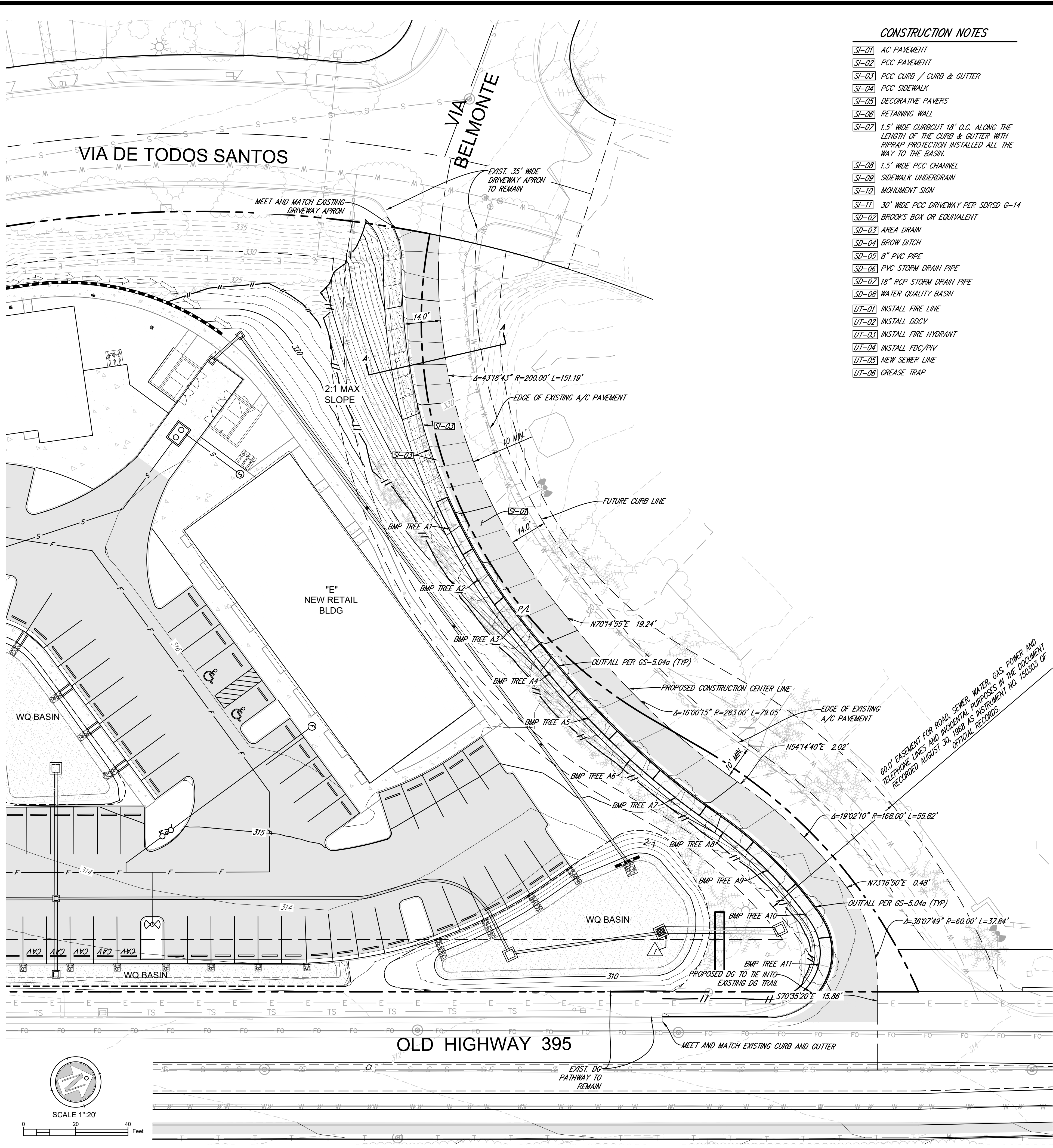


DMA EXHIBIT  
SHEET 7 OF 7 SHEETS

PALA MESA PLAZA  
SITE DEVELOPMENT PLAN  
APN 125-050-54-00

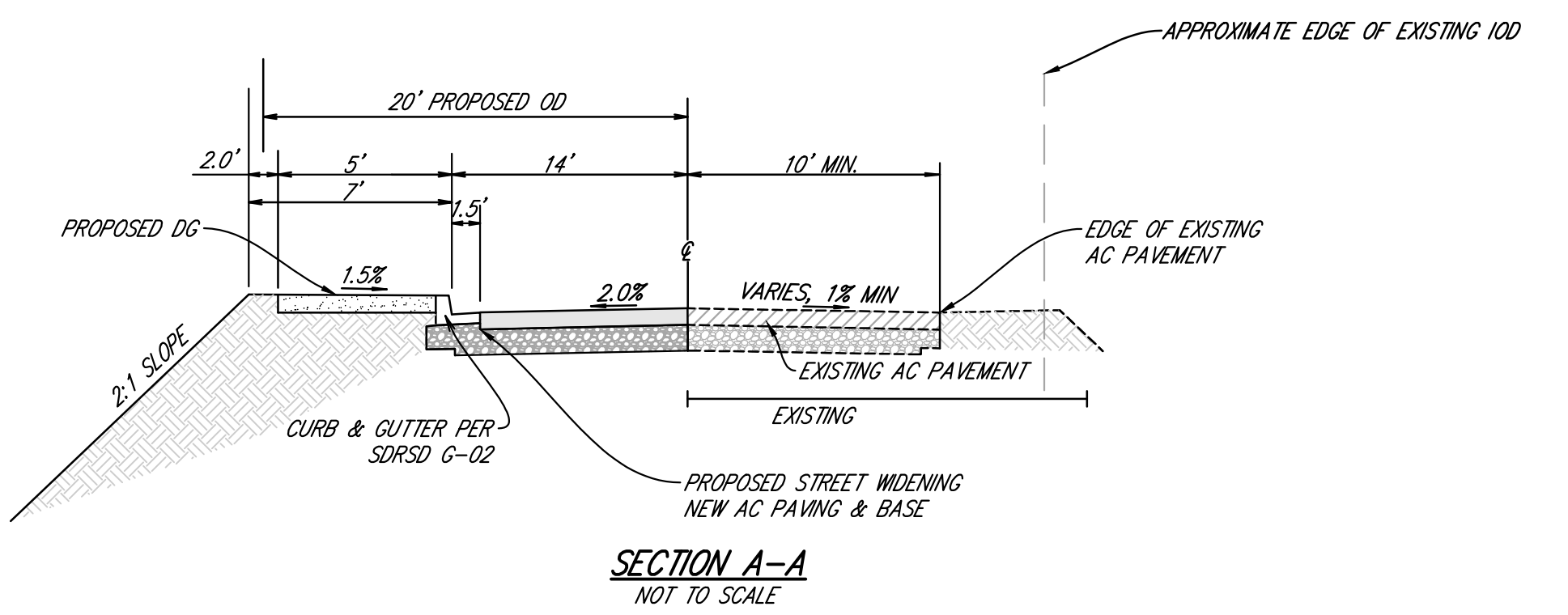
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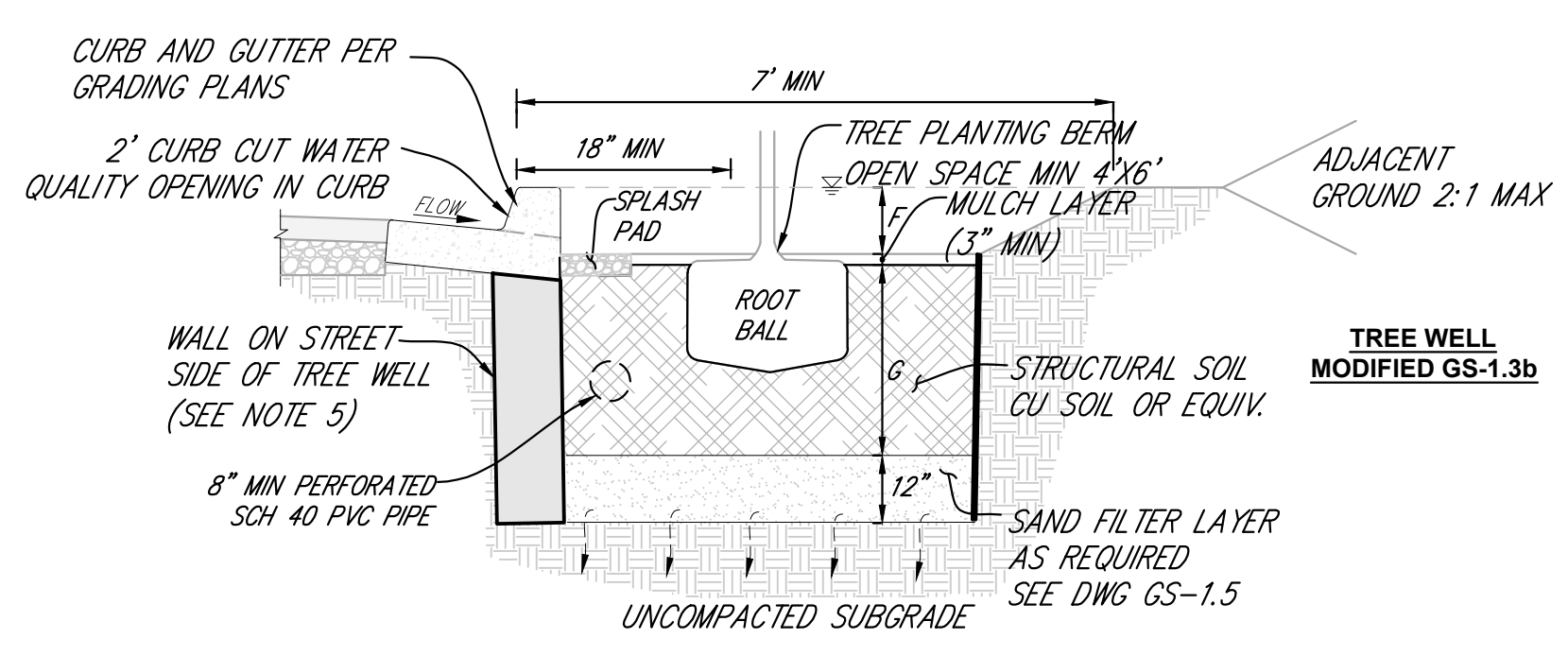
**CONSTRUCTION NOTES**

- SI-01 AC PAVEMENT
- SI-02 PCC PAVEMENT
- SI-03 PCC CURB / CURB & GUTTER
- SI-04 PCC SIDEWALK
- SI-05 DECORATIVE PAVERS
- SI-06 RETAINING WALL
- SI-07 1.5' WIDE CURB CUT 18' O.C. ALONG THE LENGTH OF THE CURB & GUTTER WITH RIPRAP PROTECTION INSTALLED ALL THE WAY TO THE BASIN.
- SI-08 1.5' WIDE PCC CHANNELED
- SI-09 SIDEWALK UNDERDRAIN
- SI-10 MONUMENT SIGN
- SI-11 30' WIDE PCC DRIVEWAY PER SDRSD G-14
- SD-02 BROOKS BOX OR EQUIVALENT
- SD-03 AREA DRAIN
- SD-04 BROW DITCH
- SD-05 8" PVC PIPE
- SD-06 PVC STORM DRAIN PIPE
- SD-07 18" RCP STORM DRAIN PIPE
- SD-08 WATER QUALITY BASIN
- UT-01 INSTALL FIRE LINE
- UT-02 INSTALL DDCV
- UT-03 INSTALL FIRE HYDRANT
- UT-04 INSTALL FDC/PIV
- UT-05 NEW SEWER LINE
- UT-06 GREASE TRAP



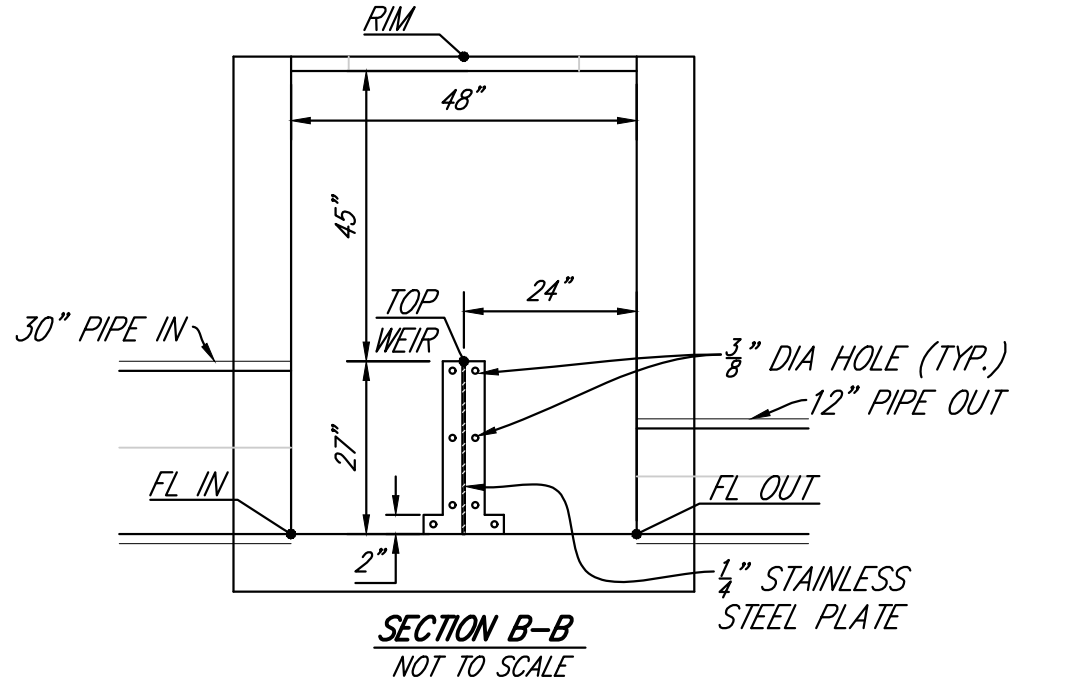
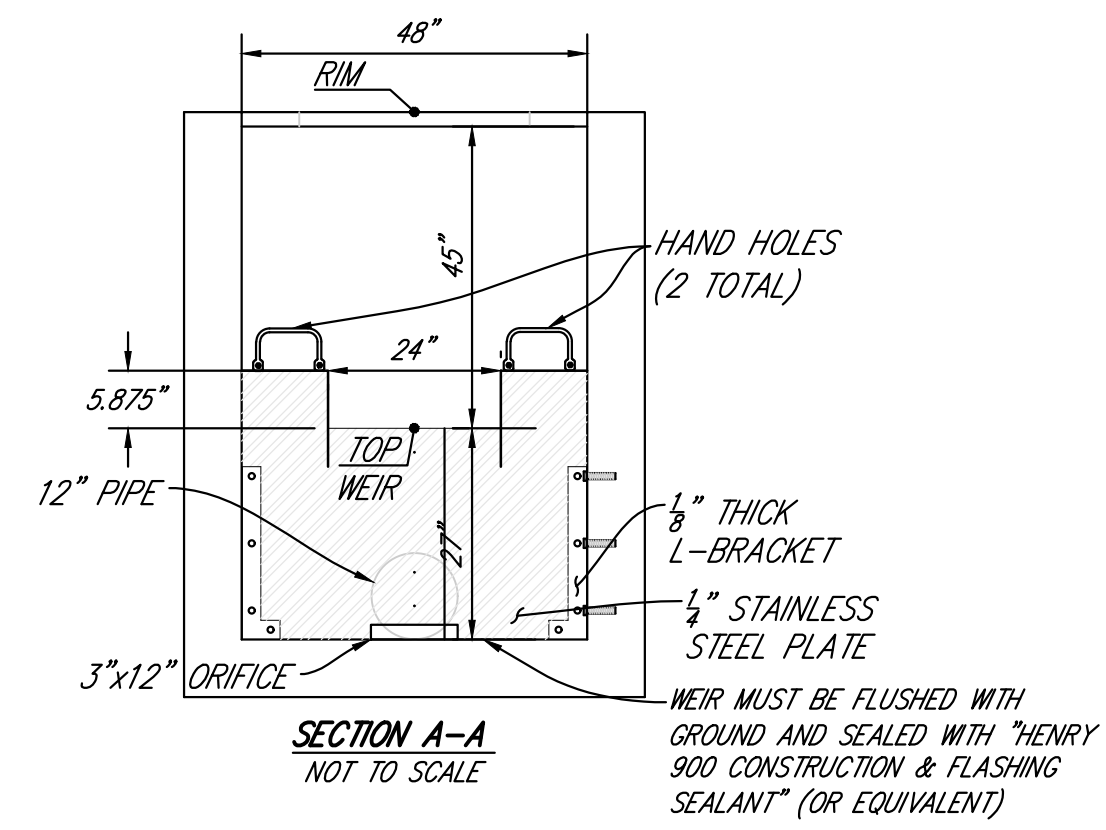
**TREE WELL**

1. MINIMUM OPEN TREE PLANTING SPACE DIMENSION 4'x6'.
2. MAXIMUM WATERSHED AREA: 6 TIMES AREA OF THE OPEN TREE PLANTING AREA.
3. FOR SIDEWALK OVER STRUCTURAL SOIL DETAIL OPTIONS, SEE DWG GS-1.5.
4. SEE SD RSD DWG L-1 THROUGH L-4 FOR TREE INSTALLATION REQUIREMENTS.
5. PROVIDE 1" THICK, 5' DEEP CONCRETE CUT-OFF WALL W/ REINFORCING ON STREET SIDE OF TREE WELL AS NECESSARY.
6. REMOVE WIRE AND BURLAP FROM ROOT BALL PRIOR TO BACKFILLING.
7. PROVIDE SPLASH PAD FOR TREE PLANTING SPACE PER GS-2.6b.
8. IN SOME INSTANCES A MODIFIED SECTION BELOW CURB MAY BE REQUIRED BASED ON SOIL CONDITIONS.
9. PROVIDE PLASTIC LINER WHERE CONCRETE WILL BE POURED ON TOP OF STRUCTURAL SOIL PER GREEN BOOK STANDARD SPECIFICATIONS SECTION 210-2.

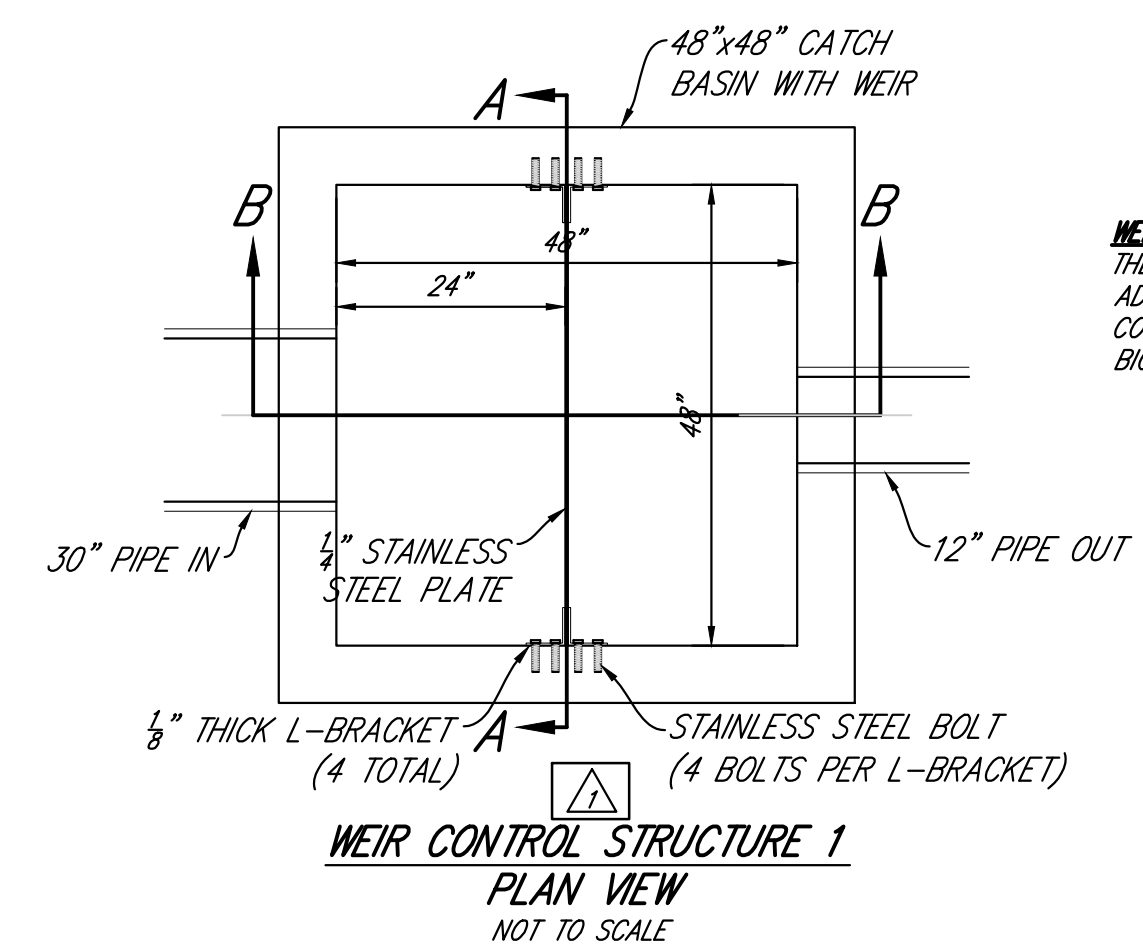


**SITE BMP SUMMARY TABLE**

DRAINAGE BASIN ID	DMA-3001
DMA TYPE (DRAINS TO BMP, DE-MINIMUS, MAINTENANCE)	DRAINS TO BMP
BMP TYPE	TREE WELLS A1-A11
IMPERVIOUS SURFACE (SF)	4,790
PERVIOUS SURFACE (SF) AMENDED SOIL SD-F C FACTOR IS 0 AND THEREFORE NOT IN CALCS.	503
IMPERVIOUS AREA TO DISPERSION SD-B	N/A
PERVIOUS AREA ACTING AS DISPERSION SD-B	N/A
AVERAGE MATURE TREE CANOPY DIAMETER (FT)	15
TOTAL TRIBUTARY AREA (SF)	5,293
INITIAL DESIGN CAPTURE VOLUME (CF)	297
PROVIDED NUMBER OF TREE WELLS	11
PROVIDE TREE CREDIT VOLUME (CF)	1,100
PROVIDED REQUIRED RETENTION VOL (RRV)	941
DCV REMAINING (CF)	0
DESIGN CAPTURE VOLUME MET? (Y/N)	Y



**WEIR CONTROL STRUCTURE 1 DETAILS**

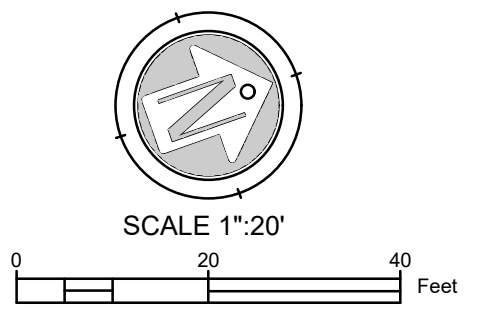


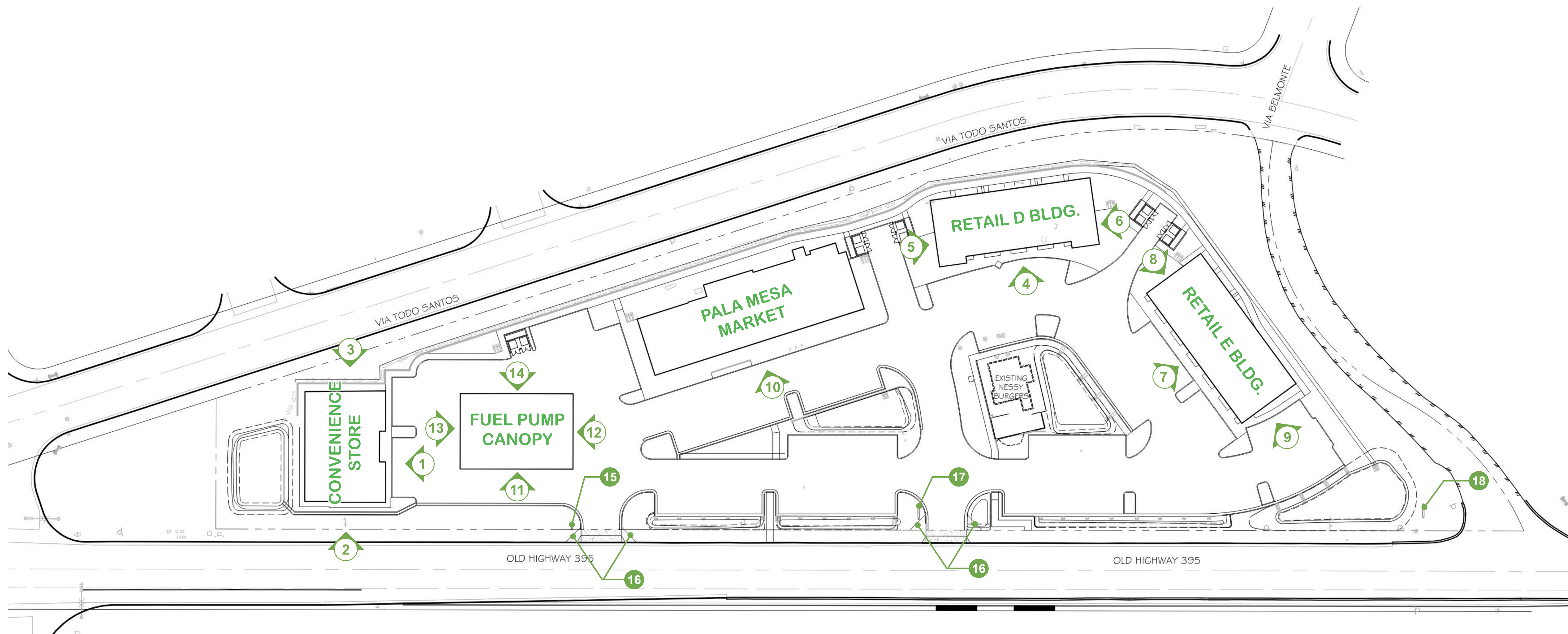
**WEIR CONTROL NOTE:**  
THE PURPOSE OF THIS STRUCTURE IS FOR ADDITIONAL HYDROMODIFICATION FLOW CONTROL SEPARATE FROM TYPICAL BIOPFILTRATION FLOW MODELING

VIA BELMONTE IMPROVEMENTS  
SHEET 8 OF 8 SHEETS

SITE DEVELOPMENT PLAN  
FOR APN 125-050-54-00  
OLD HIGHWAY 395 RETAIL CENTER

1/19/2025 Engineering [SDP] SDP BELMONT IMPROVEMENTS.dwg 5/20/2024 5:37 PM ORIGINAL PLOT SIZE: ---





**LEGEND:**

- 1 C-Store North Elevation
- 2 C-store East Elevation
- 3 C-Store West Elevation
- 4 Retail D Front Elevation
- 5 Retail D Left Elevation
- 6 Retail D Right Elevation
- 7 Retail E Front Elevation
- 8 Retail E Left Elevation
- 9 Retail E Right Elevation
- 10 Market Front Elevation
- 11 Canopy East Elevation
- 12 Canopy North Elevation
- 13 Canopy South Elevation
- 14 Canopy West Elevation
- 15 Gas Station Pricing Sign
- 16 Sight Lines
- 17 Directory Sign Monument
- 18 Market Monument Sign

Signage Site Plan

1" = 40'-0"

\*Elevation

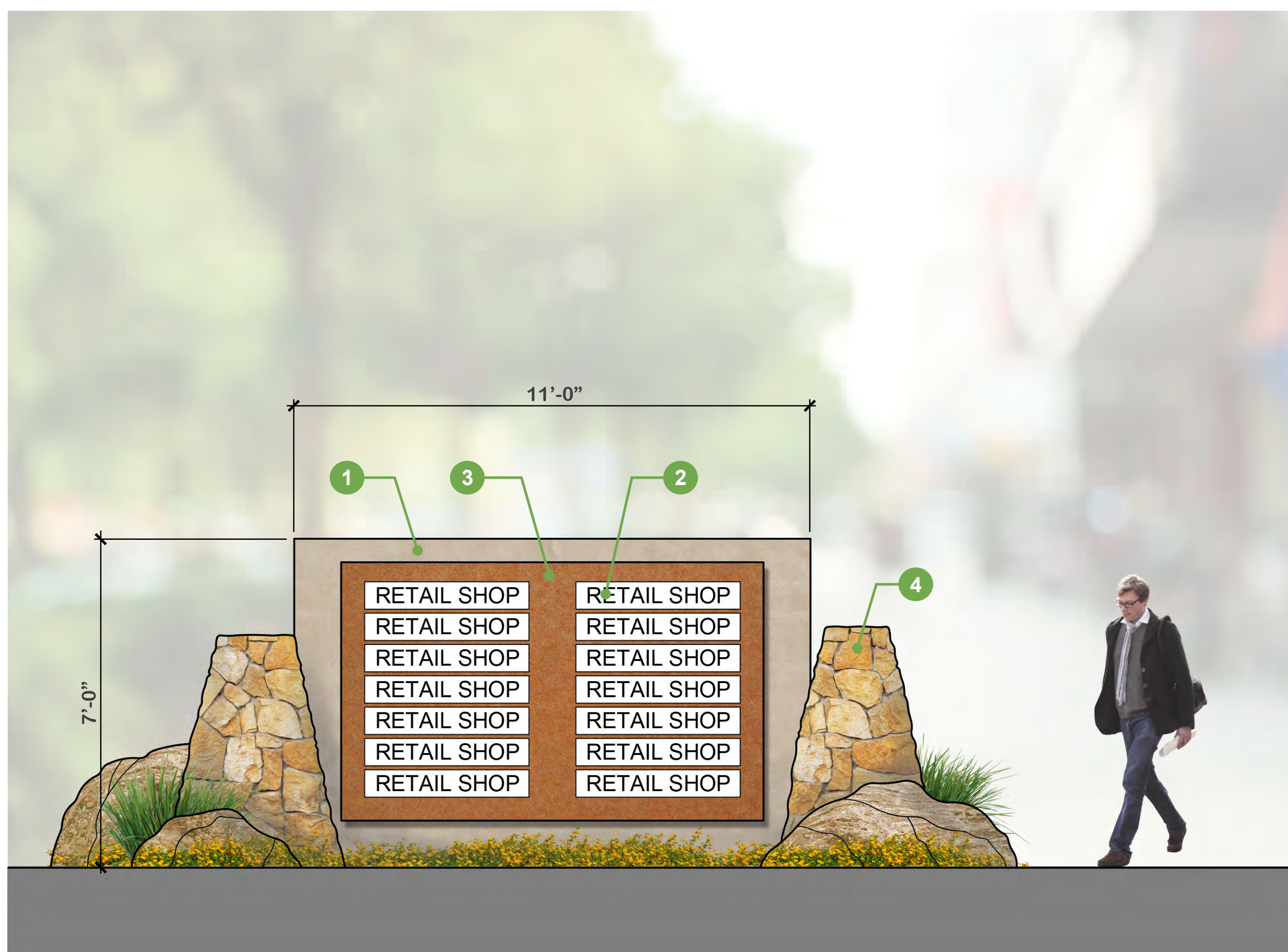


Market Monument Sign

1/2" = 1'-0"

**LEGEND:**

- 1 Poured-in-place concrete wall with recessed letters painted in black
- 2 6.8 square feet area with 7" ht. letters to be Athelas bold font
- 3 Stone veneer wall wrapped around concrete
- 4 \*Sign will be uplit with no backlighting

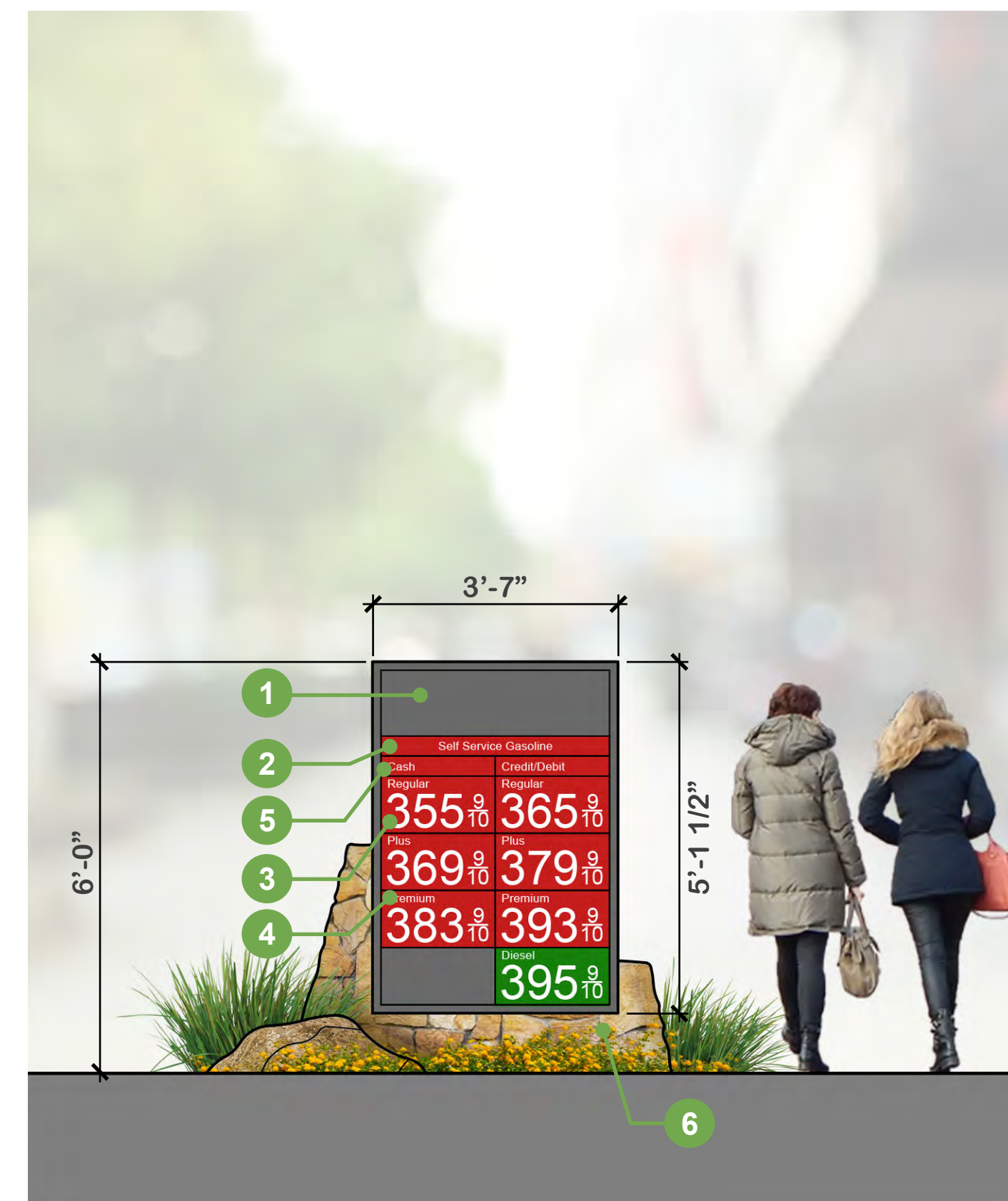


Directory Sign Monument

1/2" = 1'-0"

**LEGEND:**

- 1 Poured-in-place concrete wall set in between stone veneer pilasters surrounding the ends
  - 2 3'-6" x 7" sign with 4" ht. text max. (1" gap between shop signs)
  - 3 Corten steel plate offset from concrete wall 1"
  - 4 Stone veneer wall wrapped around concrete
- \*Each retail shop to be 2 square feet max
- \*Total directory sign monument is 28 square feet
- \*Sign will be uplit with no backlighting



Gas Station Pricing Sign

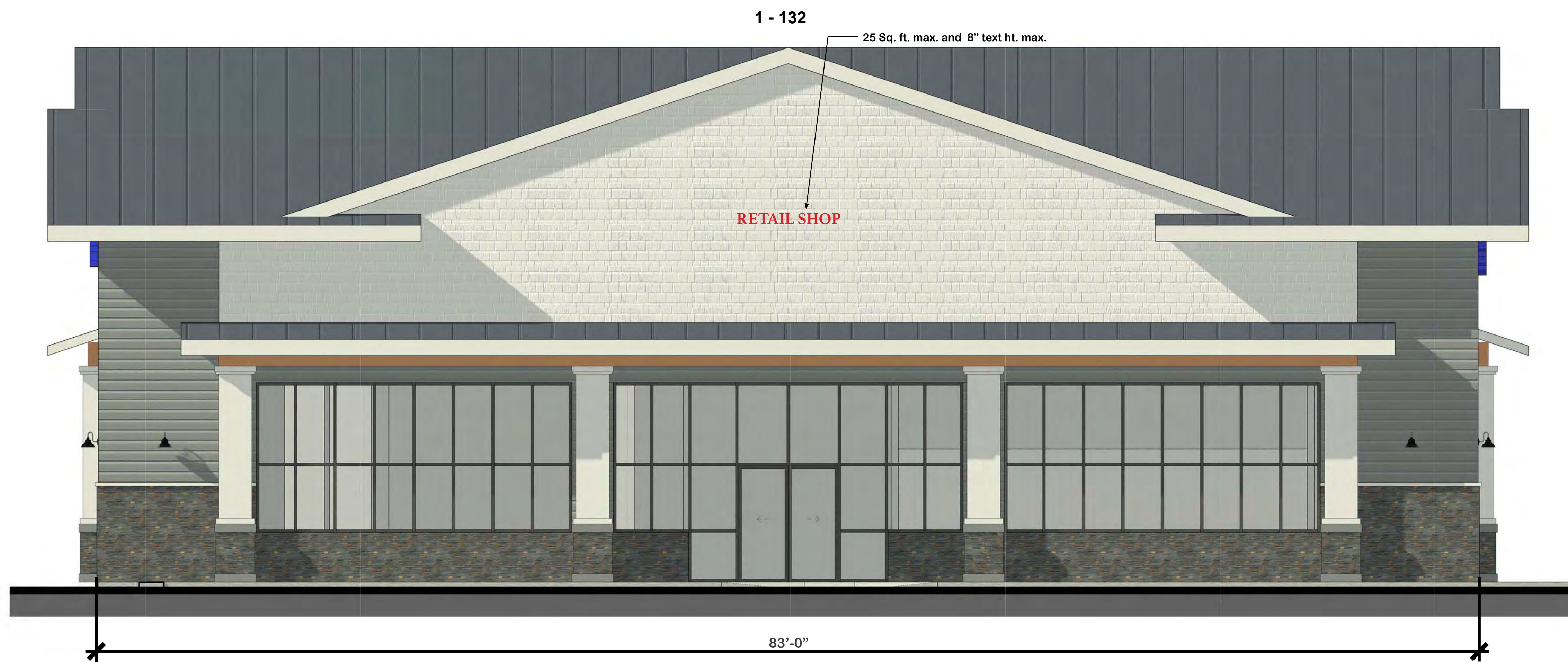
1/2" = 1'-0"

**LEGEND:**

- 1 Brand or trademark placeholder (text size to be 1 1/2" max.)
- 2 Self service gasoline label (text size to be 1 1/2" max.)
- 3 Price per gallon numerals (text size to be 6" max.)
- 4 Grade of the motor fuel (text size to be 1 1/2" max.)
- 5 Cash / Credit (text size to be 1 1/2" max.)
- 6 Stone veneer

\*All gas station texts are per California Fuel Advertising and Labeling Requirements

\*Numbers to be digital



Convenient Store North Elevaton

1/4" = 1'-0"



Convenient Store East Elevation

1/4" = 1'-0"



Convenient Store South Elevaton

1/4" = 1'-0"



Convenient Store West Elevation

1/4" = 1'-0"

10 Sq. ft. max. and 8" text ht. max.



Retail D Front Elevation

1/4" = 1'-0"

10 Sq. ft. max. and 8" text ht. max.



Retail D Left Elevation

1/4" = 1'-0"



Retail D Rear Elevation

1/4" = 1'-0"



Retail D Right Elevation

1/4" = 1'-0"



Retail E Front Elevation



Retail E Left Elevation

1/4" = 1'-0"





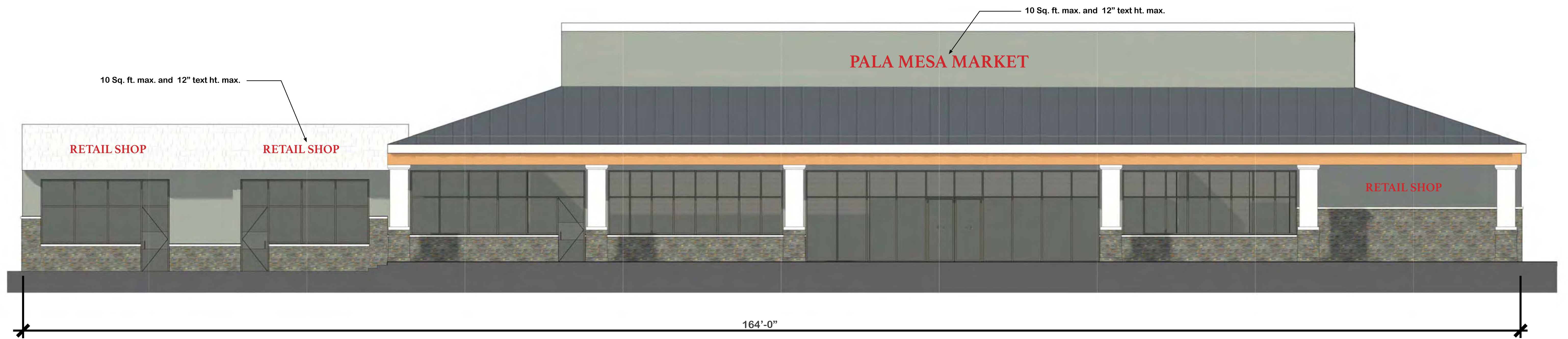
Retail E Rear Elevation

1/4" = 1'-0"



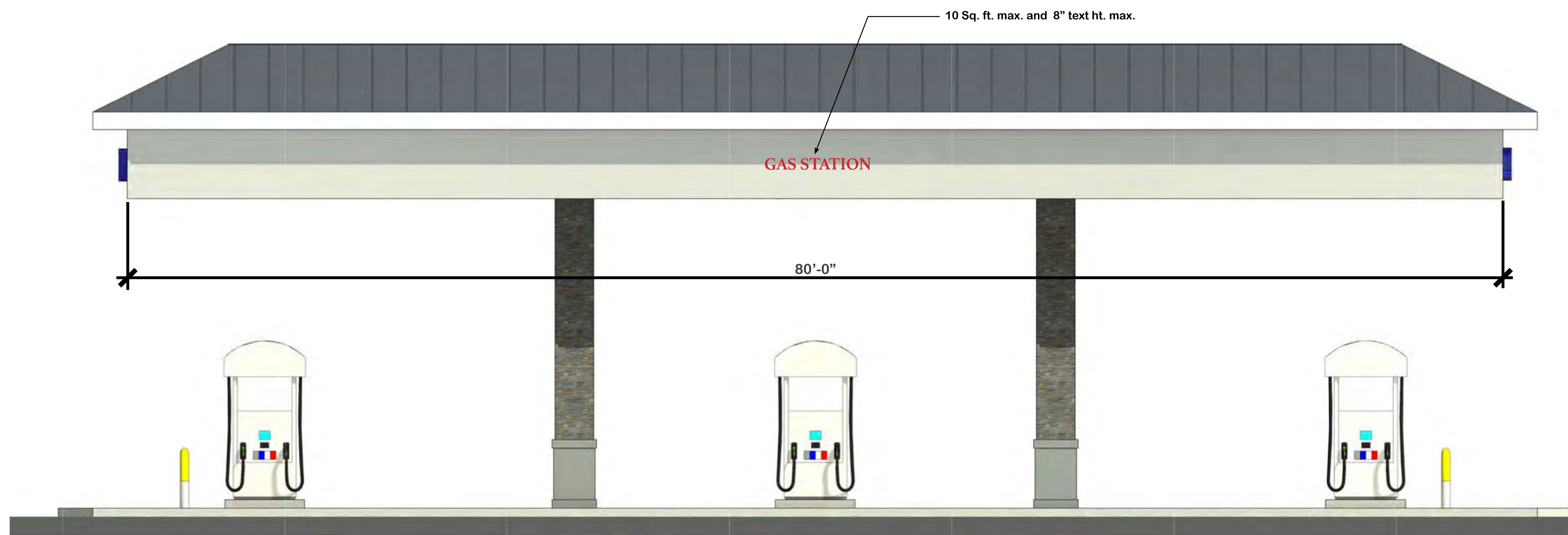
Retail E Right Elevation

1/4" = 1'-0"



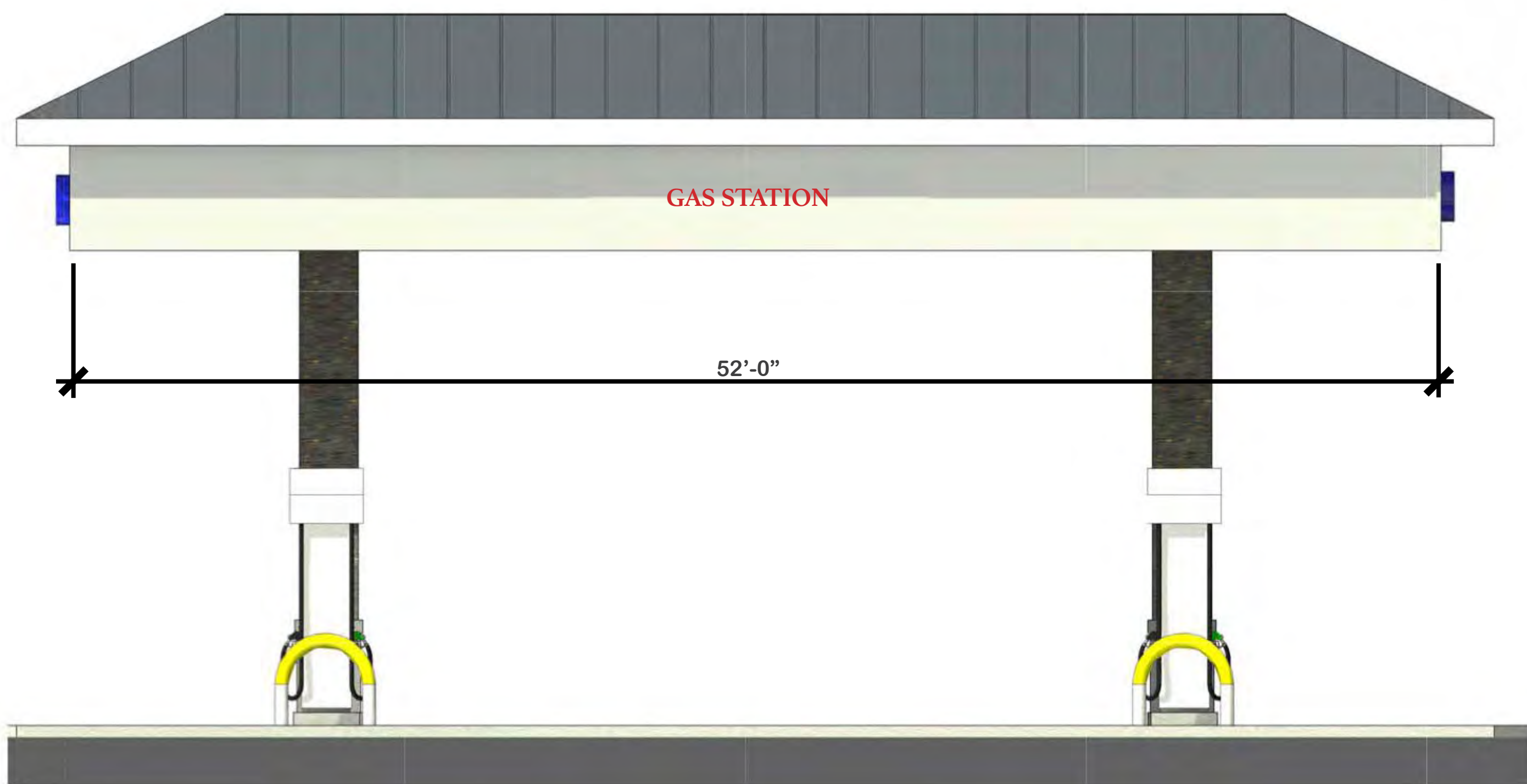
Market Front Elevation

3/16" = 1'-0"



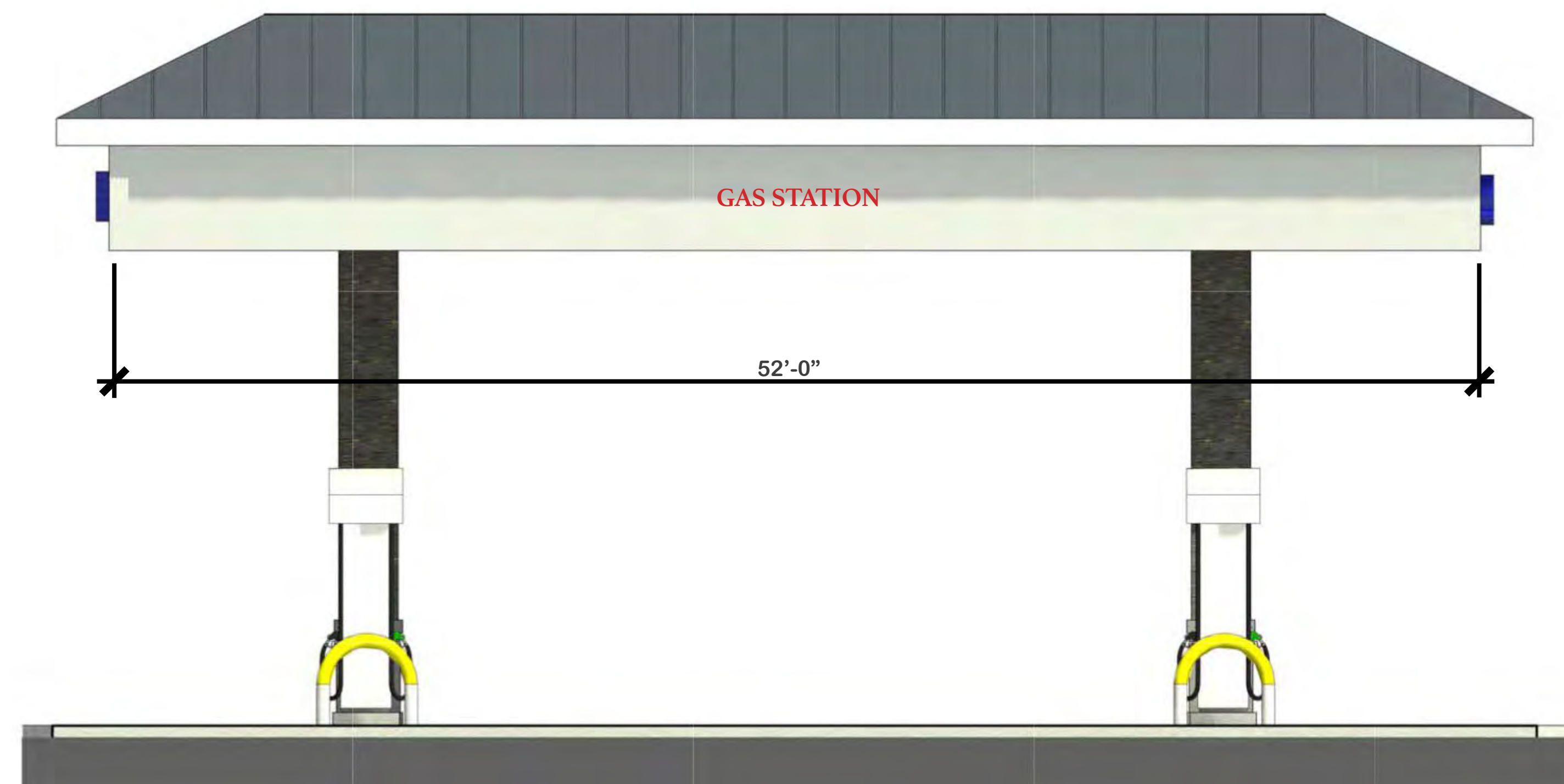
Canopy East Elevation

1/4" = 1'-0"



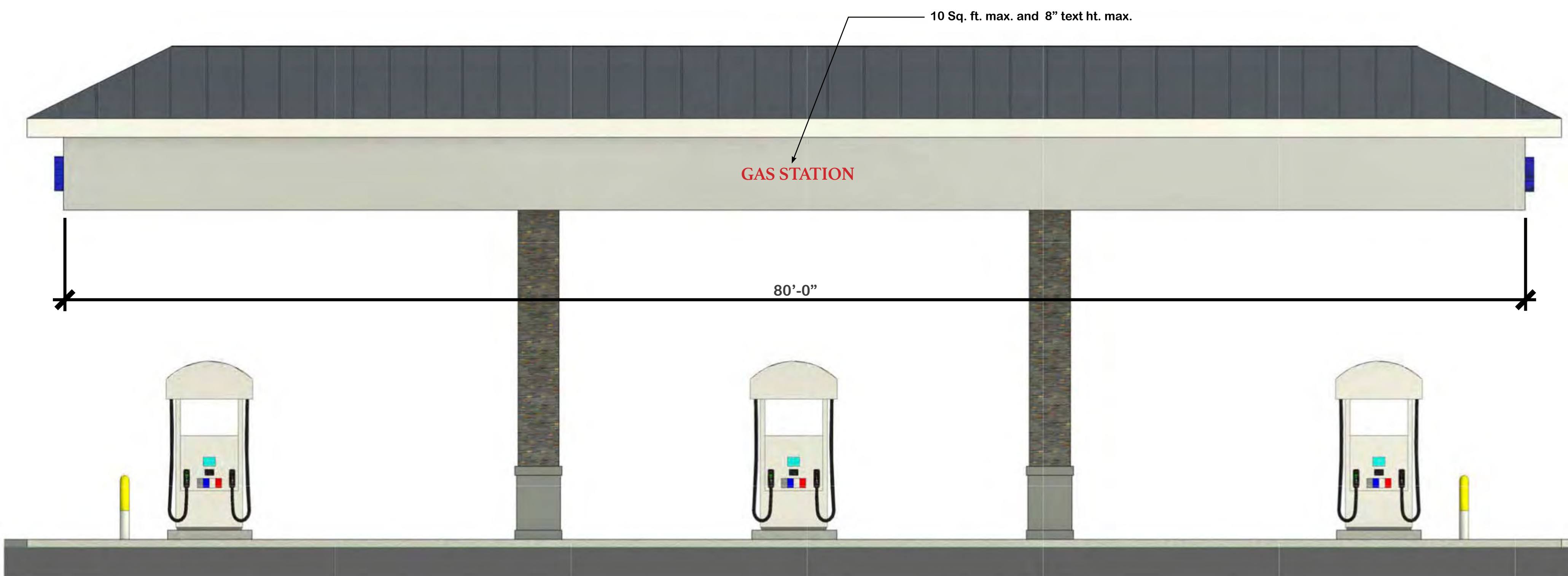
Canopy North Elevation

1/4" = 1'-0"



Canopy South Elevation

1/4" = 1'-0"



Canopy West Elevation

1/4" = 1'-0"



C-Store North Elevator



Retail E Front Elevation

1/4" = 1'-0"



## Attachment E – Public Documentation

FALLBROOK COMMUNITY PLANNING GROUP  
And  
DESIGN REVIEW BOARD

Chair, Eileen Delaney  
Eileen.fallbrook@gmail.com  
Regular Meeting  
Monday, September 19, 2022  
7:00 PM  
Fallbrook Public Utilities District Board Room  
990 E. Mission Rd. Fallbrook

\*\*\*For their convenience, members of the public may attend in-person **or** virtually through Zoom \*\*\*  
Meeting ID: 725 856 7213- Passcode: AVOCADO - Dial in by Phone: 669 900 6833 (Phone Passcode: 8180015)  
<https://us02web.zoom.us/j/7258567213?pwd=amRZRVI5NnprMFVETjA2enNDcTZaUT09>

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Este servicio de traducción se ofrece como recurso adicional para información.”

## AGENDA

1. CALL TO ORDER
2. OATH OF OFFICE: JERRY KALMAN
3. ROLL CALL
4. PLEDGE OF ALLEGIANCE
5. APPROVAL of the August 15, 2022 meeting minutes.
6. PUBLIC FORUM. Opportunity for members of the public to speak to the Group on any subject matter within the Group's jurisdiction but not on today's agenda. Three-minute limitation. Non-discussion and non-voting item.
7. Chair's Report: Public Forum
8. Updates on Rainbow MWD projects, Tom Kennedy, General Manager.
9. Presentation by Heather Conklin [heather.conklin@gmail.com](mailto:heather.conklin@gmail.com) about Mission Resource Conservation District and the services they offer Fallbrook residents.
10. Request for Site Plan Exemption for a car port with solar. Location: APN: 103-246-52-00, 624 Elder St. Fallbrook. Applicant: George Lytal, Crestwood Behavioral Health. Contact: Ben Packard, [ben.packard@currentdesigngroup.com](mailto:ben.packard@currentdesigngroup.com) County Planner: Jeffery Melo, [Jeffery.melo@sdcounty.ca.gov](mailto:Jeffery.melo@sdcounty.ca.gov) Community Input. **Design Review Board Committee.** Voting Item.
11. Minor deviation to change a Cricket Wireless wall-mount sign. Location: 1352 Mission Rd. APN: 1-04-202-22-0. Contact: Peter Lipsiwala, [peter.lapsiwala@permitsandmore.com](mailto:peter.lapsiwala@permitsandmore.com) ,County Planner: Hanna Wong, [Hannah.wong@sdcounty.ca.gov](mailto:Hannah.wong@sdcounty.ca.gov) . **Design Review Board Committee.** Community Input. Voting item.
12. Request for a sidewalk across Brooke at Stagecoach due to church and preschool pedestrian traffic from the parking lot to Christ the King Church. Contacts: Denny Linderman and John [Whitmer.smeacc@icloud.com](mailto:Whitmer.smeacc@icloud.com), **Circulation Committee.** Public input. Voting Item.
13. Update on the progress and recommendations for the County's Cannabis Ordinance. **Cannabis Adhoc Committee.** Community input. Voting item.

14. PDS2020-STP20-021 Sign Program for Pala Mesa Market Project. Location:

Contact: Jason Greminger, [jason.greminger@cciconnect.com](mailto:jason.greminger@cciconnect.com), County Planner: Sean Oberbauer, [sean.oberbauer@sdcountry.ca.gov](mailto:sean.oberbauer@sdcountry.ca.gov) . **Design Review Board Committee**. Community input. Voting item.

15. PDS2022-AD-013 .Request for an Administrative Permit for small classification for Sandia Creek Winery.

Location: APN 101-360-40-00, 40376 Sandia Creek Dr. Applicant: Vitals International Group [tim@vitalsintl.com](mailto:tim@vitalsintl.com) , Contact: Steve Rawlings [ser@rawlingspm.com](mailto:ser@rawlingspm.com) , County Planner: Alexandro Barrenechea, [alexandro.barrenechea@sdcountry.ca.gov](mailto:alexandro.barrenechea@sdcountry.ca.gov) , **Land Use Committee**. Community Input. Voting item.

16 Waiver of the Site Plan requirement for signage at Utique. Location: 405 S. Main Ave. Contact: Sandra Starr Mar, [sandeemar@icloud.com](mailto:sandeemar@icloud.com), County Planner: Dag Bunnemeyer, [dag.bunnemeyer@sdcountry.ca.gov](mailto:dag.bunnemeyer@sdcountry.ca.gov) **Design Review Board Committee**. Public input. Voting Item.

17. FOR PLANNING GROUP:

Authorization of Teleconferencing Meeting Option Pursuant to Government Code Section 54953(e)

*Motion:*

Pursuant to Government Code section 54953(e)(3), a motion to find the legislative body has reconsidered the circumstances of the State of Emergency and state and local officials continue to recommend measures to promote social distancing. Community Input. Planning Group Discussion. Voting Item.

18. FOR COMMITTEES:

Authorization of Teleconferencing Meeting Option Pursuant to Government Code Section 54953(e)

A motion to find there is a proclaimed state of emergency and state and local officials have imposed or recommended measures to promote social distancing authorizing teleconferenced meetings pursuant to Government Code section 54953(e). [This motion is intended to apply to all of the legislative body subcommittees this legislative body has created.] [If the legislative body does not hold a meeting within the next 30 days, the chair is directed to review the status of the state of emergency and whether state or local officials continue to impose or recommend measures to promote social distancing. If both conditions exist, the chair is directed to memorialize such determination in writing and such writing will be presented for ratification at the next meeting of the legislative body.] Community Input. Planning Group. Voting Item.

19. PLANNING GROUP BUSINESS AS NEEDED:

Announcements & Correspondence:

Reports:

Chair's report: Housing

Michele McCaffery: Policy I-1 and Brown Act Reference/ Updates

Lee DeMeo & Jeniene Domercq: Parliamentary Procedure Reference

Mark Mervich: Technical Advisor in-person meetings

Tom Harrington: NCFPD Reports and information

Jim Loge: Social Media, Zoom, meeting recordings

Stephani Baxter: Trails Council & CSA-81

Anna Stahan: Cal Trans

Jacqui Kaiser: Schools

Victoria Stover: Representative to the I-15 DRB

Roy Moosa: Representative to the Fallbrook Community Forum

Jeniene Domercq: Representative to the Fallbrook Chamber of Commerce

Sheriff's Dept and CHP report

20. ADJOURNMENT

NOTE: The Fallbrook Planning Group occasionally has openings on its Committees for non-elected community members. Interested parties should contact the respective committee chairs: Land Use Committee Chair Steve Brown:([fallbrookplanning@aol.com](mailto:fallbrookplanning@aol.com)), Circulation Committee (Chair Roy Moosa: 760-723-1181), Parks & Recreation Committee (Chair Stephani Baxter: ([sbaxter.fcpg@gmail.com](mailto:sbaxter.fcpg@gmail.com)), Public Facilities Committee Chair Roy Moosa: (760-723-1181), Design Review Board Committee (Chair Eileen Delaney: [eileen.fallbrook@gmail.com](mailto:eileen.fallbrook@gmail.com), Ad-Hoc Cannabis Committee Chair, Jeniene Domercq, ([jeniendomercq@gmail.com](mailto:jeniendomercq@gmail.com)))

This is a preliminary agenda. If any changes are made, a final agenda will be posted at the Chamber of Commerce at least 72 hours prior to the meeting  
Eileen Delaney, Chairperson, Fallbrook Community Planning Group & Design Review Board.  
Email: [Eileen.fallbrook@gmail.com](mailto:Eileen.fallbrook@gmail.com)

OFFICIAL



PLANNING GROUP AGENDAS, MINUTES AND INFORMATION

[www.FallbrookPlanningGroup.org](http://www.FallbrookPlanningGroup.org)

<https://www.facebook.com/Fallbrook-Planning-Group-109111577938214>

Other information can also be found at:

<https://www.sandiegocounty.gov/pds/gpupdate/comm/fallbrook.html>.

FALLBROOK COMMUNITY PLANNING GROUP  
And  
DESIGN REVIEW BOARD

Chair, Eileen Delaney  
Eileen.fallbrook@gmail.com  
Regular Meeting  
Monday, September 19, 2022  
7:00 PM

Fallbrook Public Utilities District Board Room  
990 E. Mission Rd. Fallbrook

\*\*\*For their convenience, members of the public may attend in-person or virtually through Zoom

\*\*\*

Meeting ID: 725 856 7213- Passcode: AVOCADO –  
(Phone Passcode: 8180015)

<https://us02web.zoom.us/j/7258567213?pwd=amRZRVI5NnprMFVETjA2enNDcTZaUT09>

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Translate.

Este servicio de traducción se ofrece como recurso adicional para información.”

## APPROVED MINUTES

1. CALL TO ORDER – Meeting called to order at 7:00 PM
2. OATH OF OFFICE: Jerry Kalman
3. ROLL CALL
  - Present - Roy Moosa, Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan, Jim Loge, Tom Harrington, Jacqueline Kaiser (Virtual), Jeniene Domercq, Jerry Kalman
  - Absent (excused) - Lee De Meo
4. PLEDGE OF ALLEGIANCE
5. APPROVAL of the August 15, 2022 meeting minutes.
  - Motion to approve as presented by Jim Loge, 2<sup>nd</sup> Steve Brown
  - Voting yes - Roy Moosa, Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan , Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman
  - Absent - Lee De Meo
  - Motion carried

6. PUBLIC FORUM. Opportunity for members of the public to speak to the Group on any subject matter within the Group's jurisdiction but not on today's agenda. Three-minute limitation. Non-discussion and non-voting item.

- Ted Miyahara from San Diego Community Housing Corp notified the Planning Group that they will be pursuing 54-unit (age 62+) housing units on an undeveloped property at 528 E Alvarado in the coming months.
- Member of the community wanted to make the community aware of California SB1186 that has been signed by the governor.

## 7. Chair's Report: Public Forum

PG Chair clarified that all members of the public can speak no matter where they live per The Brown Act and I-1. The group may ask for the name and address of those wanting to share during public forum. To ensure the privacy of the speakers their personal information will not be required.

## 8. Updates on Rainbow MWD projects, Tom Kennedy, General Manager.

- Waste Water project on Mission and 76 project, work will transition to Old River RD in a couple of months. Project is about 50 percent complete and is on track to complete on time early next spring.
- Gird RD project – Crews found pipe that can be re-used, savings about half million dollars
- Larger project on Wilt RD adding 20" pipeline – will help reduce costs
- Applying for grant money 2 Heli-hydrants
- Will return to share plans on future projects

### Questions from PG

- Are there any updates on drought situation – response need snowpack in the Sierras and will depend on how the government is able to work out distribution of the Colorado River
- The traffic around Bonsall Elementary school is a concern – Response They will try to work off peak hours.
- Thanks for your support to work/collaborate with other agencies.

## 9. Presentation by Heather Conklin [heather.conklin@gmail.com](mailto:heather.conklin@gmail.com) about Mission Resource Conservation District and the services they offer Fallbrook residents.

- Heather Conklin shared presentation sharing all the services Mission Road Conservation District offers.
  - Most services are low to no cost.
  - PG member asked about the community garden. MRCD does support the community garden.

10. Request for Site Plan Exemption for a car port with solar. Location: APN: 103-246-52-00, 624 Elder St. Fallbrook. Applicant: George Lytal, Crestwood Behavioral Health. Contact: Ben Packard, [ben.parkard@currentdesigngroup.com](mailto:ben.parkard@currentdesigngroup.com) County Planner: Jeffery Melo,

[Jeffery.melo@sdcountry.ca.gov](mailto:Jeffery.melo@sdcountry.ca.gov) Community Input. **Design Review Board Committee**. Voting Item.

- Ben Packard was present to represent the project.
- Design Review Sub Committee Report – Committee voted approve as presented with one abstention.
- Motion to approve as presented by Tom Harrington, 2<sup>nd</sup> Michele Mc Caffery
- Voting yes - Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan , Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman
- Abstain – Roy Moosa
- Absent - Lee De Meo
- Motion carried

11. Minor deviation to change a Cricket Wireless wall-mount sign. Location: 1352 Mission Rd. APN: 1-04-202-22-0. Contact: Peter Lipsiwala, [peter.lapsiwala@permitsandmore.com](mailto:peter.lapsiwala@permitsandmore.com) ,County Planner: Hanna Wong, [Hannah.wong@sdcountry.ca.gov](mailto:Hannah.wong@sdcountry.ca.gov) . **Design Review Board Committee**. Community Input. Voting item.

- Pulled from agenda – Plans were approved in April

12. Request for a sidewalk across Brooke at Stagecoach due to church and preschool pedestrian traffic from the parking lot to Christ the King Church. Contac: Denny Linderman and John [Whitmer.smeacc@icloud.com](mailto:Whitmer.smeacc@icloud.com), **Circulation Committee**. Public input. Voting Item.

- Applicant did not attend at Sub Committee meeting to present. The Circulation Committee agreed to carry over to Oct 2022 meeting

13. Update on the progress and recommendations for the County’s Cannabis Ordinance. **Cannabis Adhoc Committee**. Community input. Voting item.

- Ad -Hoc Committee chair gave a report
- There were several questions addressing multiple topics. The committee will provide more updates when more details are available that would address to questions.
- Motion for the committee to meet monthly by Jeniene Domercq, 2<sup>nd</sup> Stephani Baxter
- Voting yes - Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan , Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman, Roy Moosa
- Absent - Lee De Meo
- Motion carried

14. PDS2020-STP20-021 Sign Program for Pala Mesa Market Project. Location: Contact: Jason Greminger, [jason.greminger@cciconnect.com](mailto:jason.greminger@cciconnect.com), County Planner: Sean Oberbauer, [sean.oberbauer@sdcountry.ca.gov](mailto:sean.oberbauer@sdcountry.ca.gov) . **Design Review Board Committee**. Community input. Voting item.

- Jim Simmons represented the project and shared the presentation
- Design Review Committee approved as presented
- Motion to approve as presented by Jim Loge, 2<sup>nd</sup> Victoria Stover
- Voting yes - Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan , Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman, Roy Moosa
- Absent - Lee De Meo
- Motion carried

15. PDS2022-AD-013 .Request for an Administrative Permit for small classification for Sandia Creek Winery. Location: APN 101-360-40-00, 40376 Sandia Creek Dr. Applicant: Vitals International Group [tim@vitalsintl.com](mailto:tim@vitalsintl.com) , Contact: Steve Rawlings [ser@rawlingspm.com](mailto:ser@rawlingspm.com) , County Planner: Alexandro Barrenechea, [alexandro.barrenechea@sdcounty.ca.gov](mailto:alexandro.barrenechea@sdcounty.ca.gov) , **Land Use Committee**. Community Input. Voting item.

- Project pull – The planner has not been able to review

16 Waiver of the Site Plan requirement for signage at Utique. Location: 405 S. Main Ave. Contact: Sandra Starr Mar, [sandeemar@icloud.com](mailto:sandeemar@icloud.com), County Planner: Dag Bunnemeyer, [dag.bunnemeyer@sdcounty.ca.gov](mailto:dag.bunnemeyer@sdcounty.ca.gov) **Design Review Board Committee**. Public input. Voting Item.

- Sandia Mar provided presentation
- Victoria Stover recused herself with the right to sit in the public. The location sells her products.
- Design Review Committee voted to approve as presented
- Motion to approve as presented by Steve Brown, 2<sup>nd</sup> Michele Mc Caffery
- Voting yes - Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Michele Mc Caffery, Anna Strahan , Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman, Roy Moosa
- Recused - Victoria Stover
- Absent - Lee De Meo
- Motion carried

## 17. FOR PLANNING GROUP:

Authorization of Teleconferencing Meeting Option Pursuant to Government Code Section 54953(e)

*Motion:*

Pursuant to Government Code section 54953(e)(3), a motion to find the legislative body has reconsidered the circumstances of the State of Emergency and state and local officials continue to recommend measures to promote social distancing. Community Input. Planning Group Discussion. Voting Item.

- Audience dismissed by PG member prior to discussion

- Motion to continue hybrid meetings by Ross Pike, 2<sup>nd</sup> Victoria Stover
- Voting yes - Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan , Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman, Roy Moosa
- No – Steve Brown
- Absent - Lee De Meo
- Motion carried

## 18. FOR COMMITTEES:

Authorization of Teleconferencing Meeting Option Pursuant to Government Code Section 54953(e)

A motion to find there is a proclaimed state of emergency and state and local officials have imposed or recommended measures to promote social distancing authorizing teleconferenced meetings pursuant to Government Code section 54953(e). [This motion is intended to apply to all of the legislative body subcommittees this legislative body has created.] [If the legislative body does not hold a meeting within the next 30 days, the chair is directed to review the status of the state of emergency and whether state or local officials continue to impose or recommend measures to promote social distancing. If both conditions exist, the chair is directed to memorialize such determination in writing and such writing will be presented for ratification at the next meeting of the legislative body.] Community Input. Planning Group. Voting Item.

- Motion to continue virtual meetings by Ross Pike, 2<sup>nd</sup> Michele Mc Caffery
- Voting yes - Eileen Delaney, Stephani Baxter, Steve Brown, Ross Pike, Mark Mervich, Victoria Stover, Michele Mc Caffery, Anna Strahan, Jim Loge, Jacqueline Kaiser, Jeniene Domercq, Jerry Kalman, Roy Moosa
- No – Steve Brown
- Absent - Lee De Meo
- Motion carried

## 19. PLANNING GROUP BUSINESS AS NEEDED:

Announcements & Correspondence:

Reports:

Chair's report: Housing – Now publishing agendas on multiple FB groups

Michele McCaffery: Policy I-1 and Brown Act Reference/ Updates

Lee DeMeo & Jeniene Domercq: Parliamentary Procedure Reference

Mark Mervich: Technical Advisor in-person meetings

Tom Harrington: NCFPD Reports and information - Harrington shared there are multiple strike teams coordinating between different agencies

Jim Loge: Social Media, Zoom, meeting recordings – FB page will allow sharing: We will be transitioning to SD County Zoom account.

Stephani Baxter: Trails Council & CSA-81

Anna Stahan: Cal Trans

Jacqui Kaiser: Schools

Victoria Stover: Representative to the I-15 DRB

Roy Moosa: Representative to the Fallbrook Community Forum

Jeniene Domercq: Representative to the Fallbrook Chamber of Commerce

Sheriff's Dept and CHP report

20. ADJOURNMENT

Minutes Approved Oct 17, 2022 – Submitted by Jim Loge Oct 21, 2022

NOTE: The Fallbrook Planning Group occasionally has openings on its Committees for non-elected community members. Interested parties should contact the respective committee chairs: Land Use Committee Chair Steve Brown:(fallbrookplanning@aol.com), Circulation Committee (Chair Roy Moosa: 760-723-1181), Parks & Recreation Committee (Chair Stephani Baxter: ([sbaxter.fcpg@gmail.com](mailto:sbaxter.fcpg@gmail.com)), Public Facilities Committee Chair Roy Moosa: (760-723-1181), Design Review Board Committee (Chair Eileen Delaney: [eileen.fallbrook@gmail.com](mailto:eileen.fallbrook@gmail.com), Ad-Hoc Cannabis Committee Chair, Jeniene Domercq, ([jeniendomercq@gmail.com](mailto:jeniendomercq@gmail.com))\

This is a preliminary agenda. If any changes are made, a final agenda will be posted at the Chamber of Commerce at least 72 hours prior to the meeting  
Eileen Delaney, Chairperson, Fallbrook Community Planning Group & Design Review Board.  
Email: [Eileen.fallbrook@gmail.com](mailto:Eileen.fallbrook@gmail.com)

OFFICIAL

PLANNING GROUP AGENDAS, MINUTES AND INFORMATION

[www.FallbrookPlanningGroup.org](http://www.FallbrookPlanningGroup.org)

<https://www.facebook.com/Fallbrook-Planning-Group-109111577938214>

Other information can also be found at:

<https://www.sandiegocounty.gov/pds/gpupdate/comm/fallbrook.html>.

**1 - 152**  
**I-15 CORRIDOR DESIGN REVIEW BOARD**

**\*\*\*MEETING AGENDA\*\*\***

**Thursday, September 17, 2020, 5:00 PM**

**Meeting Format:**

**Zoom Conference Call**

**Phone Number: (669) 900-6833**

**Meeting ID: 762 864 8116**

**A. ROLL CALL**

**B. PUBLIC COMMUNICATION:** Opportunity for members of the public to speak to the Group on any subject matter within the Group's jurisdiction that is not on the posted agenda.

**C. APPROVAL OF MINUTES:** Approval of minutes from August 12, 2020 meeting.

**D. ACTION ITEMS:**

1. PROJECT NAME: Fallbrook Commercial  
RECORD ID: PDS2020-STP-20-021  
PROJECT ADDRESS: 3233 Old Highway 395, Fallbrook, CA, 92028  
APN: 125-050-54-00

This is a request for evaluation of a proposed commercial project for conformance with adopted standards as noted in the I-15 Corridor Scenic Preservation Guidelines.

The Proposed Project consists of developing two new 6,000 SF commercial-retail buildings, a 3,250 SF convenience store with a 6-pump gas station with associated parking, internal circulation redevelopment, ingress/egress in an existing commercial center, and offsite improvements. The existing commercial center consists of a 9,075 SF commercial building, a Hamburger Restaurant, and a Nursery. A carwash will no longer be proposed.

**Community Input. Voting Item.**

- Owner: Rafid Hamika
- Owner's agent: Jason Greminger, CCI 760-471-2365
- County Project Manager: Bradley Sonnenburg, 858-694-3640

**E. GROUP BUSINESS**

1. Announcements and Correspondence Received
2. Discussion Items

**F. ADJOURNMENT**

**Disclaimer Language**

Public Disclosure

We strive to protect personally identifiable information by collecting only information necessary to deliver our services. All information that may be collected becomes public record that may be subject to inspection and copying by the public, unless an exemption in law exists. In the event of a conflict between this Privacy Notice and any County ordinance or other law governing the County's disclosure of records, the County ordinance or other applicable law will control.

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You can review any personal information collected about you. You may recommend changes to your personal information you believe is in error by submitting a written request that credibly shows the error. If you believe that your personal information is being used for a purpose other than what was intended when submitted, you may contact us. In all cases, we will take reasonable steps to verify your identity before granting access or making corrections.



Kerry Garza-Chair  
Delores Chavez Harmes-Secretary  
William Crocker  
Victoria Stover

## I-15 CORRIDOR DESIGN REVIEW BOARD

September 17, 2020 Meeting Minutes

Zoom Meeting 5:00 pm

**Attendance:** Kerry Garza, Delores Chavez Harmes, Victoria Stover, William Crocker

**Guests:** Jason Greminger, Matt Simmons

**Call to Order:** The meeting was called to order by Chair Garza at 5:06 pm via zoom conference.

**Welcome:** Chair, Kerry Garza, welcomed all and lead the group in the pledge of allegiance.

**Minutes:** Due to Covid-19 no minutes were reviewed. August & September minutes to be reviewed in October.

**Public Comments:** None.

### Action Items:

1. **Site Plan Waiver: Fallbrook Commercial** (APN: 125-050-54-00): Developing two new 6,000sf commercial retail buildings, a 3,250sf convenience store with 6 pump gas station and associated parking, internal circulation redevelopment, ingress/egress in an existing commercial center and offsite improvements. The existing commercial center consists of a 9,075sf commercial building, hamburger restaurant and a nursery. No objectionable changes were discussed. Project cannot be seen very easily from I-15. Discussion on siding, lighting and roof materials.

Jason and Matt made a virtual presentation and the group asked several questions. The group felt the architecture and scale was appropriate. The group extensively reviewed the photo simulations presented by the applicant of the project from the I-15 driver perspective. The visual impact seemed negligible. Stover made special mention that the project must comply with the Dark Sky Ordinance and the applicant assured us that it will have to comply by County standards.

A motion to approve the project was made by Harmes, seconded by Stover.

- a. **Vote: Aye-4 Nay-0 Abstain-0. Project Approved**

**Group Business:** None

**Next Meeting:** October 14, 2020 6pm via Zoom

**Adjournment:** 5:40 pm



# San Diego County Archaeological Society, Inc.

Environmental Review Committee

November 14, 2024

To: Mr. Sean Oberbauer  
Department of Planning and Development Services  
County of San Diego  
5510 Overland Avenue, Suite 310  
San Diego, California 92123

Subject: CEQA Section 15183 Findings  
Pala Mesa Commercial Project  
PDS2020-STP-20-021; LOG NO. PDS2020-ER-20-02-003

Dear Mr. Oberbauer:

I have reviewed the subject project's cultural resources aspects on behalf of this committee of the San Diego County Archaeological Society.

Based on the information contained in the documents posted on the PDS website, we have the following comments:

1. The cultural resources report mentions aerial photos from 1953 and later as having been checked. The 1928-29 series taken by the County should be checked and any necessary changes incorporated into the mitigation.
2. Otherwise, we concur with the archaeological and Native American monitoring as proposed in the 15183 Checklist.

SDCAS appreciates the opportunity to review and comment upon this project's environmental documents.

Sincerely,

A handwritten signature in black ink, appearing to read "James W. Royle, Jr.", is written over the typed name.

James W. Royle, Jr., Chairperson  
Environmental Review Committee

cc: Laguna Mountain Environmental  
SDCAS President  
File

**From:** [TERRY BELAJACK](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Pala Mesa Commercial  
**Date:** Sunday, November 3, 2024 6:40:57 PM

---

To whom it may concern:

After reading about the project proposed I wanted to voice my opinion. My husband and I live in the Aurora Heights Development and this project will greatly impact those of us that live here.

While we are not opposed to growth and development we do not feel that we need another gas station inconvenience store. Especially considering there is one right on the corner of 76 and 395. If you spent any time around this gas station and area, you would understand our concerns. Unfortunately, in our opinion, it does not attract a very good clientele. We see what goes on with all the truckers that park there etc. Also the increase of traffic for a gas station is much more than the existing businesses. We have enough issues with the traffic now along the 395.

I have spoken with my neighbors and they feel the same.

I appreciate you taking time to read this and hope that the gas station will be eliminated from the project.

Respectfully,

Dan & Terry Belajack

3109 Via De Todos Santos

Fallbrook, Ca

**From:** [Lenny Castaneda](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Re: Project at 3233 Old Highway 395, Fallbrook  
**Date:** Monday, November 4, 2024 11:35:57 AM  
**Attachments:** [Image\\_241030\\_171010.jpeg](#)

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Hi Sean,

Also wanted to follow up on will you be adding speed pumps, traffic lights and lowering the speed limit from 55 to 35?

There are already traffic accidents that occur at Aurora Heights entrance so stop lights, speed bumps and speed limits must be considered for the project scope.

Take care,

Lenny Castaneda  
O: 619-431-0039

On Wed, Oct 30, 2024 at 5:10 PM Lenny Castaneda <[lenny.castaneda@gmail.com](mailto:lenny.castaneda@gmail.com)> wrote:

Hi Sean,

Do you know when is the estimated time to start this proposed project?

And an estimate how many months to complete?

**From:** [Steve Ballard](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Pala Mesa Commercial  
**Date:** Friday, November 8, 2024 7:17:29 AM

---

Mr Oberbauer,

I'm writing to express my concern that this project will have a very negative impact on our neighborhood. High speed auto collisions have doubled in the last year since a poorly designed lane change was completed. Having a gas station adjacent to housing will degrade the quality of life and also property values. Retail stores don't seem as problematic but a substantial masonry wall around the project would mitigate noise and crime. 35 feet tall structures and lights will have a damaging effect on homes nearby.

Please let me know when and where I can be present to address the concerns of our neighborhood. I am the HOA president in the Aurora Heights community and the vast majority are extremely concerned about this project.

Thank you,

Steve Ballard. 619-889-6583

Sent from my iPhone

November 12, 2024

Planning & Development Services  
5510 Overland Avenue, Suite 110  
San Diego, CA 92123

Subject: Project Type: Site Plan (15183 Environmental Findings)  
Project Name: Pala Mesa Commercial  
Project Case Number(s): PDS2020-SP-20-021; Log No. PDS2020-ER-20-02-003  
Application: TGHR3, LLC

We are writing this letter in representation of our statement of concerns related to the communities located directly behind the proposed development referenced in the subject lines above. Following is a list of concerns that directly affect public safety and environmentally related issues.

- **Traffic Congestion and Accident Concerns:**
  1. Since the enhancements of Highway 395 was completed directly in the front of the Aurora Heights Development there have been numerous near accidents and an actual accident.
  2. Excessive Traffic exiting onto 395 from the 15 freeway from both North and South bound has created obvious accident hazards. It has become dangerous to exit our community. Our Association has communicated this fact on numerous occasions.
  3. The addition of added parking, additional merchants and strip center would further increase accident activity.
  
- There also have been multiple issues with traffic coming up Via Belmonte West bound and entering the communities both to the left and right of the road. Traffic exits the Via Belmonte without regard for oncoming traffic, children, pets, bikes or pedestrians.
  1. The possibility of injuries are numerous, car accidents, children riding bikes in the street, pedestrians walking in their community. All of the aforementioned are at the mercy of the driver exiting the strip center into our communities.
  
- **Possible alleviation of these concerns:**
  1. Gates could be provided to restrict entry into the neighborhoods from Via Belmonte. Neighborhood traffic could access the strip center from alternate, safer streets to the North and South.
  2. Via Belmonte should become part of the commercial development and no longer open into the two residential developments to the rear of the proposed project.
  3. Part of this project could be to assist Aurora Heights and Pala Mesa communities in privatizing entry. Aurora Heights Association has

considered privatizing the streets and installing gates at the Highway 395 entrance and the entrance from Via Belmonte. To ensure public safety assistance with installation of appropriate gates and privatization of the streets should be considered.

The Sheriff's Department has previously informed our community that being so close to the Park and Ride creates theft issues, easy in and out access to our community make it vulnerable to theft. If this project is destined to be approved, please consider the long-term effect on the tax paying citizens of Aurora Heights and Pala Mesa residential housing communities. We should protect the homeowners, pedestrians, children and pets that frequent both communities daily.



Sandra R. Benson



Billy R. Benson

4731 Silent Knoll Drive  
Fallbrook, CA 92028  
760-450-4179

**From:** [Panna Hazarika](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Reference: Pala Mesa commercial project  
**Date:** Tuesday, November 12, 2024 1:42:25 PM

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Hello Mr. Oberbauer,

We are residents of the Aurora Heights Community, and our address is 4739 Silent Knoll Drive, Fallbrook, CA 92028. We are directly impacted by your proposed plan for the Pala Mesa Commercial project.

We reviewed the SD County brochure and have some comments regarding your proposed Pala Mesa Commercial project:

1. We give credit to the county for attempting to revitalize parts of our community. If done well, the proposed retail center could add value to our area. However, there are a few serious flaws with your proposal which will affect the two adjacent residential communities, Aurora Heights and Pala Mesa.
2. Our primary concern is Via Belmonte Street. A portion of the street should be closed to eliminate any direct access to Aurora Heights from the retail center. This change alone could address our concerns about unwanted traffic and criminal elements from entering Aurora Heights and the neighboring Pala Mesa communities. The portion of the Via Belmonte street that goes down from Via Del ToDos Santos (VDTS) to connect with Old Highway 395 doesn't serve much purpose. Our recommendation is to close this section of the street. Closing Via Belmonte's access to the center would help alleviate some of these safety issues.

I had written to the County a year ago, requesting four-way stop signs at the VDTS and Via Belmonte intersection due to frequent near-accidents due to blind corners, but I never received a response. Residents in our communities who currently use Belmonte to access Route 395 could just as easily use the traffic lights instead.

3. Adding a second gas station seems unnecessary in a community of this size. The existing Mobil station next door already meets our needs. The Gas Station should be removed from the current plan. Instead more retail shops and an extended parking lot should be accommodated.

These recommendations reflect the views of the Aurora Heights HOA and its members, and should be considered seriously. We suggest a face to face hearing with the residents.

Best Regards,

Panna & Ajanta Hazarika,  
 4739 Silent Knoll Drive, Fallbrook, CA 92028.  
[pannahazarika@gmail.com](mailto:pannahazarika@gmail.com), (858) 518-1462

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11/12/2024

San Diego County Planning & Development Services (PDS)  
Project Processing Counter and 5510 Overland Avenue, Suite 110  
San Diego, California 92123

Attn: Sean Oberbauer  
(619) 323-5287  
Sean.Oberbauer@sdcounty.ca.gov

**Subj:** PDS2020-STP-20-021; LOG NO. PDS2020-ER-20-02-003

PROJECT NAME: Pala Mesa Commercial

Address: 3233 Old Highway 395, Fallbrook, CA 92028

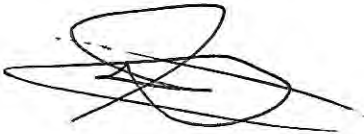
I wish to comment on the subject project. My wife and I reside in the Aurora Heights community near to the proposed project. While we do not object to a renovation of the market and attending buildings plus improved parking we do have concerns. They are as follows:

1. Increased traffic with minimal Highway 395 improvements. The proposed single left turn lane is not much of an improvement. Traffic comes down Highway 395 at speeds many times more than the posted limit. There have been numerous accidents in that area since we moved in approximately 4+ years ago. We believe that significant widening and speed reduction improvements need to be part of this plan.
2. The proposed 130+ paved parking spaces are indicative of vastly increased traffic. Many semitrucks park in the area and alongside Highway 395. This is done sometimes with truck(s) edging out into the highway. This is unsafe in our view. We believe that parking needs to address the semitruck problem.
3. A large parking area creates more crime issues. The two neighborhoods have had to deal with a lot of crime located in the current "park and ride." This has impacted our quality of life and worse created what we believe is a heavy workload for the San Diego County Sheriff's. We believe that another large parking area would worsen the crime issue.

4. The current shopping center is extremely close to long existing homes. These homes have views that extend to the eastern mountains. They also must deal with noise associated with the current shopping center. Some mitigation must occur to ensure homeowners don't lose their view and that they are not impacted by light.
5. Violation of the Light Pollution Code (LPC), also known as the Dark Sky Ordinance. We have real concerns about signage size and height plus parking lighting. It seems nothing has been presented about this. If a 35 foot (or more) sign is built that impacts view from current homes to the West. It also seems contradictory to the goal of LPC.
6. Proposed addition of 12 gasoline pumps is excessive. There is already one gasoline station within stone's throw of the proposed new project. Again, this will increase traffic in the area and put it much closer to current houses. Unsatisfactory...

I hope these concerns are understood, discussed and addressed.

Sincerely,



Stevan Martin, LCdr, USN, Ret.  
3040 Jicarilla Dr.  
Fallbrook, CA 92028  
858-229-5281

Copy to: Aurora Heights HOA Directors

**From:** [terryocean.Terry](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] 3233 Old Highway 395 Fallbrook project.PDS2020-STP-20-021  
**Date:** Friday, November 15, 2024 10:04:30 AM

---

I support the project and hope to goes through as planned Thank you Gary Terry 4735 Silent Knoll Dr. Fallbrook 92028

Get [Outlook for Android](#)

**From:** [MikeD](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Pala Mesa Commercial, PDS2020-STP-20-021; LOG NO. PDS2020-ER-20-02-003  
**Date:** Sunday, November 17, 2024 7:55:26 PM

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Dear Mr Oberbauer,

I am on the HOA Board of the Aurora Heights community which is diagonal from the Pala Mesa Market and this proposed update. We have met with the HOA Board members of Pala Mesa Village which is directly behind this market.

After reviewing the designs and architecture documents I can say that I am pleased with the updates. As I have mentioned to others, anyone who didn't think this update was coming at some point just isn't paying attention. The old market sits on prime real estate and if not updated by the current owner, would be bought by some developer sooner or later.

We do have a few concerns that we're hopeful you and the owner also share.

- Additional traffic. The upgrade to the old gas station across the street brought in a lot more traffic at all hours of the night. Putting another gas station directly in front of the Pala Mesa Village will do even more. Quality of life for those close by is definitely affected. We do question the need for a 2nd gas station right across the street from an existing one. If anything, that should be on the east side of the highway.

- Homelessness and lack of public restrooms. Homeless tend to congregate around the other station, and there's no rest room for them. They walk to the entrance of Pala Mesa Village and do their business behind the wall. A few sad souls go with shovels every other week to clean it up. What is really needed is a full shower and restroom station in the rest stop within the other gas station parking lot. Why that wasn't part of that upgrade is a question.

- Signage and lighting. I'm pleased to see that no 50' spinning Fuel Station sign is included in the plans. Any other lighting should be angled down and away from the homes nearby.

- Retaining walls and landscape break. I'm also pleased to see that additional landscape trees are to be planted as a break at the north end of the plaza along Via Belmonte. And there is a new retaining wall behind the plaza. It isn't clear how tall that wall will be, but if it extends higher than the street level of Via di Todos Santos, it will block the lighting and noise for the homes directly behind.

Otherwise, the designs look nice, and have retained a country feel that is desired in this Fallbrook area.

Thank you for taking our concerns into consideration.

--

[Mike](#)

-----

Michael J DeLessio  
**Aurora Heights HOA Board**  
760-271-4832  
[mikedellessiohoa@gmail.com](mailto:mikedellessiohoa@gmail.com)

**From:** [Valencia Warner-Saadat](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Fwd: Aurora Heights Neighbor Concerns regarding PDS202-STP-20-021 Pala Mesa Commercial Project.  
**Date:** Monday, November 18, 2024 2:58:35 PM

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Dear Mr. Oberbauer,

I am writing as a concerned resident regarding the proposed construction of a new gas station and retail shops near our neighborhood. While I understand the potential benefits of economic development, I have significant concerns about its impact on the residential community, particularly given the proximity of another gas station less than half a mile away.

The proposed location is adjacent to residential homes in a low-light community that has already experienced an uptick in crime, partly attributed to existing structures in the vicinity. To minimize potential negative impacts, I would like to request the following accommodations be considered if the project moves forward:

**1. Restricting Access to Neighborhoods Behind the Station:**

Install gates on the access road adjacent to the proposed construction area and near the traffic light to reduce traffic into the residential areas. This would help maintain the safety and privacy of the neighborhood.

**2. Low-Intensity Lighting:**

Ensure that lighting is designed to minimize light pollution, particularly after hours, to prevent illumination from affecting nearby homes.

**3. High Barrier Wall:**

Construct a high sound barrier wall between the construction project and the residential area to reduce noise pollution and enhance privacy.

I believe these measures are reasonable and would significantly mitigate the potential disruptions and risks to the community. I hope the developers and decision-makers will take these suggestions into account to support a more harmonious coexistence between the new development and the surrounding residential area.

Thank you for your attention to this matter. I would appreciate an opportunity to discuss these concerns further and look forward to your response.

Respectfully,  
Valencia I. Saadat  
3153 Via De Todos Santos  
Fallbrook CA, 92028

[valsaadat@gmail.com](mailto:valsaadat@gmail.com)  
760-497-3101

**From:** [Leah G.](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Pala Mesa Commercial: Gas Station Proposal  
**Date:** Monday, November 18, 2024 2:49:30 PM

---

Hello Sean,

I am reaching out regarding the new gas station proposal in the Pala Mesa Commercial Shopping Center. I believe this new station will be a tremendously positive addition to our community. Currently we have only one station within a 15 minute drive, and unfortunately this station is very poorly maintained. There has been countless times when myself or neighbors are in a pinch for gas, and when we pull into the current station they are out of gas. This has strained my family, and neighbors on what seems to be a weekly occurrence. Neighbors have told stories about running out of gas on the side of the road due to that station not having gas. Not only does the current station run out of gas routinely, but they also have backed up traffic and a lack a bathroom. On any given time of day the pumps have a 15+ minute wait to fill up. If someone needs to use a restroom during their wait there are no restrooms available. Overall, the current gas situation for the Fallbrook community is very inconvenient and a hinderance to a growing community.

The convenience of a new facility would be greatly appreciated by my family, and many of the neighbors in the Horse Creek Ridge Community. As our neighborhood continues to grow with new construction, in addition to the 15 Freeway widening, I believe a new gas station is inevitable. Please let me know If you would like to hear more from the local neighbors the Horse Creek Ridge Community Association (located across the freeway from the proposed site).

If there is any more information you need please don't hesitate to reach out.

Thank you,  
Leah Mesler  
35866 Bay Sable Ln,  
Fallbrook, CA 92028

**From:** [Patrick Hesano](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Pala Mesa Commercial  
**Date:** Monday, November 18, 2024 1:41:12 PM

---

Hello this is Patrick Hesano. I am the homeowner at 35962 Shetland Hills East, Fallbrook. I just wanted to say that my wife and I are excited about a possible new gas station in the area. There is only one gas station here currently and it's always super busy. The customer service is horrible and their pumps are always down. A new station will definitely help out.

Sent from my iPhone

**From:** [Thomas Cosgrove](#)  
**To:** [Oberbauer, Sean](#)  
**Subject:** [External] Gas station  
**Date:** Wednesday, November 20, 2024 10:33:09 AM

---

Hi Sean,

Tom Cosgrove here. I am on the board of Aurora Heights HOA. We are very concerned about the addition of another gas station so close to our community. The crime rate here and number of transient people here is a major concern. The existing Mobil gas station is bad enough with the riff raff it brings. Other concerns are the high speed traffic, lighting, signage and noise this will bring to us and the Pala adobe homes just south of us.

We are ok with building renovations but are dead set against the addition of another gas station and are willing to do what ever it takes to stop that addition. Please reconsider.

Thanks for your time.

Tom Cosgrove

Aurora Heights HOA.





**County of San Diego, Planning & Development Services**  
***Project Planning Division***

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**Memorandum**

**TO:** File

**FROM:** Sean Oberbauer, Project Manager

**SUBJECT:** Response to Comments; Pala Mesa Commercial; PDS2020-STP-20-021;  
PDS2020-ER-20-02-003

**DATE:** January 23, 2025

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The following are staff's responses to comments received during the public disclosure period for the draft Statement of Reasons for Exemption from Additional Environmental Review and 15183 Checklist pursuant to CEQA Guidelines 15183, dated October 17, 2024. The draft Statement of Reasons for Exemption from Additional Environmental Review and 15183 Checklist was circulated for public disclosure from October 17, 2024 to November 18, 2024. 12 comments and one late letter were received and responses have been provided below.

**O-1. Response to comments received from Jim Royle, San Diego County Archaeological Society, Inc., November 14, 2024:**

**O-1:** The comment states that historic imagery earlier than 1953 should be reviewed including the potential for the site to be available in the 1920s series. The commenter concurs with the remaining analysis and measures outlined in the 15183 checklist.

Historic aerials were reviewed earlier than 1953 including imagery from the 1930s. No additional structures or materials were observed as part of the imagery. The site has been previously cleared and used for a variety of agricultural uses or fallow land where agricultural uses ended up until the construction of the market and commercial uses in the 1970s.

No changes were made to the environmental findings and checklist as a result of the comment.

**I-1. Response to comments received from Dan and Terry Belajack, November 3, 2024:**

**I-1:** The comment states concerns related to not needing another gas station in the area, having individuals gather on-site with crime risks, and increases in traffic.

The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with 2362 and 2980 of the Zoning Ordinance, the use of a gas station is a

## Response to Comments PDS2020-STP-20-021

permitted use within the zone with limitations to not allow open storage of goods or materials, and all repair and lubrication services shall take place in an enclosed building. The Project does not include open storage of goods or materials or outdoor repair and lubrication services.

The Project includes remodeling of an existing commercial plaza on a Project site with vacant areas on the northern and southern portions of the property. Construction of the Project will result in removal of the vacant areas and additional lighting within a parking lot which will assist with security concerns. All lighting is required to comply with the lighting ordinance and performance standards of the Zoning Ordinance which requires all lighting to be placed facing downward and shielded.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

No changes were made to the environmental findings and checklist as a result of the comment.

**I-2. Response to comments received from Lenny Castaneda, November 4, 2024:**

**I-2:** The comment discusses questions regarding when construction may occur for the Project and concerns associated with traffic and accident history in the area and if there will be any speed bumps or other similar traffic design features.

The Project has not been approved as of the date of response to comments or at the time when the comment was received. If the Project was approved, the applicant would need to satisfy conditions as well as obtain construction-related permits which can take several months to several years.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

No changes were made to the environmental findings and checklist as a result of the comment.

### **I-3. Response to comments received from Steve Ballard, November 8, 2024:**

**I-3:** The comment states concerns associated with traffic concerns, the use of a gas station, property values, noise, crime, and lighting.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements for the processing of the Site Plan. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with 2362 and 2980 of the Zoning Ordinance, the use of a gas station is a permitted use within the zone with limitations to not allow open storage of goods or materials, and all repair and lubrication services shall take place in an enclosed building. The Project does not include open storage of goods or materials or outdoor repair and lubrication services.

The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient

noise levels are anticipated to be in excess of 60 dBA. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the northwestern property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development northwest of the Project site. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

The Project includes remodeling of an existing commercial plaza on a Project site with vacant areas on the northern and southern portions of the property. Construction of the Project will result in removal of the vacant areas and additional lighting within a parking lot which will assist with security concerns. All lighting is required to comply with the lighting ordinance and performance standards of the Zoning Ordinance which requires all lighting to be placed facing downward and shielded.

Commercial lighting would be required to conform with the County's Light Pollution Code to prevent spillover onto adjacent properties and minimize impacts to dark skies. The project has been conditioned to ensure conformance with the County's Lighting Code during any processing of a building permit for the project. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties. Lighting for the signage and architectural features of the project have been designed to be fully shielded and externally illuminated such as the monument sign for the Project.

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Department of Public Works staff will be reaching out to the commenter in order to address offsite concerns regarding lane changes that have been completed north of the Project site.

No changes were made to the environmental findings and checklist as a result of the comment.

#### **I-4. Response to comments received from Sandra R. Benson and Billy R. Benson, November 12, 2024:**

**I-4:** The comment discusses concerns associated with traffic operations and access specifically related to previous restriping of Old Highway 395 as well as operations of Via Belmonte.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements for the processing of the Site Plan. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

A portion of the Project site is adjacent to Via Belmonte and Via Belmonte is a road identified in the County-maintained system and open to the public. Multiple properties

have access to Via Belmonte and a gate cannot block a County-maintained-road if it provides access to the public. The Project does not show any access points to Via Belmonte and the entrance of the Project site will be provided by two driveways connecting to Old Highway 395 which will be improved as part of the Project. The half width improvements for Via Belmonte will require approval of an improvement plan which is reviewed by the Department of Public Works as part of construction permits for the Project.

No changes were made to the environmental findings and checklist as a result of the comment.

## **I-5. Response to comments received from Panna and Ajanta Hazarika, November 12, 2024:**

**I-5:** The comment discusses support for remodeling the retail center with concerns focusing on traffic operations related to Via Belmonte as well as the addition of a second gas station in the community.

The Project is submitted by a private applicant and not a County-initiated project.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements for the processing of the Site Plan. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent

with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

A portion of the Project site is adjacent to Via Belmonte and Via Belmonte is a road identified in the County-maintained system and open to the public. Multiple properties have access to Via Belmonte and a gate cannot block a County-maintained-road if it provides access to the public. The Project does not show any access points to Via Belmonte and the entrance of the Project site will be provided by two driveways connecting to Old Highway 395 which will be improved as part of the Project. The half width improvements for Via Belmonte will require approval of an improvement plan which is reviewed by the Department of Public Works as part of construction permits for the Project.

The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with 2362 and 2980 of the Zoning Ordinance, the use of a gas station is a permitted use within the zone with limitations to not allow open storage of goods or materials, and all repair and lubrication services shall take place in an enclosed building. The Project does not include open storage of goods or materials or outdoor repair and lubrication services.

No changes were made to the environmental findings and checklist as a result of the comment.

## **I-6. Response to comments received from Stevan Martin, November 12, 2024:**

**I-6:** The comment discusses concerns associated with increased traffic and road improvements to Old Highway 395, parking concerns, increased crime due to parking areas, noise and light generated by the Project due to the Project's proximity to residential homes, and the addition of 12 gas pump stations and the use of a second gas station in the development area.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technically Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT



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because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements for the processing of the Site Plan. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

The number of spaces required for the Project are set by the Zoning Ordinance and square footages of buildings and uses. The Project is required to have adequate access and turning radius space for the use of semi-trucks or loading trucks. The Project is not able to control operations of an off-site Park and Ride.

The Project includes remodeling of an existing commercial plaza on a Project site with vacant areas on the northern and southern portions of the property. Construction of the Project will result in removal of the vacant areas and additional lighting within a parking lot which will assist with security concerns. All lighting is required to comply with the lighting ordinance and performance standards of the Zoning Ordinance which requires all lighting to be shielded.

The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the northwestern property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development northwest of the Project site. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties

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are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

Commercial lighting would be required to conform with the County's Light Pollution Code to prevent spillover onto adjacent properties and minimize impacts to dark skies. The project has been conditioned to ensure conformance with the County's Lighting Code during any processing of a building permit for the project. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties. Lighting for the signage and architectural features of the project have been designed to be fully shielded and externally illuminated such as the monument sign for the Project. The Project is set at a lower elevation than adjacent homes and a conceptual sign package for the Project does not include a 35-foot sign.

The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with 2362 and 2980 of the Zoning Ordinance, the use of a gas station is a permitted use within the zone with limitations to not allow open storage of goods or materials, and all repair and lubrication services shall take place in an enclosed building. The Project does not include open storage of goods or materials or outdoor repair and lubrication services. The traffic concerns associated with the use of a gas station have been previous discussed in the response.

No changes were made to the environmental findings and checklist as a result of the comment.

## **I-7. Response to comments received from Gary Terry, November 15, 2024:**

**I-7:** The comment discusses general support for the Project.

The comment includes general support for the Project and no changes to the environmental findings and checklist were made as a result of the comment.

## **I-8. Response to comments received from Michael J. DeLessio, November 17, 2024:**

**I-8:** The comment discusses general support for the redevelopment and remodeling of the Project as well as concerns and focus items associated with additional traffic caused by the use of an additional gas station in the area, homelessness and public restrooms, signage and lighting, and retaining walls and landscaping.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements for the processing of the Site Plan. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

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The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with 2362 and 2980 of the Zoning Ordinance, the use of a gas station is a permitted use within the zone with limitations to not allow open storage of goods or materials, and all repair and lubrication services shall take place in an enclosed building. The Project does not include open storage of goods or materials or outdoor repair and lubrication services. The traffic concerns associated with the use of a gas station have been previously discussed in the response.

The Project does not include public restrooms. However, each commercial business may include restrooms for customers. The Project does not include construction to the off-site Park and Ride where the commenter states that a shower and restroom station should have been included.

Commercial lighting would be required to conform with the County's Light Pollution Code to prevent spillover onto adjacent properties and minimize impacts to dark skies. The project has been conditioned to ensure conformance with the County's Lighting Code during any processing of a building permit for the project. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties. Lighting for the signage and architectural features of the project have been designed to be fully shielded and externally illuminated such as the monument sign for the Project. The Project is set at a lower elevation than adjacent homes and a conceptual sign package for the Project does not include a 35-foot sign.

The commenter supports additional landscaping and requests clarification on the height of retaining walls. The majority of retaining walls for the Project are required in the rear of the Project site and range in size from approximate two to 12 feet tall.

The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the northwestern property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development northwest of the Project site. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties

are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

No changes were made to the environmental findings and checklist as a result of the comment.

### **I-9. Response to comments received from Valencia I. Saadat, November 18, 2024:**

**I-9:** The comment discusses concerns associated with access to adjacent neighborhoods along adjacent roadways, lighting, and noise walls.

A portion of the Project site is adjacent to Via Belmonte and Via Belmonte is a road identified in the County-maintained system and open to the public. Multiple properties have access to Via Belmonte and a gate cannot block a County-maintained-road if it provides access to the public. The Project does not show any access points to Via Belmonte and the entrance of the Project site will be provided by two driveways connecting to Old Highway 395 which will be improved as part of the Project. The half width improvements for Via Belmonte will require approval of an improvement plan which is reviewed by the Department of Public Works as part of construction permits for the Project.

Commercial lighting would be required to conform with the County's Light Pollution Code to prevent spillover onto adjacent properties and minimize impacts to dark skies. The project has been conditioned to ensure conformance with the County's Lighting Code during any processing of a building permit for the project. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties. Lighting for the signage and architectural features of the project have been designed to be fully shielded and externally illuminated such as the monument sign for the Project.

The comment discusses requests for construction of a barrier wall for noise and privacy. The existing landscaping along the western boundary of the Project site adjacent to Via

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de Todos Santos is not proposed to be removed as part of the Project and can assist with screening the site adjacent roadways and views. The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the northwestern property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development northwest of the Project site. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

No changes were made to the environmental findings and checklist as a result of the comment.

**I-10. Response to comments received from Leah Mesler, November 18, 2024:**

**I-10:** The comment discusses general support for the Project including the use of an additional gas station in the Project vicinity.

The comment includes general support for the Project and no changes to the environmental findings and checklist were made as a result of the comment.

**I-11. Response to comments received from Patrick Hesano, November 18, 2024:**

**I-11:** The comment discusses general support for the Project including the use of an additional gas station in the Project vicinity.

The comment includes general support for the Project and no changes to the environmental findings and checklist were made as a result of the comment.

**LL-1. Response to comments received from Tom Cosgrove, November 20, 2024:**

**LL-1:** The comment was received on November 20, 2024 and is considered a late letter as it was received after the conclusion of the public disclosure period. The comment discusses concerns associated with the use of a gas station, crime, traffic, lighting, signage, and noise.

The property is subject to the General Commercial (C36) Zoning Use Regulation. In accordance with 2362 and 2980 of the Zoning Ordinance, the use of a gas station is a permitted use within the zone with limitations to not allow open storage of goods or materials, and all repair and lubrication services shall take place in an enclosed building. The Project does not include open storage of goods or materials or outdoor repair and lubrication services. The traffic concerns associated with the use of a gas station have been previously discussed in the response.

The Project includes remodeling of an existing commercial plaza on a Project site with vacant areas on the northern and southern portions of the property. Construction of the Project will result in removal of the vacant areas and additional lighting within a parking lot which will assist with security concerns. All lighting is required to comply with the lighting ordinance and performance standards of the Zoning Ordinance which requires all lighting to be placed facing downward and shielded.

The Project consists of remodeling and expanding an existing commercial plaza. In accordance with a Local Mobility Analysis prepared for the Project by Darnell and Associates dated March 4, 2022, the Project is anticipated to generate a gross estimate of 2,918 average daily trips. All proposed uses of the Project are considered locally serving commercial uses that are intended to serve residents in the Fallbrook community and motorists traveling along Interstate 15. By taking into account pass-by trips due to several factors such as the nature of the uses of the Project, the combination of

commercial uses, and the location of the Project near Interstate 15 and Old Highway 395, the Project is anticipated to generate a net total of 1,310 daily trips. In accordance with the County of San Diego Transportation Study Guide dated September 2022 and the Governor's Office of Planning and Research Technical Advisory on Evaluating Transportation Impacts in CEQA dated December 2018, the Project is considered to have a less than significant impact related to Transportation and Traffic associated with VMT because it consists of less than 50,000 square feet of locally serving commercial uses. In accordance with the Transportation Study Guidelines, a Local Mobility Analysis was prepared for the Project and evaluated operations of nearby road segments and intersections within the Project vicinity. The Local Mobility Analysis includes a recommendation for the installation of a left turn lane and minor widening to Old Highway 395 in order to improve operations of Old Highway 395. The center turn lane will assist in reducing queuing of vehicles turning into the Project from Old Highway 395. The Project also includes half width improvements to Via Belmonte. The Project required coordination with the Department of Public Works for review of road improvements related to safety and design standards. The Department of Public Works has concurred with the Project design related to road improvements for the processing of the Site Plan. The Project does not include speed bumps associated with the Project. Therefore, the Project is consistent with traffic related requirements with the inclusion of project design features and road improvements and is anticipated to have a less than significant impact related to traffic and transportation.

Commercial lighting would be required to conform with the County's Light Pollution Code to prevent spillover onto adjacent properties and minimize impacts to dark skies. The project has been conditioned to ensure conformance with the County's Lighting Code during any processing of a building permit for the project. The Project is conditioned to be subject to the performance and lighting standards outlined Section 6300 of the Zoning Ordinance in order to prevent light pollution and spill onto adjacent properties. Lighting for the signage and architectural features of the project have been designed to be fully shielded and externally illuminated such as the monument sign for the Project. The Project is set at a lower elevation than adjacent homes and a conceptual sign package for the Project has been included as part of the application.

The Project consists of remodeling and expansion of an existing commercial plaza and does not propose residential uses or noise sensitive land uses. Due to the proximity of the Project site and surrounding residential land uses to Interstate 15 and Old Highway 395 as well as the noise contours identified in the General Plan, the existing ambient noise levels are anticipated to be in excess of 60 dBA. Noise generated as a result of traffic by the Project will be negligible and is not anticipated to be discernible compared to the existing noise generated by traffic along Old Highway 395 and Interstate 15 and the existing operations of the Project. It should be noted that the residences near the Project site are located along the northwestern property boundaries and portions of the residential development contain existing noise walls that were constructed as part of the residential development northwest of the Project site. Additionally, the majority of grading associated with the Project will occur in the rear of the property in fill slopes and will



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include retaining walls in order to support the slopes in the rear of the property that are anticipated to assist in attenuating noise. Non-transportation noise generated by the project is not expected to exceed the standards of the Noise Ordinance at or beyond the project's property line. The site is zoned General Commercial (C36) that has a one-hour average sound limit of 60 dBA daytime and 55 dBA nighttime. The adjacent properties are zoned Single Family Residential (RS), Transportation and Utility Corridor (S94), and General Commercial (C36). The Transportation and Utility Corridor (S94) and General Commercial (C36) land use regulations are located directly north and east of the site and are subject to the sound limit of 60 dBA daytime and 55 dBA nighttime. The Single Family Residential (RS) land use regulation is located directly west of the site and is subject to the sound limit of 50 dBA daytime and 45 dBA nighttime. The mean one-hour average sound limit at the nearest residential property line is a sound limit of 55 dBA daytime and 50 dBA nighttime. Operations of the Project do not involve amplified music or noise producing equipment that would exceed applicable noise levels at the adjoining property line. The Project does not involve any noise producing equipment that would exceed applicable noise levels at the adjoining property line. The majority of noise generated by the Project is anticipated to be from traffic or maintenance vehicles. Therefore, the project does not propose any noise sensitive land uses and would not expose any existing noise sensitive receptors to noise levels that exceeds the County's noise standards and standards identified in the Noise Element of the General Plan.

No changes were made to the environmental findings and checklist as a result of the comment.